

Indiana Department of Transportation

County Elkhart Route CR 18, CR 13/CR 115 Des. No. 1401749

FHWA-Indiana Environmental Document CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM GENERAL PROJECT INFORMATION

Road No./County:

County Road (Rd) 18, Elkhart County

Designation Number:

1401749

Project Description/Termini:

Intersection Improvement

Northern Terminus (CR 13): 290 ft. north
Southern Terminus (CR 115): 600 ft. south
Western Terminus (CR 18): 530 ft. east
Eastern Terminus (CR 18): 410 ft. west

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

X	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager)
	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division)
	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval

N/A

ESM Signature

Date



Digitally signed by

Brandon Miller

Date: 2021.01.22

07:28:18 -05'00'

ES Signature

1/22/2021

Date

N/A

FHWA Signature

Date

Release for Public Involvement

N/A

ESM Initials

Date



ES Initials

10/21/2020

Date

Certification of Public Involvement

Angela Nicholson
Office of Public Involvement

01/06/2021

Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ES/District Env.

Reviewer Signature:

Ferris Fair

Date: 01/25/2021

Name and Organization of CE/EA Preparer: Lisa Vale, Troyer Group

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County Elkhart

Route CR 18, CR 13/CR
115

Des. No. 1401749

Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?	<table border="1"><tr><td>Yes</td><td>No</td></tr><tr><td><input type="checkbox"/></td><td><input checked="" type="checkbox"/></td></tr></table>	Yes	No	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Yes	No				
<input type="checkbox"/>	<input checked="" type="checkbox"/>				
If No, then:					
Opportunity for a Public Hearing Required?	<table border="1"><tr><td><input checked="" type="checkbox"/></td><td><input type="checkbox"/></td></tr></table>	<input checked="" type="checkbox"/>	<input type="checkbox"/>		
<input checked="" type="checkbox"/>	<input type="checkbox"/>				

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks: Notice of Entry letters were mailed to potentially affected property owners near the project area on October 25, 2018 and again on September 23, 2019, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. Sample copies of the Notice of Entry letters are included in Appendix G-1 through G-2.

Section 106

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of No Historic Properties Affected was published in *The Goshen News* on May 6, 2020 offering the public an opportunity to submit comment pursuant to 36 CR 8000.2(d), 800.3 (e), and 800.6(a)(4). The public comment period closed 30 days later on June 5, 2020. The text of the public notice and the affidavit of publication appear in Appendix D-69.

Public Information Meeting

A public information meeting was held at Crossroads Community Church on September 12, 2019 from 6:00-8:00 PM EST. The proof of public notice advertising the meeting time and location can be found in Appendix G-10. Public Information Meeting Handout examples can be found in Appendix G-11. A meeting notice was mailed to known property owners within the project area. The meeting consisted of a short presentation regarding the proposed improvements to the intersection and the opportunity for public feedback and discussion following the presentation. Public Information Meeting Handout examples can be found in Appendix G-11. The public concerns recorded at the meeting was in regard to the crash history, speed of vehicles, the proposed detour, and if utility relocation was feasible. The summary of comments and responses following the meeting can be found in Appendix G-21. Presentation and exhibits are published on the Elkhart County Highway website <http://www.elkcohwvy.org/portfolio/county-road-18-cr-13-cr-115-intersection-improvement/>.

Project Does Meet

The proposed project meets the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Public Involvement Manual* which requires the project sponsor to offer the public an opportunity to submit comment and/or request a public hearing. Upon the release of this document for public involvement, a legal notice was published in the *Elkhart Truth* on November 4, 2020 and November 12, 2020. The text of the legal notice is located in Appendix G-24 and the proof of its publication is located in Appendix G-26. The notices informed the public of the planned improvements and provided 15 days and 7 days, respectively, to provide comment or request a public hearing. Letters containing the text of the legal notices (Appendix G-28) were also sent on October 30, 2020 to property owners to be directly impacted by the project. Emails and letters were distributed among local elected officials. The letters with a list of its recipients are located in Appendix G-30.

Four public comments were received – two that requested a public hearing, and two that did not. The two comments that did not request a hearing were from Elkhart County Residents, one of which was an adjacent property owner. The adjacent property owner inquired about right-of-way impacts to his parcel and was provided a response indicating the amount of right-of-way proposed for acquisition from his parcel (Appendix G-31). The other commenter, an Elkhart County resident, commented that nobody is in favor of the roundabout, too much money is being spent, and that the roundabout will present issues for buses and emergency vehicles. This resident was provided an email response addressing those concerns (Appendix G-35).

Among the two requests for a public hearing, one was from an elected official – Concord Township Trustee Jim Weeber.

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Also requesting a public hearing was Concord Township Fire Chief Phil Sumpter. In an effort to address the concerns of Mr. Weeber and Mr. Sumpter, a meeting was scheduled with them, the County Highway Department and the engineering consultant. The meeting was conducted at 1:30 pm on Thursday, December 4th at Concord Township Fire Station No. 1. Mr. Weeber invited other elected officials and interested parties to the meeting. A complete list of the meeting invitees was requested of Mr. Weeber on December 21, 2020, but no response was received. Final meeting minutes documenting the discussion were issued after all participants had an opportunity to review the draft meeting minutes. No comments regarding the draft meeting minutes were received from the attendees. The meeting minutes can be found in Appendix G-37. The emails inviting draft review and the final issuances can be found in Appendix G-46. Primary discussion topics during the meeting involved the project's purpose and need, the project cost, alternatives considered, accommodation of large vehicles, and the maintenance of traffic plan. Comments were addressed either during the meeting or during post-meeting follow-up coordination. The maintenance of traffic plan will require unique special provisions to ensure one-way traffic access is maintained through the construction zone, which intends to accommodate trips by school buses and emergency vehicles. This is a firm project commitment. This will avoid lengthy detours for passage by these vehicles across the Elkhart River.

No additional written requests for a public hearing were received following the coordination meeting described in the previous paragraph. The project sponsor (Elkhart County) and INDOT were consulted to determine if a public hearing would be necessary. It was determined that a public hearing was not warranted given the nature of the comments received and how they have been resolved to date by way of the email responses and project discussion documented in the 12/4/2020 meeting minutes (Appendix G-37 through G-41).

INDOT Fort Wayne District reviewed and certified the public involvement procedures on January 6, 2021 as indicated on the signature on the first page of this Categorical Exclusion document.

Public Controversy on Environmental Grounds

Will the project involve substantial controversy concerning community and/or natural resource impacts?

Yes

No

☐☒

Remarks:

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Elkhart County INDOT District: Fort Wayne
Local Name of the Facility: CR 18, CR 115 and CR 13

Funding Source (mark all that apply): Federal ☒ State ☐ Local ☒ Other* ☐

*If other is selected, please identify the funding source: _____

PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

Need

The project is needed because operational safety issues currently exist at the intersection, and because operational efficiency continues to deteriorate due to current and projected traffic volumes.

During project scoping, crash data was obtained from Elkhart County's database between the dates of January 2014 to May 2016. A total of seventeen crashes occurred at the intersection. Of the seventeen crashes, eight were rear end collisions. All crashes resulted in property damage and there were no personal injuries or fatal crashes. Lack of awareness of the intersection may also be contributing to the accidents occurring, due to limited intersection sight distance and the short storage lane on CR 18 between CR 115 and CR 13.

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The intersection's current sight distance measurements are below the recommended minimum sight distance for this type of roadway based on the current *Indiana Design Manual* for CR 115 and CR 13. The minimum sight distance required for CR 115 is 305 ft. The current sight distance for CR 115 at this intersection is 105 ft. The minimum sight distance required for CR 13 is 200 ft. The current sight distance for CR 13 is 177 ft. The sight distance on CR 18 is currently compliant.

Level of Service (LOS) is a rating system for traffic congestion that analyzes roadways and intersections by categorizing traffic flow and assigning quality levels based on performance measures like vehicle speed, density, congestion, and other factors. A summary of the grade associated to each level of service is listed below:

- LOS A – free flow traffic
- LOS B – reasonable free flow
- LOS C – stable flow, or at near free flow
- LOS D – approaching unstable flow
- LOS E – unstable flow, operating at capacity
- LOS F – breakdown in flow or gridlock.

The existing intersection operates at a LOS of A to D during peak hours. However, at the predicted growth rate it is expected that the LOS will decrease to a level ranging from A to E within 20 years for the horizon year predictions in the year 2035. Specifically, northbound CR 115 is operating at LOS D which is approaching unstable flow, and is expected to be LOS E in 2035, representing unstable flow, operating at capacity. This increase in congestion would likely lead to an increase in traffic accidents at this intersection. Level of service A through D are considered acceptable in urban areas; however, LOS E and F are unacceptable. PM peak hour appears to be the controlling peak hour.

Existing Intersection Configuration LOS Rating Comparing 2015 & 2035:

Facility	Peak Hour Period	2015 LOS	2035 LOS
CR 18, Eastbound	PM Peak Hours	B	B
CR 18, Westbound	PM Peak Hours	B	B
CR 13, Southbound	PM Peak Hours	A	A
CR 115, Northbound	PM Peak Hours	D	E

Purpose

The purpose of the project is to increase the operational safety at the intersection, which includes reducing vehicular accidents that occur at the intersection and improving the current sight distance to meet or exceed the sight distance requirements. The intersection must meet these goals while maintaining the operational efficiency at a LOS D or better in both the construction year and horizon year.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Elkhart Municipality: Elkhart

Limits of Proposed Work: Northern Terminus: 290' north; southern terminus: 600' south; eastern terminus: 530' east; western terminus: 410' west

Total Work Length: 0.13 Mile(s) Total Work Area: 3.31 Acre(s)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?

If yes, when did the FHWA grant a conditional approval for this project?

Yes¹

No

☐

☒

Date:

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

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Location

This project is located along CR 18, east of the City of Elkhart, at the intersections with CR 13 and CR 115, in Elkhart County, Concord Township, Section 14, Township 37-N, Range 5-E. A project location map is located in Appendix B-1 and USGS topographic map is located in Appendix B-2. Additional aerial photographs and ground level photographs are attached in Appendix B-4 through B-5. The project area location depicted in Appendix B-2 USGS project location extends farther east but has since been reduced to 530 ft east of the intersection.

Existing Conditions

The existing intersections are offset by approximately 130 ft. CR 13 is also skewed to the mainline roadway (CR 18).

- CR 18 is an east-west undivided roadway with 12-ft travel lanes with 0-to-1 ft gravel shoulders. CR 18 has a posted speed limit of 35 mph for both east and west approaches to the intersection. CR 18 is classified as minor arterial roadways on INDOT's functional classification map. The CR 18 west approach has a dedicated left-turn lane extending from the Elkhart River bridge to CR 13, and the east approach has one shared through/right-turn lane and one through lane. CR 18 is stop controlled with CR 13 and free at CR 115. CR 18 currently has a sight distance of 360 ft at this intersection.
- CR 13 is a north-south undivided roadway with 12-ft travel lanes with 1-ft paved shoulders and existing curbs. CR 13 has a posted speed limit of 30 mph. CR 13 is classified as minor arterial roadway on the INDOT functional classification map. CR 18 has a right turn lane, left turn lane, and a northbound through lane. CR 13 currently has a sight distance of 177 ft at this intersection.
- CR 115 and CR 13 are both north-south undivided roadways that form two T-intersections with CR 18. CR 115 and CR 13 are stop controlled at CR 18. CR 115 has 12-ft travel lanes with no roadway shoulder. The posted speed limit is 45 mph. CR 115 currently has a sight distance of 105 ft.
- A bridge located on CR 18 on the west side of the intersection, structure number 20-0156, a three-span prestressed concrete continuous beam structure spanning over Elkhart River which is approximately 220 ft long.
- There is a storm sewer outlet culvert on the east side of the intersection on the south side of CR 18, that is a 36" corrugated metal pipe (CMP).

This project area is set in a rural residential environment. The land adjacent to the intersection has been identified as wooded residential area with rolling topography.

Preferred Alternative

The proposed improvements include construction of an elongated, "peanut-shaped" roundabout. This alternative will provide a facility capable of handling the anticipated traffic growth as well as reducing the number of stop-controlled intersections along arterial corridors. Replacement of the storm sewer and outlet will occur. A retaining wall will be constructed on the southeast corner of the intersection. CR 115 will be realigned for maximum 600 ft to lessen horizontal curve to correct sight distance. Lighting improvements will also be included located along the roundabout approaches. The storm sewer drainage system will be replaced, and drainage will be diverted to the same location as before with a new 24" culvert.

The peanut roundabout proposes the conversion of the intersections CR 18, CR 13 and CR 115 into a four-leg, one-lane roundabout. This alternative will not require significant improvements to the adjacent bridge located over the Elkhart River, other than the planned guardrail updates and replacement of pavement markings that will extend over the bridge. The roundabout design offers many safety advantages, while also maintaining the efficiency of the intersection in the construction year and horizon year. The sight distances will be improved for CR 115 and CR 13. Due to the roadway realignment the sight distance on CR 115 will increase from the current 105 ft to 305 ft. CR 13 sight distance will increase from the current 177 ft to approximately 200 ft. CR 18 sight distance will remain the same at 360 ft. The proposed improvement will result in 2.68 acres of new permanent ROW and constriction activity will require an additional 0.77 acre of temporary ROW. See Appendix B-10 for ROW acquisition plan sheet.

Additional Information

The maintenance of traffic (MOT) for the project will require a full closure and detour. Westbound CR 18 traffic and northbound CR 13 traffic will be diverted on Sterling Ave to E Indiana Ave to Toledo Rd to CR 13. This detour is approximately eight miles long and will add four miles of additional travel. Eastbound CR 18 and southbound CR 115 traffic will be diverted to CR 17 to Toledo Ave, with eastbound CR 18 traffic additionally traveling from Toledo Rd to E Indiana Ave to south on Sterling Ave to CR 17 to CR 45. This detour is approximately 7.5 miles, adding approximately five miles of additional travel.

The preferred alternative has independent utility, meaning its intent is to correct safety and operational efficiency issues at the intersection and the project can function as stand-alone improvements without needing other improvements which may have additional impacts. Due to the project's limited scope as an intersection improvement, the project's logical termini are limited to the necessary approach work extending east, west, and south of the intersection as well as the minor improvements to the bridge north of the intersection.

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The preferred alternative meets the project's purpose and need by improving intersection safety through increased driver sight distance that meets or exceeds design standards while introducing intersection geometry that reduces vehicular conflict points within the intersection to reduce the number of crashes. The roundabout will also maintain the intersection's operational efficiency of a LOS D or better for construction year and horizon year traffic. See Appendix B-6 for design plans.

OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

Seven other alternatives were considered:

- 1. Unsignalized Intersection with CR 115 RIRO:** This alternative would not involve any major geometric improvements to the existing facility that would extend much beyond the existing edge of pavement. The possible changes include additional road signs or replacing the existing ones, installation of a median on CR 18 to eliminate the westbound left-turn movement onto CR 115, thereby converting CR 115 into a right-in-right-out (RIRO) intersection. By eliminating the westbound left turn movement from this intersection that would negatively impact residents that live on CR 115 south of the intersection. This alternative would not improve safety through increased sight distance at the intersection; therefore, does not meet the project's purpose and need.
- 2. Unsignalized Intersection with Realignment of CR 115:** This alternative involves realignment of CR 115 from the south to relocate one of the two existing intersections. CR 115 would be rerouted parallel to the US 20 Bypass, turning north and intersecting with CR 18 approximately 1,600 ft east of the current location. Although this alternative meets the purpose and need of the project it would require significantly more right-of-way to be acquired.
- 3. Roundabout with Two Intersections:** This alternative would involve the conversion of the intersection of CR 18 and CR 13, and CR 115 into a roundabout, which would allow continuous traffic flow through the intersection. At the CR 115 intersection, a raised center island would be installed to channelize traffic onto the CR 18 eastbound approach, resulting into a RIRO. The westbound left turn movement and southbound through movement would be handled in a similar manner as described in Alternative 1. Closing westbound access from CR 18 and CR 13 to CR 115, and from CR 115 onto CR 18 would significantly impact those who live on CR 115 south of the intersection. CR 18 is the only local crossing of the Elkhart River within 2.7 miles. Although this alternative meets the project purpose and need it was eliminated from consideration resulting from the access issue created for residents who live on CR 115 south of the intersection and because it would have a larger number of vehicular conflict points for potential crashes due to the additional intersection, thereby decreasing the safety improvement.
- 4. Consolidated Roundabout:** This alternative proposes the conversion of the intersection of CR 18, CR 13, and CR 115 into a four-leg, one-lane circular roundabout. This alternative would allow southbound through and westbound left turn traffic movements to go south on CR 115 since the CR 115 leg would be shifted east to be part of the roundabout. Although this alternative would meet the purpose and need of the project; it would require significantly more right-of-way acquisition from the property located in the south-east corner of the intersection which could have resulted in a possible relocation; therefore, this alternative was not selected.
- 5. Traffic Signal:** This alternative would involve the installation of a traffic signal at the intersection of CR 18 and CR 13. The eastbound approach would have an exclusive left turn lane. The southbound approach would have exclusive left and right turn lanes. The intersection of CR 18 with CR 115 would become a RIRO. The current safety issues resulting from poor intersection sight distance would remain with a traffic signal. Therefore, it would not meet the project's purpose and need.
- 6. Coordinated Traffic Signal:** This alternative involves the installation of one or more traffic signals which would be controlled by one signal controller. The northbound and southbound approaches would be split phased with eastbound and westbound approaches operating simultaneously. The current safety issues resulting from poor intersection sight distance would remain with traffic signal(s). Therefore, it would not meet the project's purpose and need.
- 7. Do Nothing:** The do nothing alternative would have involved the CR 18, CR 13, and CR 115 intersection remaining in its current state. The "Do Nothing" alternative would not address the overall purpose and need to increase the operational safety at the intersection while still maintaining, or improving, operational capacity for the projected traffic volume demand. Therefore, the "Do Nothing" alternative was not determined to meet the purpose and need and was not considered further.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

- It would not correct existing capacity deficiencies;
 It would not correct existing safety hazards;
 It would not correct the existing roadway geometric deficiencies;
 It would not correct existing deteriorated conditions and maintenance problems; or

X
X

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It would result in serious impacts to the motoring public and general welfare of the economy.
Other (Describe)

ROADWAY CHARACTER:

CR 18:

Functional Classification: Minor Arterial
 Current ADT: 10,506 VPD (2019) Design Year ADT: 10,934 VPD (2039)
 Design Hour Volume (DHV): 984 Truck Percentage (%) 0.5
 Designed Speed (mph): 35 Legal Speed (mph): 35

Existing

Proposed

Number of Lanes:	3		2	
Type of Lanes:	Vehicular- 1 EB, 1 WB through & and 1 left turn lane		Vehicular- 1 EB, 1 WB	
Pavement Width:	36	ft.	24	ft.
Shoulder Width:	0	ft.	1.5	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

CR 13:

Functional Classification: Minor Arterial
 Current ADT: 8,475 VPD (2019) Design Year ADT: 9,364 VPD (2039)
 Design Hour Volume (DHV): 843 Truck Percentage (%) 0.5
 Designed Speed (mph): 30 Legal Speed (mph): 30

Existing

Proposed

Number of Lanes:	3		2	
Type of Lanes:	Vehicular – 1 NB, 1 SB, L & R turn lane		Vehicular – 1 NB, 1 SB	
Pavement Width:	24	ft.	24	ft.
Shoulder Width:	1	ft.	1.5	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

CR 115 :

Functional Classification: Minor Collector
 Current ADT: 1,845 VPD (2019) Design Year ADT: 2,342 VPD (2039)
 Design Hour Volume (DHV): 211 Truck Percentage (%) 0.3
 Designed Speed (mph): 40 Legal Speed (mph): 45

Existing

Proposed

Number of Lanes:	2		2	
Type of Lanes:	Vehicular – 1 NB, 1 SB,		Vehicular – 1 NB, 1 SB	
Pavement Width:	24	ft.	24	ft.
Shoulder Width:	0	ft.	1.5	ft.
Median Width:	N/A	ft.	N/A	ft.

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Sidewalk Width: N/A ft. N/A ft.

Setting: ☐ Urban ☐ Suburban ☒ Rural
Topography: ☐ Level ☒ Rolling ☐ Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

DESIGN CRITERIA FOR BRIDGES:

Structure/NBI Number(s): 20-00156 Sufficiency Rating: 71.2, Bridge Inspection Report (Appendix I-7)
(Rating, Source of Information)

Existing

Proposed

Bridge Type:	Prestressed concrete continuous		Prestressed concrete continuous	
Number of Spans:	3		3	
Weight Restrictions:	Unknown	ton	Unknown	ton
Height Restrictions:	99.99	ft.	99.99	ft.
Curb to Curb Width:	39.6	ft.	39.6	ft.
Outside to Outside Width:	43	ft.	43	ft.
Shoulder Width:	0	ft.	0	ft.
Length of Channel Work:	0		0	

Describe bridges and structures; provide specific location information for small structures.

Remarks:

Presence

The affected structure is bridge number 20-00156. The 220-ft long bridge is a three span prestressed concrete continuous beam structure spanning over Elkhart River. This bridge was built in 1970 and was reconstructed in 1983; this bridge is not historic. The recommended scope of work includes overlaying the bridge approaches and replacing the pavement markers and replacing the guardrail at the western bridge approach. See appendix I-1 through I-19 for bridge inspection report.

The existing culvert outlet for the storm sewer system will be replaced with a 24" CMP culvert outlet for the storm water system at the same outlet location.

Will the structure be rehabilitated or replaced as part of the project? **Yes** **No** **N/A**
☒ ☐ ☐
 If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Remarks: The maintenance of traffic (MOT) for the project will require a full closure and detour. Westbound CR 18 traffic and northbound CR 13 traffic will be diverted on Sterling Ave to E Indiana Ave to Toledo Rd to CR 13. This detour is approximately eight miles long and will add four miles of additional travel. Eastbound CR 18 and southbound CR 115 traffic will be diverted to CR 17 to Toledo Ave, with eastbound CR 18 traffic additionally traveling from Toledo Rd to E Indiana Ave to south on Sterling Ave to CR 17 to CR 45. This detour is approximately 7.5 miles, adding approximately five miles of additional travel. The intersection closures will be in place for approximately one construction season.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences will cease upon project completion. Delays may occur during construction but will cease with project completion.

Access to all properties will be available through all phases of construction.

The project sponsor will be responsible for contacting school districts and emergency services at least 2 weeks prior to construction. Delays may occur during construction but will cease with project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 250,000 (2019) Right-of-Way: \$ 500,000 (2020) Construction: \$ 2,925,000 (2021)

Anticipated Start Date of Construction: Spring 2021

Date project incorporated into STIP July 2, 2019

Is the project in an MPO Area? ☒ **Yes** ☐ **No**

If yes,

Name of MPO Michiana Area Council of Governments (MACOG)

Location of Project in TIP 2020-2024 MACOG TIP (Appendix H-5)

Date of incorporation by reference into the STIP July 2, 2019

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.73	0.53
Commercial	0	0
Agricultural	0	0
Forest	0	0
Wetlands	0	0
Other: Kiwanis Club of Elkhart	0.14	0.12
Other:	0	0
TOTAL	0.87	0.65

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and there impacts on the environmental analysis should be discussed.

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Remarks: **Right-of-way (ROW) required**

The project requires approximately 0.87 acre of permanent right-of-way (ROW) to accommodate the proposed roundabout footprint. The permanent ROW consists of 0.73 acres from residential properties and 0.14 acre from the Kiwanis Club of Elkhart. The project also requires approximately 0.65 acre of temporary right-of-way for grading and construction activities. The temporary ROW consists of 0.53 acre of residential and 0.12 acre from the Kiwanis Club of Elkhart. This area lacks clear title of the ROW under the existing roadway footprint and a total of 2.41 acres of existing apparent ROW must be reacquired from the adjacent property owners.

The proposed typical ROW widths are 25 feet from center line and a maximum width of 95 feet. The existing typical ROW widths are 25 feet from the centerline and maximum ROW widths are 80 ft.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Streams, Rivers, Watercourses & Jurisdictional Ditches	x		x
Federal Wild and Scenic Rivers			
State Natural, Scenic or Recreational Rivers			
Nationwide Rivers Inventory (NRI) listed			
Outstanding Rivers List for Indiana			
Navigable Waterways			

Remarks: **Presence, no impact**

Based on a desktop review, a site visit on November 7, 2018 by Orbis Environmental Consulting (Orbis), the aerial map of the project area (Appendix B-3), and the water resource map in the Red Flag Investigation (RFI) report (Appendix E-8) there is one river present adjacent to the project area.

The Elkhart River runs along the west edge of the site, including under the bridge along CR 18. The Elkhart River is likely considered a jurisdictional 'Waters of the US'. Improvements to the bridge located over the Elkhart River will occur for planned guardrail updates and replacement of pavement markings that will extend over the bridge. However, the proposed improvements do not require construction below Elkhart River's Ordinary High Water Mark (OHWM). Therefore, no impacts are expected.

Waters Report

A *Waters of the U.S. Determination / Wetland Delineation Report* was completed for the project on October 25, 2019. Please refer to Appendix F-1 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that approximately 104 linear ft of the Elkhart River are present within the project area and will likely be considered jurisdictional. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

Early Coordination

Early coordination letters were sent on September 10, 2019 (Appendix C-1 through C-3). The Indiana Department of Natural Resources (IDNR DFW) responded to early coordination on October 10, 2019 and provided recommendations for controlling erosion and guidance to prevent sediment from entering a stream (Appendix C-21). The United States Fish and Wildlife Service (USFWS) responded on September 23, 2019 but did not provide any recommendations regarding stream or waterway information (Appendix C-16). The United States Army Corps of Engineers (USACE) responded to early coordination on October 15, 2019 and stated all impacts to any waterways will require prior authorization through

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their regulatory permit process (Appendix C-23). All applicable IDNR DFW, USFWS, and USACE recommendations are included in the Environmental Commitments section of this CE document.

Other Surface Waters

Reservoirs

Lakes

Farm Ponds

Detention Basins

Storm Water Management Facilities

Other: _____

Presence

Impacts

Yes	No

Remarks:

No presence, no impact

Based on a desktop review, a site visit on October 10, 2019 by Orbis, the aerial map of the project area (Appendix B-3), and the water resources map in the RFI report (Appendix E-8), there are thirteen lakes located within the 0.5 mile search radius. There are no other surface waters present within or adjacent to the project area.

The nearest lake segment is located 0.04 mile north of the project area. The Wetland Report completed on October 25, 2019 by Orbis (Appendix F) did not report surface waters in or adjacent to the project area. Therefore, no impacts are expected.

There is no practicable alternative to the proposed new construction in wetlands and the proposed action includes all practicable measures to minimize harm to wetlands which may result from such use. FHWA approval of this document will constitute approval of the adverse impacts to wetlands.

Waters Report

A *Waters of the U.S. Determination / Wetland Delineation Report* was completed for the project on October 25, 2019 by Orbis. Please refer to Appendix F for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that there are no other surface waters located adjacent or within the project area. The USACE makes all final determinations regarding jurisdiction.

Early Coordination

IDNR DFW responded to early coordination on October 10, 2019 and provided recommendation for erosion and sediment control (Appendix C-21). The USFWS responded on September 23, 2019 (Appendix C-16) and provided recommendations but did not include information regarding surface waters. All applicable IDNR DFW recommendations are included in the Environmental Commitments section of this CE document.

Wetlands

Presence

x

Impacts

Yes	No
x	

Total wetland area: 0.01 acre(s)

Total wetland area impacted: 0.01 acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

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Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments
W-001-PF0	Forested wetland	0.01	0.01	

Documentation

ES Approval Dates

Wetlands (Mark all that apply)

Wetland Determination

x

N/A (does not apply to non-INDOT sponsored projects)

Wetland Delineation

x

N/A (does not apply to non-INDOT sponsored projects)

USACE Isolated Waters Determination Mitigation Plan

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business, or other improved properties;

Substantially increased project costs;

Unique engineering, traffic, maintenance, or safety problems;

Substantial adverse social, economic, or environmental impacts, or

The project not meeting the identified needs.

x

x

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

Remarks:

Presence, with impacts less than one acre

Based on a review of the National Wetlands Inventory (NWI) online mapper (<https://www.fws.gov/wetlands/data/Mapper.html>), a site visit on October 10, 2019 by Orbis, the USGS topographic map (Appendix B-2), and the RFI report (Appendix E), there are sixteen wetlands located within the 0.5 mile search radius. There is one wetland present within or adjacent to the project area.

A wetland delineation was conducted and one forested wetland, W-001-PF0, was found and will likely be considered jurisdictional. Approximately 0.01 acre of W-001-PF0 is located within the project area. The USACE makes all final determinations regarding jurisdiction.

Impacting 0.01 acre of the wetland is anticipated as there is no practicable alternative to avoid the wetland and this proposed action includes all practicable measures to minimize harm to wetlands that may result.

Waters Report

A *Waters of the U.S. Determination / Wetland Delineation Report* was completed for the project on October 25, 2019. Please refer to Appendix F for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that W-001-PF0 is likely a jurisdictional feature. The USACE makes all final determinations regarding jurisdiction.

Early Coordination

IDNR DFW responded to early coordination on October 10, 2019 and provided recommendation regarding the impact and mitigation of wetlands (Appendix C-21). USFWS responded on September 23, 2019 (Appendix C-16) and provided recommendations to the adjacent Elkhart River corridor. USACE responded to early coordination on October 15, 2019 and suggested that all impacts to any jurisdictional wetlands would require prior authorization through their regulatory permit process (Appendix C-23). All applicable IDNR DFW, USFWS, and USACE recommendations are included in the Environmental Commitments section of this CE document.

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	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Terrestrial Habitat	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	
Unique or High Quality Habitat			

Use the remarks box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

Remarks: **Presence, with impacts**
 Based on a desktop review, a site visit on November 7, 2018 by Orbis, and aerial map of the project area (Appendix B-3), there are mature trees and shrubs within the project area. Land use surrounding the area consists largely of residential and wooded landscapes. There will be approximately 1.1 acres of tree removal that will take place for construction. The dominant tree species that are included in the tree removal are silver maple (*Acer saccharinum*), black walnut (*Juglans nigra*), white mulberry (*Morus alba*), and black locust (*Robinia pseudoacacia*). All work and tree removal or trimming will take place within 100 feet from the existing pavement. No trees will be removed from 100-300 ft from existing roadway. The total work area disturbance is anticipated to be 3.31 acre, exceeding one acre; therefore a Rule 5 permit is anticipated to be required. No mitigation is expected for this project.

Early Coordination
 USFWS responded on September 23, 2019 with tree removal and mitigation recommendations (Appendix C-23). IDNR DFW responded on October 10, 2019 regarding tree removal and erosion control measures (Appendix C-21). All applicable USFWS and IDNR DFW recommendations are included in the Environmental Commitments section of this CE document.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

Karst	<u>Yes</u>	<u>No</u>
Is the proposed project located within or adjacent to the potential Karst Area of Indiana?		<input checked="" type="checkbox"/>
Are karst features located within or adjacent to the footprint of the proposed project?		<input checked="" type="checkbox"/>
If yes, will the project impact any of these karst features?		

Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks: **Outside karst area**
 Based on a desktop review, the project is located outside the designated karst region of Indiana as outlined in the October 13, 1993 Memorandum of Understanding (MOU). According to the topo map of the project area (Appendix B-2), and the RFI report (Appendix E) there are no karst features identified within or adjacent to the project area. In the early coordination response, the Indiana Geological Survey (IGS) did not indicate that karst features exist in the project area (Appendix C-5). The IGS response did indicate that the project area has moderate liquefaction potential, and is in a floodway. Response from IGS has been communicated with the designer on September 10, 2019. No impacts are expected.

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Threatened or Endangered Species	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	
Within the known range of any federal species			
Any critical habitat identified within project area			
Federal species found in project area (based upon informal consultation)			
State species found in project area (based upon consultation with IDNR)			
Is Section 7 formal consultation required for this action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

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Remarks: Based on a desktop review and the RFI report (Appendix E), completed by Troyer Group on August 21, 2019, the IDNR Elkhart County Endangered, Threatened, and Rare (ETR) Species List has been checked and is included in (Appendix E-11). The highlighted species on the list reflect the federal and state identified ETR species located within the county. According to the IDNR-DFW early coordination response letter dated October 10, 2019 (Appendix C-21), the Natural Heritage Program's Database has been checked and the state endangered Greater Redhorse (*Moxostoma valenciennesi*) has been documented in the Elkhart River within 0.5 mile of the project area. According to the USFWS early coordination response dated September 23, 2019, the project is also within range of the Eastern Massasauga rattlesnake (*Sistrurus catenatus*). However, no habitat for the Eastern Massasauga is within the proposed project area, and the USFWS agrees the project is not likely to adversely affect this threatened species (Appendix C-16).

Indiana Bat and Northern Long-Eared Bat
Bats, Programmatic Informal Consultation – Not Likely to Adversely Affect

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C-27). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*).

The project qualifies for the Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB), dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on April 20, 2019 and based on the responses provided, the project was found to "Not Likely to Adversely Affect" the Indiana bat and/or the NLEB. INDOT reviewed and verified the effect finding on April 20, 2020 and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding (Appendix C-33). Avoidance and Mitigation Measures (AMMs) are included as firm commitments in the Environmental Commitments section of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

SECTION B – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area
 Public Water System(s)
 Residential Well(s)
 Source Water Protection Area(s)
 Sole Source Aquifer (SSA)

Presence

X
X
X

Impacts

Yes	No
X	
	X
	X

If a SSA is present, answer the following:

Is the Project in the St. Joseph Aquifer System?
 Is the FHWA/EPA SSA MOU Applicable?
 Initial Groundwater Assessment Required?
 Detailed Groundwater Assessment Required?

Yes	No
X	
X	
	X
	X

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Remarks:

Sole Source Aquifer
Inside SSA
 The Environmental Protection Agency's Sole Source Aquifer website (<https://www.epa.gov/dwssa>) was accessed on April 23, 2019 by Troyer Group. The project is located in Elkhart County, parts of which are located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. The FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is applicable to this project. The United States Environmental Protection Agency (US EPA) responded to early coordination on October 21, 2019 (Appendix C-26) which stated that the project is not likely to contaminate the St. Joseph Sole Source Aquifer.

EPA provided recommendation to ensure that local ground water supplies and neighboring drinking water wells are not endangered. All applicable recommendations are included in the Environmental Commitments section of this CE document.

Wellhead Protection Area and Source Water
Not located in a Wellhead Protection Area or Source Water Area
 The IDEM map for wellhead proximity (<https://www.in.gov/idem/cleanwater/pages/wellhead/>) has been checked and the project is not located in a Wellhead Protection Area. Therefore, no impact is expected.

Water Wells
 The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on January 28, 2019 by Troyer Group. The nearest well is located approximately 0.025 mile south of the project area. The features will not be affected because the project will not involve earth disturbance work in that area. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that these wells are affected, a cost to cure will likely be included in the appraisal to restore the wells.

Urban Area Boundary
In an Urban Area Boundary Location
 Based on a desktop review of the INDOR MS4 website (<https://entapps.indot.in.gov/MS4>) by Troyer Group on September 26, 2019, and the RFI report; this project is located in an Urban Area Boundary (UAB) location. An early coordination letter was sent on September 10, 2019, to the Elkhart County MS4 Coordinator (Appendix C-4). The MS4 coordinator did not respond within the 30-day timeframe.

Public Water System
 Based on a desktop review, a site visit on October 31, 2019 by Troyer Group, and the aerial map of the project area (Appendix B-3) this project is located where there is a public water system. Elkhart Public Utilities has been coordinated with regarding the proposed location and elevation of the site excavation activities and the water line is in conflict and will most likely require relocation prior to construction.

Flood Plains	Presence	Impacts	
		Yes	No
Longitudinal Encroachment	x	x	
Transverse Encroachment			
Project located within a regulated floodplain	x	x	
Homes located in floodplain within 1000' up/downstream from project	x		x

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks:

In floodplain
 Based on a desktop review of The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<https://dnrmmaps.dnr.in.gov/appsphp/fdms/>) by Troyer Group on January 28, 2019, and the RFI report; this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F-29). An early coordination letter was sent on September 10, 2019, to the local Floodplain Administrator. The floodplain administrator did not respond within the 30-day time frame. This project qualifies as a Category 3 per the current INDOT CE Manual, which states:

Category 3- The modifications to drainage structures included in this project will result in an insubstantial change in their

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capacity to carry flood water. This change could cause a minimal increase in flood heights and flood limits. These minimal increases will not result in any substantial adverse impacts on the natural and beneficial floodplain values; they will not result in substantial change in flood risks or damage; and they do not have substantial potential for interruption or termination of emergency service or emergency routes; therefore, it has been determined that this encroachment is not substantial.

A Construction in a floodplain permit is anticipated for this project. Formal coordination with IDNR will occur as part of this project. Detailed plans will be sent to IDNR with permit application regarding the project impacts prior to construction.

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Farmland			
Agricultural Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006*
**If 160 or greater, see CE Manual for guidance.*

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

Remarks: **No presence, no impact**
 Based on a desktop review, a site visit on October 31, 2019 by Troyer Group, the aerial map of the project area (Appendix B-3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on September 10, 2019 to the Natural Resources Conservation Services (NRCS). According to the NRCS response, dated September 30, 2019 the project will not cause a conversion of prime farmland (Appendix C-20).

SECTION C – CULTURAL RESOURCES

	Category	Type	INDOT Approval Dates	N/A
Minor Projects PA Clearance				X

Eligible and/or Listed Resource Present

Results of Research

Archaeology	<input type="checkbox"/>
NRHP Buildings/Site(s)	<input type="checkbox"/>
NRHP District(s)	<input type="checkbox"/>
NRHP Bridge(s)	<input type="checkbox"/>

Project Effect

No Historic Properties Affected ☒ No Adverse Effect ☐ Adverse Effect ☐

Documentation Prepared

Documentation (mark all that apply)

		ES/FHWA Approval Date(s)	SHPO Approval Date(s)
Historic Properties Short Report	<input type="checkbox"/>		
Historic Property Report	x	1/22/2020	3/2/2020
Archaeological Records Check/ Review	<input type="checkbox"/>		
Archaeological Phase Ia Survey Report	x	1/22/2020	3/2/2020
Archaeological Phase Ic Survey Report	<input type="checkbox"/>		

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Archaeological Phase II Investigation Report	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase III Data Recovery	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
APE, Eligibility and Effect Determination	<input checked="" type="checkbox"/>	4/24/2020	5/11/2020
800.11 Documentation	<input checked="" type="checkbox"/>	4/24/2020	5/11/2020

Memorandum of Agreement (MOA) ☐ **MOA Signature Dates** (List all signatories)

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

Remarks: **Area of Potential Effect (APE):**
The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE consists of an irregularly shaped area surrounding the intersection of CR 18 at CR 13/CR 115. The width of the APE is generally one property deep. The APE was determined by adjacent properties to the proposed construction limits and line of sight to and from the proposed construction project limits.

Coordination with Consulting Parties:
INDOT and The Troyer Group invited twelve consulting parties, listed below, (Appendix D-25 through D-27) as part of the Section 106 early coordination issued on April 16, 2020. INDOT is acting on behalf of FHWA and Indiana State Historic Preservation Office (SHPO) is considered an automatic consulting party.

1. Eastern Shawnee Tribe of Oklahoma
2. Elkhart County, Indiana Commissioners
3. Elkhart County, Indiana County Council
4. Elkhart County, Indiana Highway Department
5. Elkhart County, Indiana Historian
6. Forest County Potawatomi Community
7. Indiana Landmarks, Northern Regional Office
8. Miami Tribe of Oklahoma
9. Michiana Area Council of Governments
10. Peoria Tribe of Indians of Oklahoma
11. Pokagon Band of Potawatomi Indians

Affirmative responses were received from the following: IN SHPO, the Forest County Potawatomi Community, the Miami Tribe of Oklahoma, and the Pokagon Band of Potawatomi Indians accepted the invitation to be a consulting party while the Indiana Landmarks-Northern Regional Office responded but declined (Appendix D-24).

The Pokagon Band of Potawatomi Indians responded to the Early Coordination Letter (ECL) on April 24, 2019 issuing a No Adverse Effect determination on any historic, religious, or culturally significant resources to the Pokagon Band of Potawatomi Indians. However, the tribe requested that work stop and the Tribal Historic Preservation Office (THPO) be contacted immediately if archaeological sites are uncovered during the investigation (Appendix D-32).

The IN SHPO responded to the ECL on May 9, 2019 concurring with the list of consulting parties (Appendix D-36 through D-37).

The Forest County Potawatomi Community responded to the ECL on May 14, 2019 via email stating they determined this area is of interest and requires a copy of the archaeological report and SHPO comments related to the project (Appendix D-33).

The Miami Tribe responded to the ECL on May 14, 2019 offering no objection and are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project area (Appendix D-34).

The following individuals and organizations were invited by letter dated June 7, 2019 to review and

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respond to the Historic Property Short Report (HPSR) and Archaeology Short Report (Tribes only):

1. Indiana State Historic Preservation Officer (IN SHPO)
2. Forest County Potawatomi Community
3. Pokagon Band of Potawatomi Indians

The above-listed parties have been provided with access to copies of the HPSR and the Archaeological Short Report (Tribes only). Of the above-listed parties the IN SHPO, Forest County Potawatomi Community, and the Pokagon Band of Potawatomi Indians have responded (Appendix D-23).

The Pokagon Band of Potawatomi Indians responded in a letter dated June 13, 2019 providing a No Adverse Effect determination on any historic, religious, or culturally significant resources to the Pokagon Band of Potawatomi Indians. However, the tribe requested that work stop and the Tribal Historic Preservation Office (THPO) be contacted immediately if archaeological sites are uncovered during the investigation (Appendix D-43).

IN SHPO, in a letter dated July 9, 2019, responded determining No Historic Properties Affected concur with the No Historic Properties Affected finding and agrees that the "peanut shaped" roundabout contains no above-ground properties listed in or eligible for inclusion in the National Register of Historic Places ("NRHP"). SHPO spoke to archeological concerns in this same letter concurring with the no further archaeological investigations are necessary at the project area. However, that is for the "peanut-shaped" roundabout; if another alternative is selected it may result in a different APE depending on the size of scope (Appendix D-44 through D-45). Orbis consulting responded to IN SHPO in an email dated July 23, 2019 describing that the peanut shape roundabout is in fact the preferred alternative at this time (Appendix D-46).

The Forest County Potawatomi Community response letter dated July 16, 2019 concurring with the No Historic Properties Affected under two conditions. First, the tribe reserved the right to reconsider based on new evidence; second, in the event that human remains or archaeological materials are exposed as a result of the project, the work will halt and the Tribe will be contacted prior to its removal (Appendix D-44).

The following individuals and organizations were invited by letter dated January 27, 2020 (Appendix D-48 through D-50) to review and respond to the Historic Property Short Report Addendum (HPSR) and an Archaeology Short Report (Tribes only):

- Indiana State Historic Preservation Officer (IN SHPO)
- Forest County Potawatomi Community
- Pokagon Band of Potawatomi Indians

The above-listed parties have been provided with access to copies of the HPSR and the Archaeological Report (Tribes only). Of the above-listed parties, IN SHPO and the Pokagon Band of Potawatomi Indians have responded.

The Pokagon Band of Potawatomi Indians, in a letter dated January 28, 2020 responded that this work is within one mile of a historic place for the Tribe and they concur with the No Adverse Effect on any historic, religious, or culturally significant resources to the Pokagon Band of Potawatomi Indians. (Appendix D-62)

IN SHPO responded in a letter dated March 2, 2020 concurring with the No Historic Properties Affected and with the concurrence that no further archaeological investigation is required (Appendix D-60 through D-62).

No other consultant party comments were received.

Archaeology:

A Phase Ia Archaeological Field Reconnaissance was conducted on November 14, 2018 and an accompanying Archaeological Report was completed on May 23, 2019. The Phase Ia archaeological investigation revealed no previously unidentified archaeological sites, based on historic background resources and no artifacts discovered during the field visit. The project area contained no sites eligible for listing in the NPHR and no further archaeological work was recommended (Appendix D-14).

Historic Properties:

A Historic Properties Report (HPR) was prepared by Orbis Consulting in June 2019. The National Register of Historic Places (NRHP) and Indiana Register of Historic Sites and Structures (State Register) were checked and the APE contains no properties listed in the National Register and no properties that are recommended or eligible for listing in the

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National Register (Appendix D-16).

Documentation, Findings:

A “No Historic Properties Affected” finding was approved by INDOT CRO, acting on behalf of FHWA, on January 27, 2020 (Appendix D-59). SHPO concurred with the finding of “No Historic Properties Affected” in a letter dated March 2, 2020 (Appendix D-60 through D-61).

Public Involvement:

To meet the public involvement requirements of Section 106, a notice of the Federal Highway Administration’s (FHWA’s) finding of “No Historic Properties Affected” was advertised in *The Goshen News* newspaper on May 6, 2020. A 30-day comment period was given. No comments were received from the public. The legal notice’s proof of publication and text are located in Appendix D-69.

SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

Parks & Other Recreational Land

Publicly owned park
Publicly owned recreation area
Other (school, state/national forest, bikeway, etc.)

Presence

Use

Yes	No

**Evaluations
Prepared**

Programmatic Section 4(f)*
“De minimis” Impact*
Individual Section 4(f)

**FHWA
Approval date**

--

Wildlife & Waterfowl Refuges

National Wildlife Refuge
National Natural Landmark
State Wildlife Area
State Nature Preserve

Presence

Use

Yes	No

**Evaluations
Prepared**

Programmatic Section 4(f)*
“De minimis” Impact*
Individual Section 4(f)

**FHWA
Approval date**

--

Historic Properties

Sites eligible and/or listed on the NRHP

Presence

--

Use

Yes	No

**Evaluations
Prepared**

Programmatic Section 4(f)*
“De minimis” Impact*
Individual Section 4(f)

**FHWA
Approval date**

--

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**FHWA approval of the environmental document also serves as approval of any Section 4f Programmatic and/or De minimis evaluation(s) discussed below.*

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, "de minimis" and Individual Section 4(f) evaluations please refer to the "Procedural Manual for the Preparation of Environmental Studies". Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:

No presence, no impact

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

A Historic Properties Report (HPR) was prepared by Orbis Consulting in June 2019. The National Register of Historic Places (NRHP) and Indiana Register of Historic Sites and Structures (State Register) were checked and the APE contains no properties listed in the National Register and no properties that are recommended or eligible for listing in the National Register. Therefore, no historic properties considered Section 4(f) resources are present within or adjacent to this project.

Based on a desktop review, a site visit on October 31, 2018 by Troyer Group, the aerial map of the project area (Appendix B-3), and the RFI report (Appendix E) there is one 4(f) resource located within the 0.5 mile search radius. The Concord East Side Elementary School is located 0.29 mile north of the project, but will not be impacted by the proposed improvements. There are no Section 4(f) resources within or adjacent to the project area. Therefore, no use is expected.

Section 6(f) Involvement

Presence

Use

Yes

No

Section 6(f) Property

☐☐☒

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

No presence no impact

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conservation of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the Land and Water Conservation Fund (LWCF) website at <https://www.lwcfcoalition.com/tools> revealed a total of seventeen properties in Elkhart County (Appendix I-22). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources as a result of this project.

SECTION E – Air Quality

Air Quality

Conformity Status of the Project

Is the project in an air quality non-attainment or maintenance area?

Yes
x

No

If YES, then:

Is the project in the most current MPO TIP?

x

Is the project exempt from conformity?

x

If the project is NOT exempt from conformity, then:

Is the project in the Transportation Plan (TP)?

Is a hot spot analysis required (CO/PM)?

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Level of MSAT Analysis required?

Level 1a ☒ Level 1b ☐ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Remarks:

STIP/TIP

This project is included in the Fiscal Year (FY) 2020-2024 Michiana Area Council of Governments (MACOG) Metropolitan Planning Organization Transportation Improvement Program (MPO TIP) and 2020-2024 Statewide Transportation Improvement Program (STIP) (Appendix H-3 and H-5).

Attainment Status

Maintenance area

This project is located in Elkhart County, which is currently a maintenance area for 1997 ozone standard pollutants according to https://www.in.gov/idem/airquality/files/nonattainment_county_list.pdf. Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

MSAT Level 1a Analysis

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CR 771.117 (c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such a Mobile Source Air Toxics analysis is not required.

SECTION F - NOISE

Noise

Yes

No

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

☐☒

No

Yes/ Date

ES Review of Noise Analysis

☐☐

Remarks:

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?

Yes

No

☒☐

Will the proposed action result in substantial impacts to community cohesion?

☐☒

Will the proposed action result in substantial impacts to local tax base or property values?

☐☒

Will construction activities impact community events (festivals, fairs, etc.)?

☐☒

Does the community have an approved transition plan?

☒☐

If No, are steps being made to advance the community's transition plan?

☐☐

Does the project comply with the transition plan? (explain in the remarks box)

☒☐

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Remarks: The proposed intersection improvement will not impact development patterns in the area. There will be no negative impacts to community cohesion, the local tax base, or property values. The construction is not anticipated to impact any community events such as festivals or fairs.

Elkhart County has an approved ADA transition plan, which is viewable online at <http://www.elkcohw.org/resources/ada/ada-transition-plan/>. There are no existing pedestrian facilities within the project limits and the purpose and need of the project does not include pedestrian facilities; furthermore, this project will not affect any area identified as priorities for improvement. Therefore, the project is considered in compliance with the county's ADA transition plan.

An event calendar was viewed on Elkhart Counties website <https://www.visitelkhartcounty.com/events/>. No community events are planned for the immediate project area during the time of construction. Therefore, impacts to the area expected to be minimal. However, adjacent to the project area is a Boy Scout Kiwanis Cabin, and according to their online meeting schedule found [https://www.kiwanisclubofelkhart.org/\(S\(Q35QMN55ULNRSHJFESWR3Y55\)\)/Page/18101](https://www.kiwanisclubofelkhart.org/(S(Q35QMN55ULNRSHJFESWR3Y55))/Page/18101) this club will be still operating during construction. Access to this property will remain open for these events.

The purpose of the project is to increase community safety by reducing the number of vehicle accidents at the site of the intersection, thereby benefiting anyone who drives through this intersection. It will not change existing traffic patterns in any substantial way.

Indirect and Cumulative Impacts

Will the proposed action result in substantial indirect or cumulative impacts?

Yes No
☐ ☒

Remarks: Indirect impacts are effects which are caused by the action and are later in time for farther removed in distance but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such actions.

This project will not add capacity to the roadway, nor will it cause any substantial change to the surrounding properties. Therefore, no significant indirect or cumulative impacts will result from the project.

Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? *Discuss how the maintenance of traffic will affect public facilities and services.*

Yes No
☐ ☒

Remarks: **Presence with impacts**

Based on a desktop review, a site visit on October 31, 2019 by Troyer Group, the aerial map of the project area (Appendix B-3), and the RFI report (Appendix E), there is one cemetery, one school, and one recreation facility located within the 0.5 mile of the project.

Concord Township Fire Department is located 0.21 miles west of the project area but is on the west side of the Elkhart River. During construction, the bridge will be closed and no traffic from west of the river will be able to travel through the intersection. The corresponding MOT will impact emergency response times. Concord Township Fire Department was sent an early coordination letter on September 10, 2019 but did not respond. Representatives of the Fire Department attended the Public Information meeting on September 12, 2019 and conveyed their concerns about the delayed response time during construction. Elkhart County Highway Department representatives discussed the measures implemented when the CR 18 bridge over the Elkhart River was recently closed for rehabilitation. A temporary staging area east of the bridge at Ivy Tech Community College was established and ensured minimal response time to emergencies occurring east of the Elkhart River. Elkhart County Highway Department will coordinate similar accommodations during this project's construction.

An early coordination response from INDOT ESD was received on September 30, 2019 that referenced several nearby projects. Coordination has occurred with the listed INDOT PM's for each project. The projects will not pose any additional impacts to this project.

Additional coordination among the designer, Elkhart County Highway, and Concord Township Fire Department will

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occur in advance of construction so delays in emergency response time are minimized to the greatest extent possible. This is a firm project commitment.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high or disproportionate impacts to EJ populations?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

EJ Analysis, EJ Populations

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require 3.45 acres of ROW. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Elkhart County. The community that overlaps the project area is called the affected community (AC). In this project, the AC is Census Tracts 19.01, 19.02, and 20. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data was obtained from the US Census Bureau Website <https://data.census.gov/cedsci/> on June 17, 2020 by Troyer Group. The data collected for minority and low-income populations within the AC are summarized in the below table.

Table: Minority and Low-Income Data (Source Data and Year)				
	COC - Elkhart County	AC-1 – Census Tract 19.01, Elkhart County, Indiana	AC-2 – Census Tract 19.02, Elkhart County, Indiana	AC-3 – Census Tract 20, Elkhart County, Indiana
Percent Minority	24.93%	35.08%	23.33%	18.72%
125% of COC	31.16%	AC > 125% COC	AC < 125% COC	AC < 125% COC
EJ Population of Concern		Yes	No	No
Percent Low-Income	13.34%	29.68%	5.90%	14.15%
125% of COC	16.69%	AC > 125% COC	AC < 125% COC	AC < 125% COC
EJ Population of Concern		Yes	No	No

*Refer to the INDOT EJ guidance for calculating percentages

AC-1, Census Tract 19.01 has a percent minority of 35.08% which is below 50% but is above the 125% COC threshold. AC-2, Census Tract 19.02 has a percent minority of 23.33% and is below the 125% COC threshold. AC-3, Census Tract 20 has a percent minority of 18.72% and is below the 125% COC threshold. Therefore, AC-1, contains a minority population of EJ concern and both AC-2 and AC-3, do not contain minority populations of EJ concern.

AC-1, Census Tract 19.01 has a percent low-income of 29.68% which is below 50% but is above the 125% COC threshold. AC-2, Census Tract 19.02 has a percent low-income of 5.90% which is below 50% and is below the 125% COC threshold. AC-3 Census Tract 20 has a percent low-income that is 14.15% which is below 50% and is below the 125% COC threshold. Therefore, AC-1, contains a low-income population of EJ concern and both AC-2 and AC-3, do not contain low-income populations of EJ concern.

Conclusion

Potential impacts to EJ populations of concern are most likely to be the result of ROW acquisition and maintenance of traffic. 3.45 acres of permanent ROW acquisition will be required for this project. All ROW is being acquired from

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residential properties. The right of way that is being acquired from the parcels in AC-1 Census Tract 19.01 will be from one parcel that consists of a total ROW amount of 0.12 acre. The MOT for the project will require a full closure and detour. Westbound CR 18 traffic and northbound CR 13 traffic will be diverted on Sterling Ave to E Indiana Ave to Toledo Rd to CR 13. This detour is approximately eight miles long and will add four miles of additional travel. Eastbound CR 18 and southbound CR 115 traffic will be diverted to CR 17 to Toledo Ave, with eastbound CR 18 traffic additionally traveling from Toledo Rd to E Indiana Ave to south on Sterling Ave to CR 17 to CR 45. This detour is approximately 7.5 miles, adding approximately five miles of additional travel. The intersection closures will be in place for approximately one construction season.

No impacts to public facilities or community cohesion are expected, and there will be no relocations for the project.

An email was sent to INDOT ESD on June 17, 2020 requesting their comments on these conclusions. Their response, dated July 17, 2020, stated that "INDOT-Environmental Services Division (ESD) has reviewed the project information along with the Environmental Justice (EJ) Analysis for the above referenced project. With the information provided, the project may require minimal right-of-way, require no relocations, and would not disrupt community cohesion or create a physical barrier. With the information provided, INDOT-ESD would not consider the impacts associated with this project as causing a disproportionately high and adverse effect on minority and/or low income populations of EJ concern relative to non EJ populations in accordance with the provisions of Executive Order 12898 and FHWA Order 6640.23a. No further EJ Analysis is required." See Appendix I-27 for INDOT ESD Coordination.

Since the project impacts are related to ROW acquisition and the MOT and both are considered minimal and largely temporary in nature, the identified EJ populations of concern are not anticipated to experience disproportionately high and adverse impacts as a result of this project.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

Is a Business Information Survey (BIS) required?

Is a Conceptual Stage Relocation Study (CSRS) required?

Has utility relocation coordination been initiated for this project?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks:

No Relocations

No relocations of people, businesses, or farms will take place as a result of this project.

Some utilities will require relocation prior to commencement of construction. Utility coordination is ongoing and relocation plans will be developed to ensure conflicts are kept to a minimum while replacing the utilities in appropriate locations within the existing and proposed permanent ROW.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Documentation

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

No Yes/ Date

ES Review of Investigations	x	
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Include a summary of findings for each investigation.

This is page 24 of 27 Project name: CR 18 at CR 13/CR 115, Intersection Improvement Date: January 14, 2021

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Remarks: **Presence, no impact**
Based on a review of GIS and available public records, a RFI was completed on August 21, 2019 by Troyer Group (Appendix E). One Underground Storage Tank (UST) is located within 0.5 mile of the project area; however, no hazmat sites were identified in or within 0.5 mile of the project area that will impact the project area. The nearest UST is 0.37 mile southwest from the project area. No impacts are expected. Further investigation for hazardous material concerns is not required at this time.

SECTION I – PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Individual Permit (IP)	<input type="checkbox"/>
Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	<input checked="" type="checkbox"/>
Pre-Construction Notification (PCN)	<input type="checkbox"/>
Other	<input type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>

IDEM

Section 401 WQC	<input checked="" type="checkbox"/>
Isolated Wetlands determination	<input type="checkbox"/>
Rule 5	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>

IDNR

Construction in a Floodway	<input checked="" type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>
Lake Preservation Permit	<input type="checkbox"/>
Other	<input type="checkbox"/>
Mitigation Required	<input type="checkbox"/>

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the remarks box below)

Remarks: Due to the storm sewer outlet replacement being within Elkhart River's regulatory floodway, a Construction in a Floodway (CIF) permit from IDNR is anticipated. Due to the minimal change anticipated in the cross-section flow area, a non-modeling assessment approach will likely apply.

Due to the anticipated wetland impacts for the storm sewer outlet replacement 401/404 RGP's are anticipated.

An IDEM Rule 5 permit is anticipated due to more than one acre of soil disturbance required.

If changes to project scope or right-of-way become necessary, permit requirements will be re-evaluated.

Applicable recommendations provided are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

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SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s), and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks:

FIRM:

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. Additional coordination among the designer, Elkhart County Highway Department, and Concord Township Fire Department will occur in advance of construction so delays in emergency response time are minimized to the greatest extent possible. (INDOT ESD)
4. During construction and maintenance, it is suggested that appropriate safeguards and best management practices are in place to ensure that local ground water supplies and neighboring drinking water wells are not endangered. Such precautions could include notifying general contractors that the site is sensitive, using "green infrastructure" practices wherever possible to reduce potential impacts of stormwater run-off, securing adequate precautions for fueling/servicing large equipment, and developing contingency plans to handle the release of any hazardous materials. (US EPA)
5. GENERAL AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMS. (USFWS)
6. LIGHTING AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
7. LIGHTING AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable. (USFWS)
8. TREE REMOVAL AMM 1: Modify all phases/aspect of the project (e.g. temporary work areas, alignments) to avoid tree removal. (USFWS)
9. TREE REMOVAL AMM 2: Apply time of year restrictions during the bat active season, from April 1 through September 30, for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS and IDNR DFW)
10. TREE REMOVAL AMM 3: Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g. install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
11. TREE REMOVAL AMM 4: Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year. (USFWS)
12. The maintenance of traffic plan will require unique special provisions to ensure one-way traffic access is maintained through the construction zone, which intends to accommodate trips by school buses and emergency vehicles. (INDOT ESD)

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SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks: Early coordination letters were sent to nineteen organizations and agencies on September 10, 2019 (response dates listed next to each agency):

- IDEM, 9/11/2019
- IGS, 9/10/2019
- IDNR DFW, 10/10/2019
- USFWS, 9/23/2019
- Army Corps of Engineers – Detroit District, 10/15/2019
- INDOT, Environmental Services Division, 10/30/2019
- INDOT, Public Involvement Office, 9/11/2019
- USDA, Natural Resource Conservation Service, 9/30/2019
- US Department of Housing & Urban Development, no response
- National Park Service, no response
- USEPA, Sole Source Aquifer Coordinator, 10/21/2019
- Michiana Area Council of Governments, no response
- Elkhart County Surveyor, no response
- Elkhart County Highway Department, no response
- Elkhart County Board of Commissioners, no response
- Elkhart County Council, no response
- Concord Township Fire Department, no response
- Concord Community Schools, no response

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APPENDIX A

CE Threshold Chart

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	"No Historic Properties Affected"	"No Adverse Effect"	-	"Adverse Effect" Or Historic Bridge involvement ²
Stream Impacts	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	Individual 404 Permit
Wetland Impacts	No adverse impacts to wetlands	< 0.1 acre	-	< 1 acre	≥ 1 acre
Right-of-way³	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)	"No Effect", "Not likely to Adversely Affect" (Without AMMs ⁴ or with AMMs required for all projects ⁵)	"Not likely to Adversely Affect" (With any other AMMs)	-	"Likely to Adversely Affect"	Project does not fall under Species Specific Programmatic
Threatened/Endangered Species (Any other species)	Falls within guidelines of USFWS 2013 Interim Policy	"No Effect", "Not likely to Adversely Affect"	-	-	"Likely to Adversely Affect"
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁶
Sole Source Aquifer	Detailed Assessment Not Required	-	-	-	Detailed Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Coastal Zone Consistency	Consistent	-	-	-	Not Consistent
National Wild and Scenic River	Not Present	-	-	-	Present
New Alignment	None	-	-	-	Any
Section 4(f) Impacts	None	-	-	-	Any
Section 6(f) Impacts	None	-	-	-	Any
Added Through Lane	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Coast Guard Permit	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ⁷
Approval Level	Concurrence by INDOT District Environmental or Environmental Services	Yes	Yes	Yes Yes	Yes Yes Yes

¹Coordinate with INDOT Environmental Services. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

²Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³Permanent and/or temporary right-of-way.

⁴AMMs = Avoidance and Mitigation Measures.

⁵AMMs determined by the IPAC decision key to be needed that are listed in the USFWS *User's Guide for the Range-wide Programmatic Consultation for Indiana bat and Northern long-eared bat* as "required for all projects".

⁶Potential for causing a disproportionately high and adverse impact.

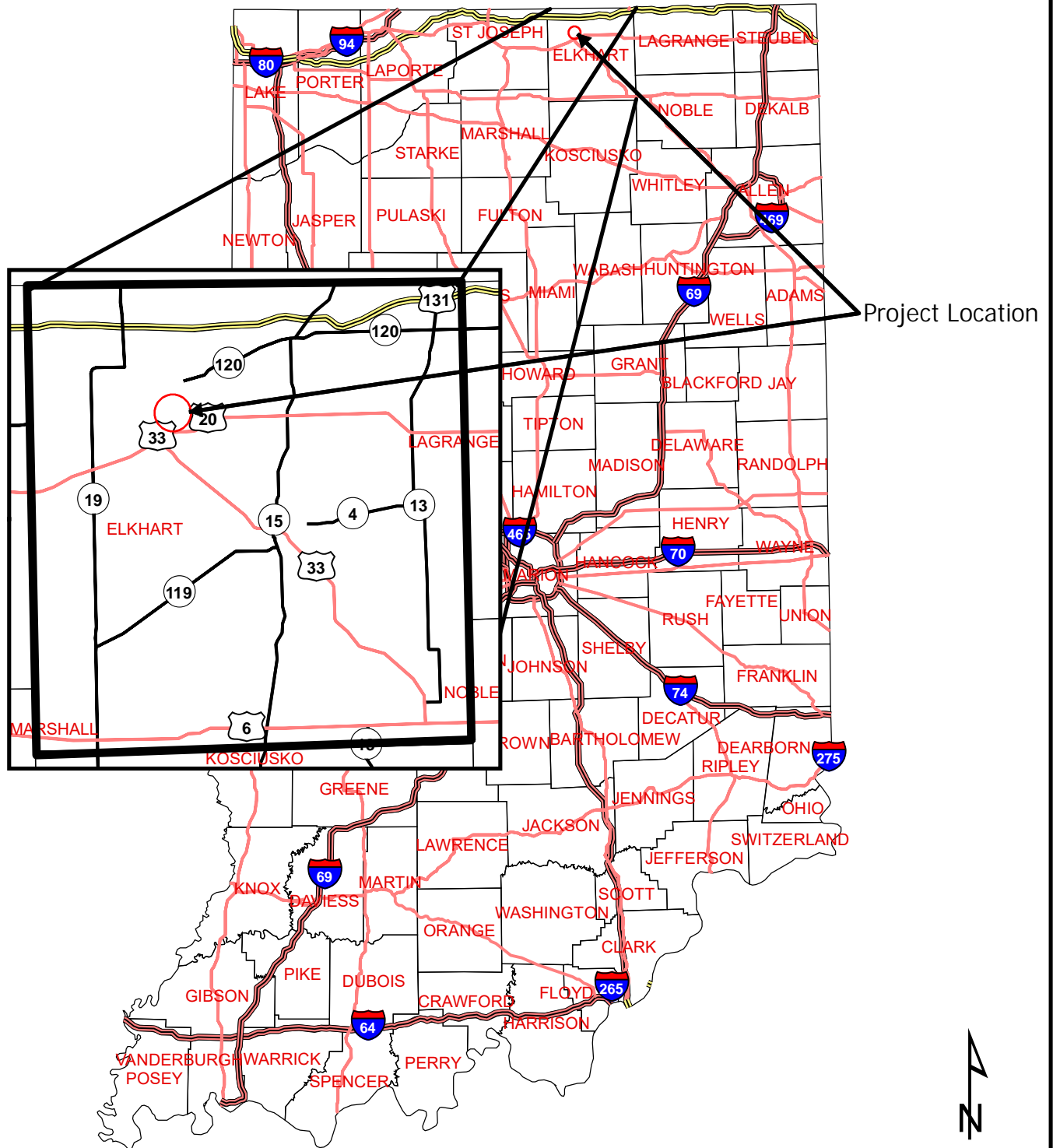
⁷Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

*Substantial public or agency controversy may require a higher-level NEPA document.

APPENDIX B

Graphics

Project Location



PROJECT

PCR 18 at CR 13/ CR 115
 Intersection Improvement, Roundabout
 (Des. No. 1401749)

Elkhart County, Indiana

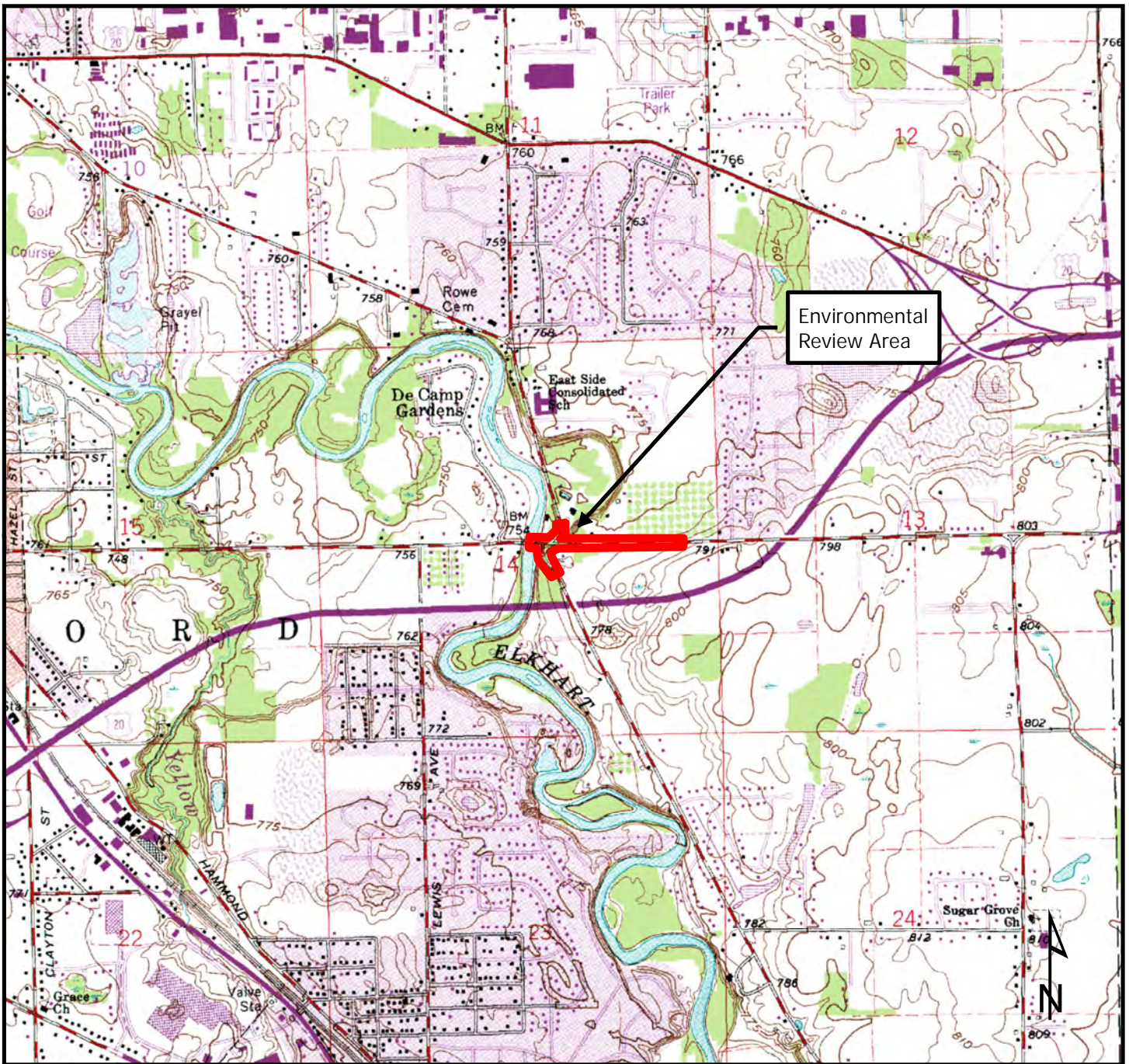
SCALE

NTS

SHEET


Exhibit 1
 Project Location Map

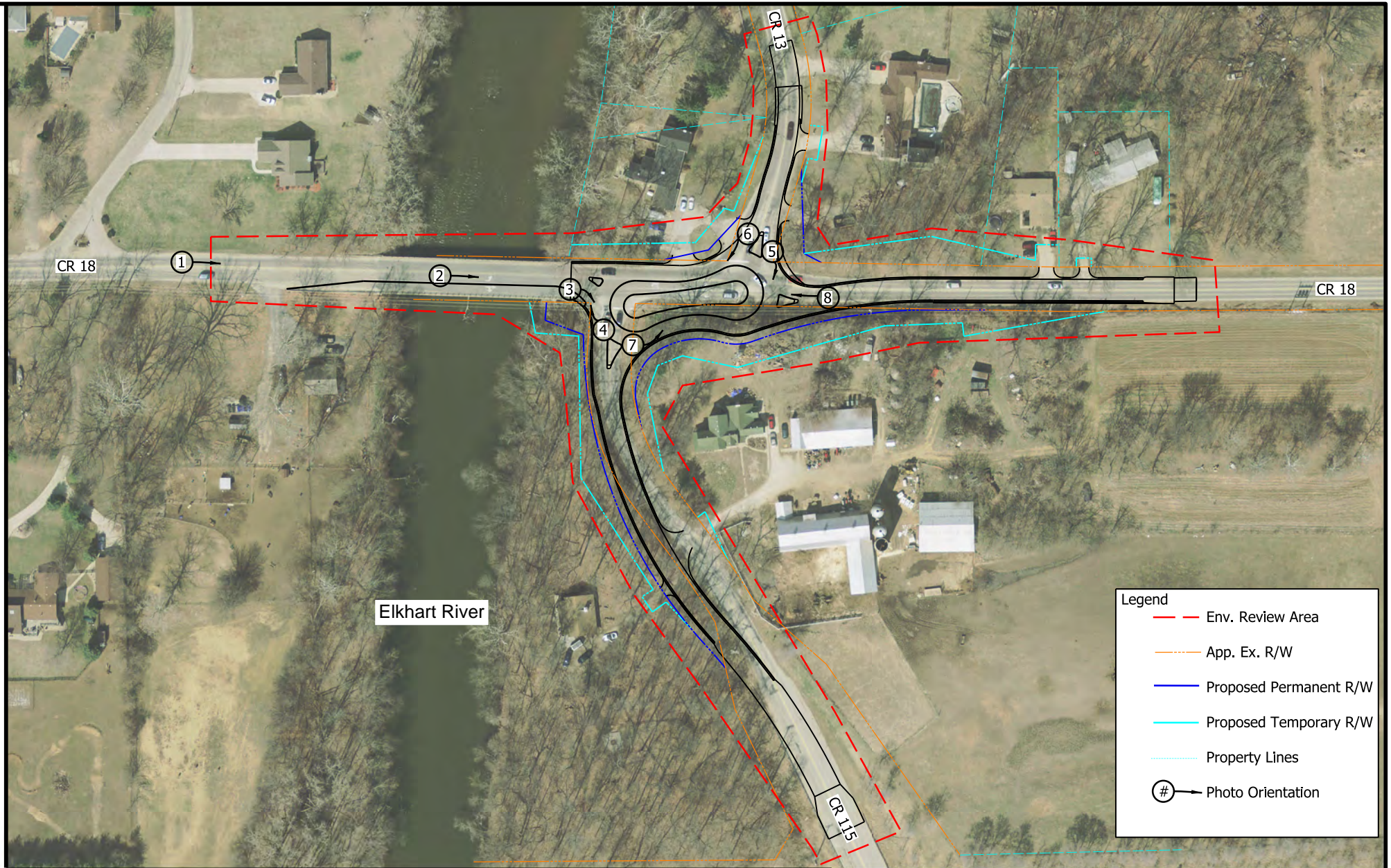
USGS Project Location Map



Portion of 7.5-Minute Series Map, Elkhart County, Elkhart Quadrangle, Indiana
Source: USGS National Map

■ Project Area

 <p>550 Union St. Mishawaka, IN 46544 574.259.9976 troyergroup.com Together, We Will</p>	<p>PROJECT</p> <p>PCR 18 at CR 13/ CR 115 Intersection Improvement, Roundabout (Des. No. 1401749)</p> <p>Elkhart County, Indiana</p>	<p>SCALE</p> <p>1:24,000</p>
		<p>SHEET</p> <p>Exhibit 2 USGS Map</p>



Legend

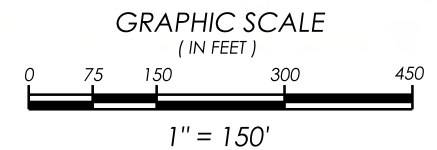
- Env. Review Area
- App. Ex. R/W
- Proposed Permanent R/W
- Proposed Temporary R/W
- Property Lines
- Photo Orientation



PROJECT
CR 18 at CR 13/CR 115
Intersection Improvement
Elkhart County, IN

DES. NO. **1401749**

SHEET
Exhibit 3



SCALE: 1"=150'

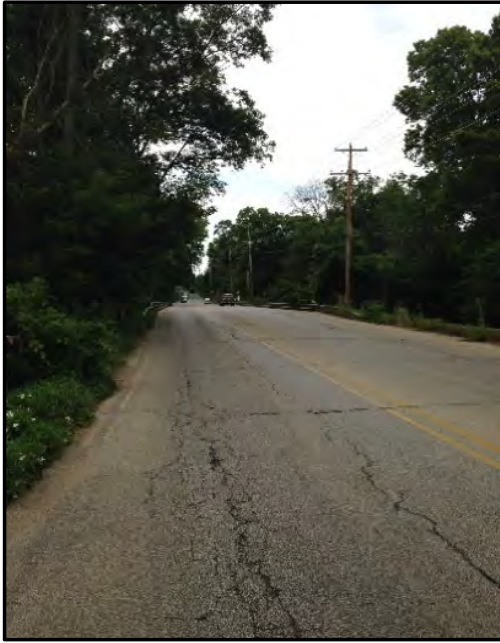


Photo 1. CR 18 eastbound, approaching bridge



Photo 2. CR 18 eastbound, west of intersection, from bridge

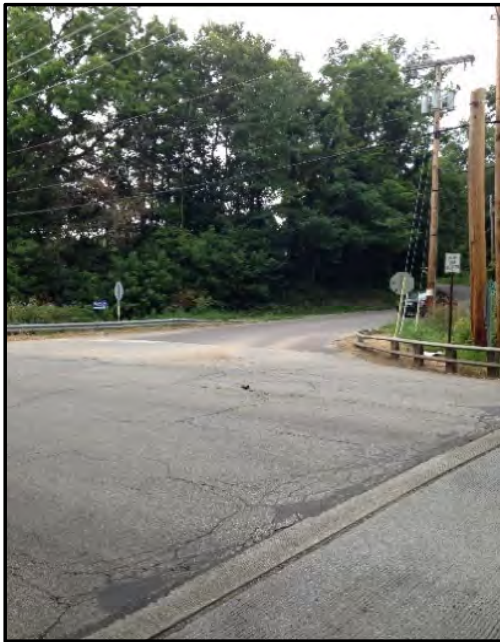


Photo 3. East end of CR 18 bridge, facing southeast



Photo 4. CR 115, facing east along CR 18

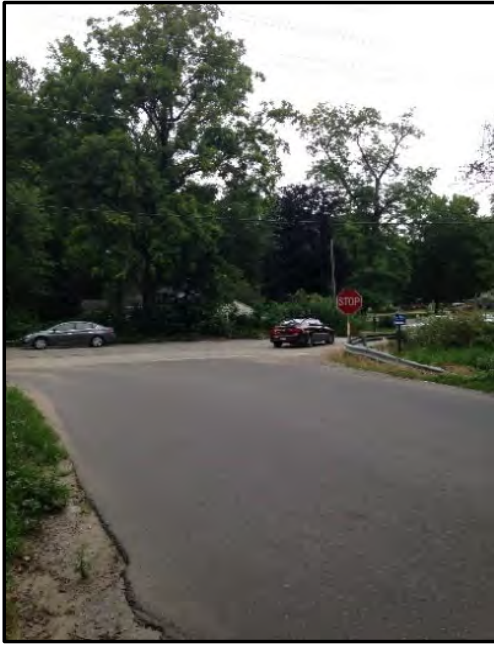


Photo 5. CR 115, facing north
toward CR 18

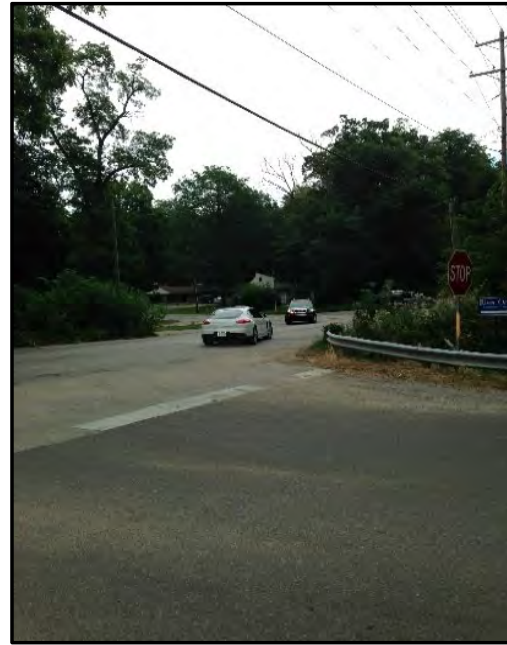


Photo 6. CR 115, facing NE
toward CR 13



Photo 7. CR 13, facing south
toward CR 18



Photo 8. CR 18, east of CR 13
facing west.

PROJECT	DESIGNATION
1401749	1401749
CONTRACT	

SCALES:

PLAN:



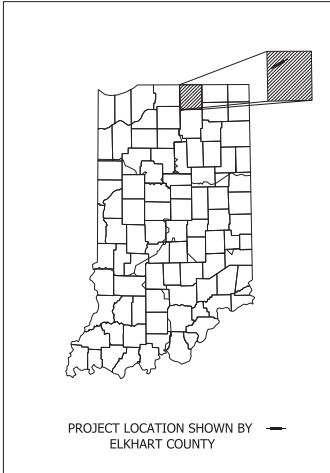
PROFILE HORIZ:



PROFILE VERT:



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



LATITUDE: 41° 39' 34" LONGITUDE: 85° 54' 56"

GROSS LENGTH:	0.13	MI.
NET LENGTH:	X.XX	MI.
MAXIMUM GRADE:	6.0%	%

HUC 14 # 04050001210040

STAGE 1 SUBMITTAL
MARCH 22, 2019

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS

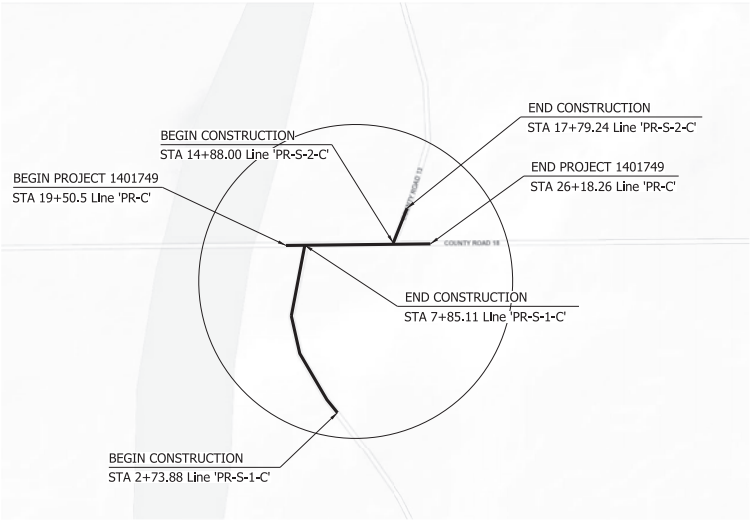
INDIANA DEPARTMENT OF TRANSPORTATION



ROAD PLANS

LOCATION: CR 18 AT CR13/CR115 - INTERSECTION IMPROVEMENT
PROJECT NO. 1401749 P.E.
1401749 R/W
CONST.

INTERSECTION IMPROVEMENT ON CR18 AT CR 13/CR115, LOCATED IN SECTION 14, T-37-N, R-5-E, CONCORD TOWNSHIP, ELKHART COUNTY, INDIANA



LOCATION MAP
CONCORD TOWNSHIP
SEC 14, T37N, R5E
ELKHART COUNTY

TRAFFIC DATA		CR 18
A.A.D.T.	2019	10506 V.P.D.
A.A.D.T.	(2039)	10934 V.P.D.
D.J.V.	(2039)	984 V.P.H.
DIRECTIONAL DISTRIBUTION		50% % EB
TRUCKS		0.5% A.A.D.T.
DESIGN DATA		
DESIGN SPEED		35 M.P.H.
PROJECT DESIGN CRITERIA		3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION		MINOR ARTERIAL
RURAL/URBAN		RURAL
TERRAIN		ROLLING
ACCESS CONTROL		NONE

TRAFFIC DATA		CR 13
A.A.D.T.	2019	8475 V.P.D.
A.A.D.T.	(2039)	9364 V.P.D.
D.J.V.	(2039)	843 V.P.H.
DIRECTIONAL DISTRIBUTION		50 % SB
TRUCKS		0.5 % A.A.D.T.
DESIGN DATA		
DESIGN SPEED		30 M.P.H.
PROJECT DESIGN CRITERIA		3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION		MAJOR COLLECTOR
RURAL/URBAN		RURAL
TERRAIN		ROLLING
ACCESS CONTROL		NONE

TRAFFIC DATA		CR 115
A.A.D.T.	2019	1845 V.P.D.
A.A.D.T.	(2039)	2342 V.P.D.
D.J.V.	(2039)	211 V.P.H.
DIRECTIONAL DISTRIBUTION		50 % SB
TRUCKS		0.3 % A.A.D.T.
DESIGN DATA		
DESIGN SPEED		40 M.P.H.
PROJECT DESIGN CRITERIA		3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION		LOCAL
RURAL/URBAN		RURAL
TERRAIN		ROLLING
ACCESS CONTROL		NONE



PRELIMINARY
NOT FOR CONSTRUCTION
DATE: 3/22/20

PLANS PREPARED BY:	Troyer Group	(574)259-9976	PHONE NUMBER
CERTIFIED BY:			DATE
APPROVED FOR LETTING:			DATE
		INDIANA DEPARTMENT OF TRANSPORTATION	

DESIGNATION	
1401749	
SURVEY BOOK	SHEETS
CONTRACT	1 of 25
	PROJECT
	1401749

UTILITY	OWNER	ADDRESS
AMERICAN ELECTRIC POWER	J. Jay Marlow (260)408-3447	-
NIPSCO GAS	Dave Schaafsma	utilitycoordination@nfsources.com
ELKHART PUBLIC UTILITIES	Kara Young (574)293-2572	1201 S Nappanee St Elkhart, IN 46514
CENTURYLINK	Level 3 Communications Network Relocation (877)366-8344	Nationalrelo@centurylink.com
AT&T - TRANSMISSION	Kenneth Colwell (630)383-9249	1010 N. Saint Mary St. San Antonio, TX 78215 kc1298@att.com

[illegible]

**	All earth shoulders, median areas, cut or fill slopes shall be plain or mulched seeded except where sodding is specified.
**	The paper relocation will be cross sectioned by the Engineer before construction.
	Slag material shall not be used for the No. 53 aggregate used within the Subgrade Treatment Type IV, Modified limits.
	Any disturbed monuments shall be reset.
**	Represents General Notes Required

- Gas ——— Gas Utility Line
- Storm ——— Storm Sewer Line
- San ——— Sanitary Sewer Line
- U.C. Tele. ——— Underground Telephone Line
- F.O. ——— Fiber Optic Line
- Elec. ——— Electric Line (Overhead/Underground)

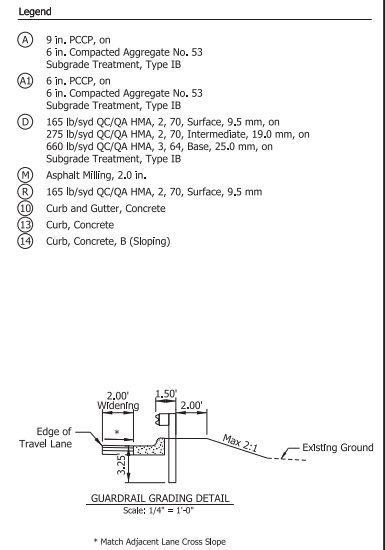
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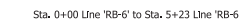
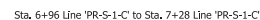
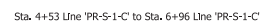
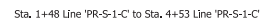
INDIANA
DEPARTMENT OF TRANSPORTATION

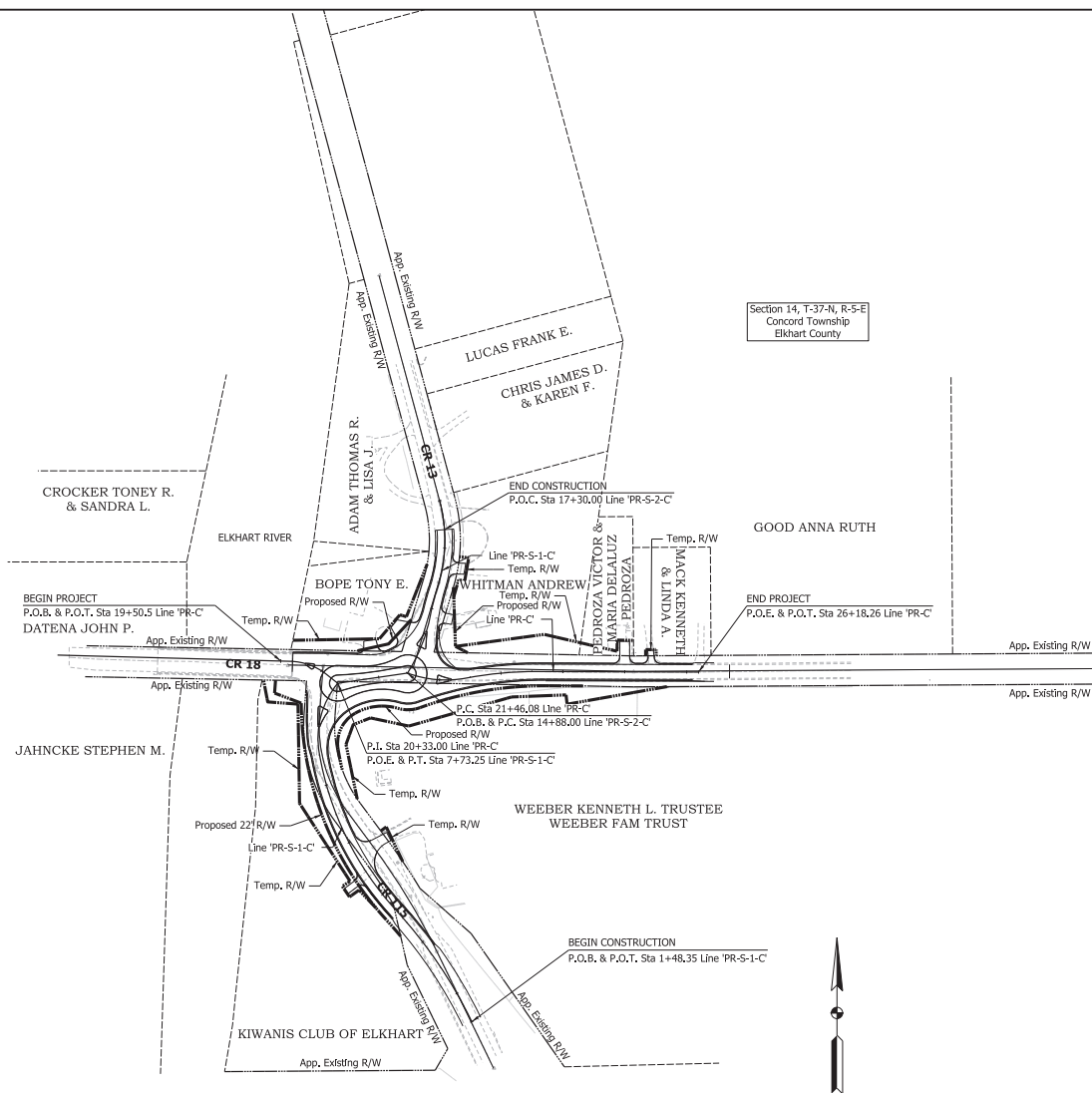
CR 18 AT CR 13/CR115 INTERSECTION IMPROVEMENT

Index

HORIZONTAL SCALE	BRIDGE FILE		
N/A			
VERTICAL SCALE	DESIGNATION		
N/A			
SURVEY BOOK	SHEETS		
ELECTRONIC	2	of	29
CONTRACT	PROJECT		



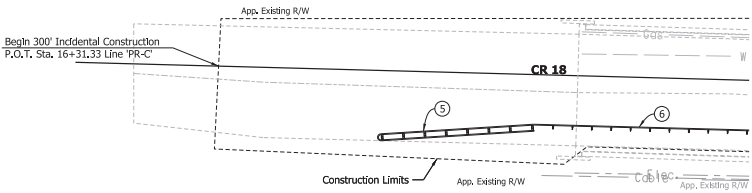




\\012012 - 3/29/2014 - 14
C:\projects\14\Bent County\141140\CR13\CR15\141140\CR13\CR15.dwg
141140\CR13\CR15.dwg

Section 14, T-37-N, R-5-E
Concord Township
Elkhart County

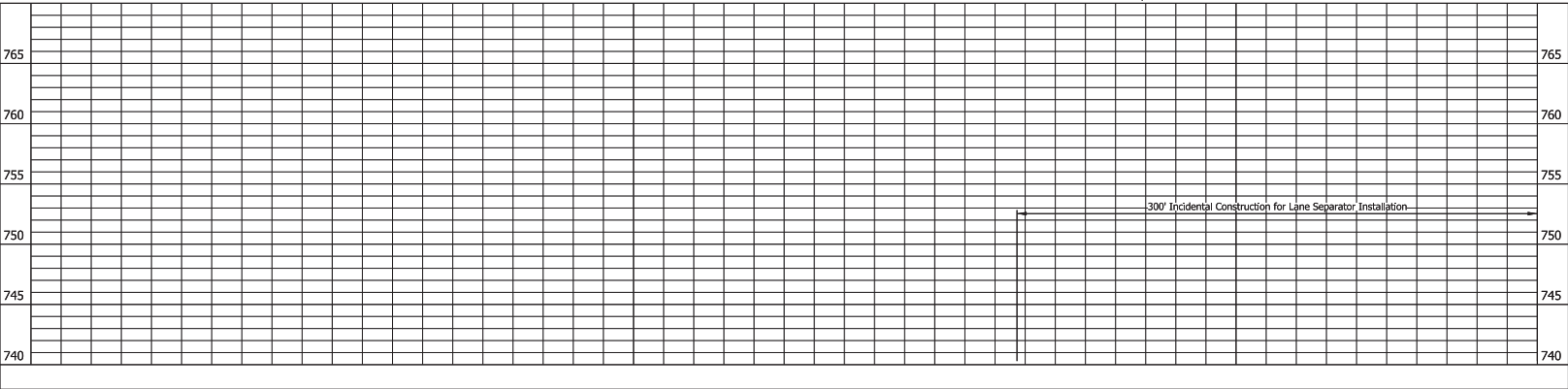
DATENA JOHN P.



JAHNCKE STEPHEN M.

- Legend**
- (A) 9 in. PCPP, on
6 in. Compacted Aggregate No. 53
Subgrade Treatment, Type IB
 - (A1) 6 in. PCPP, on
6 in. Compacted Aggregate No. 53
Subgrade Treatment, Type IB
 - (C) 6 in. PCPP For Approaches, on
Subgrade Treatment, Type II
 - (D) 165 lb/syd QC/QA HMA, 2, 70, Surface, 9.5 mm, on
275 lb/syd QC/QA HMA, 2, 70, Intermediate, 19.0 mm, on
660 lb/syd QC/QA HMA, 3, 64, Base, 25.0 mm, on
Subgrade Treatment, Type IB
 - (M) Asphalt Milling, 2.0 in.
 - (R) 165 lb/syd QC/QA HMA, 2, 70, Surface, 9.5 mm
 - (S) Guardrail MGS W-Beam, 6 Ft 3 In Spacing
 - (6) Traffic Rail Type T631
 - (10) Curb and Gutter, Concrete
 - (11) Lane Separator
 - (13) Curb, Concrete
 - (14) Curb, Concrete, B (Sloping)
 - (99) Remove

ALL TOPOGRAPHY ON THIS SHEET DESCRIBED FROM LINE 'PR-C' UNLESS OTHERWISE NOTED. ALL R/W ON THIS SHEET DESCRIBED FROM LINE 'C' UNLESS OTHERWISE NOTED.

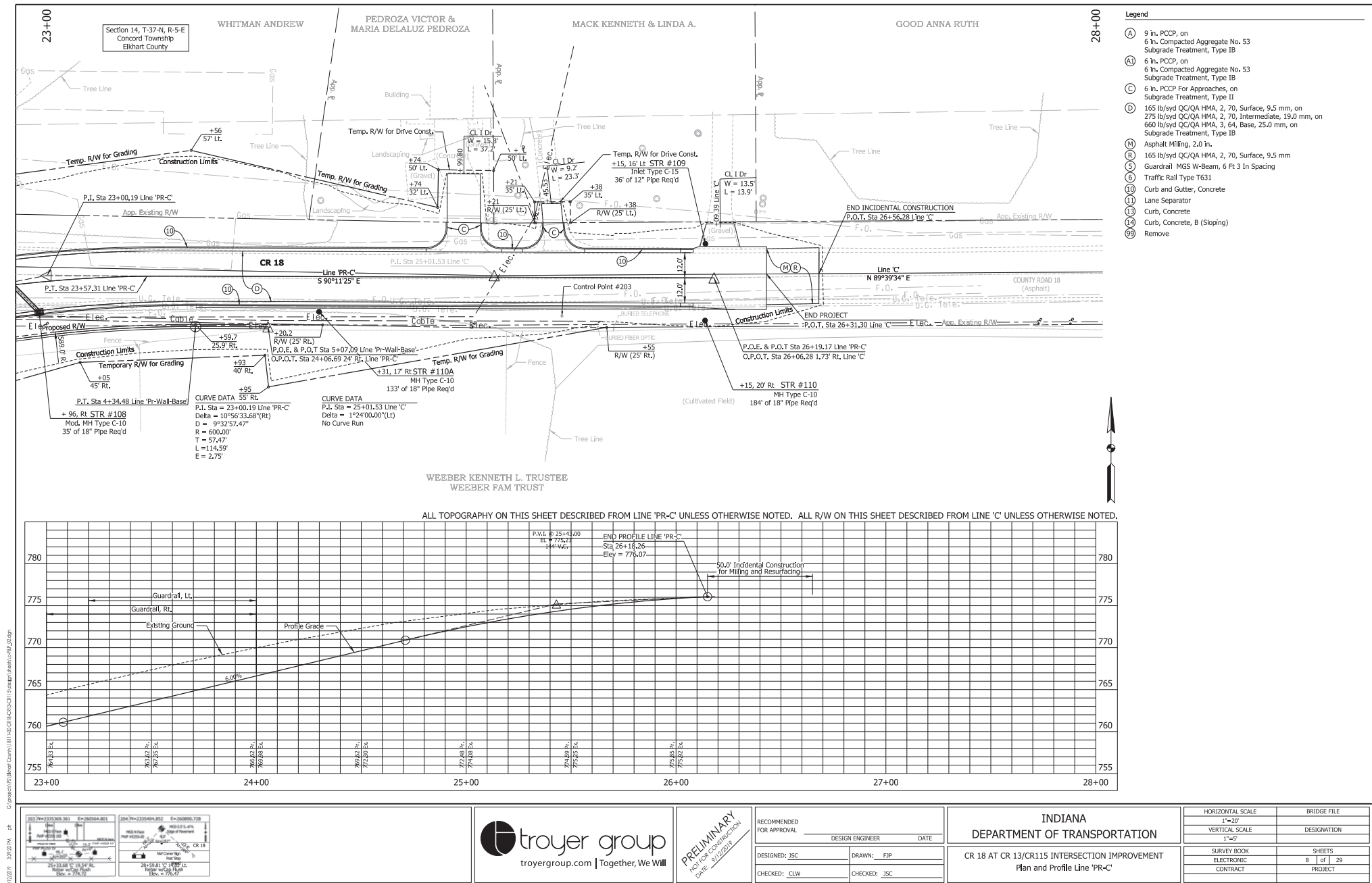


PRELIMINARY
NOT FOR CONSTRUCTION
DATE: 9/17/2013

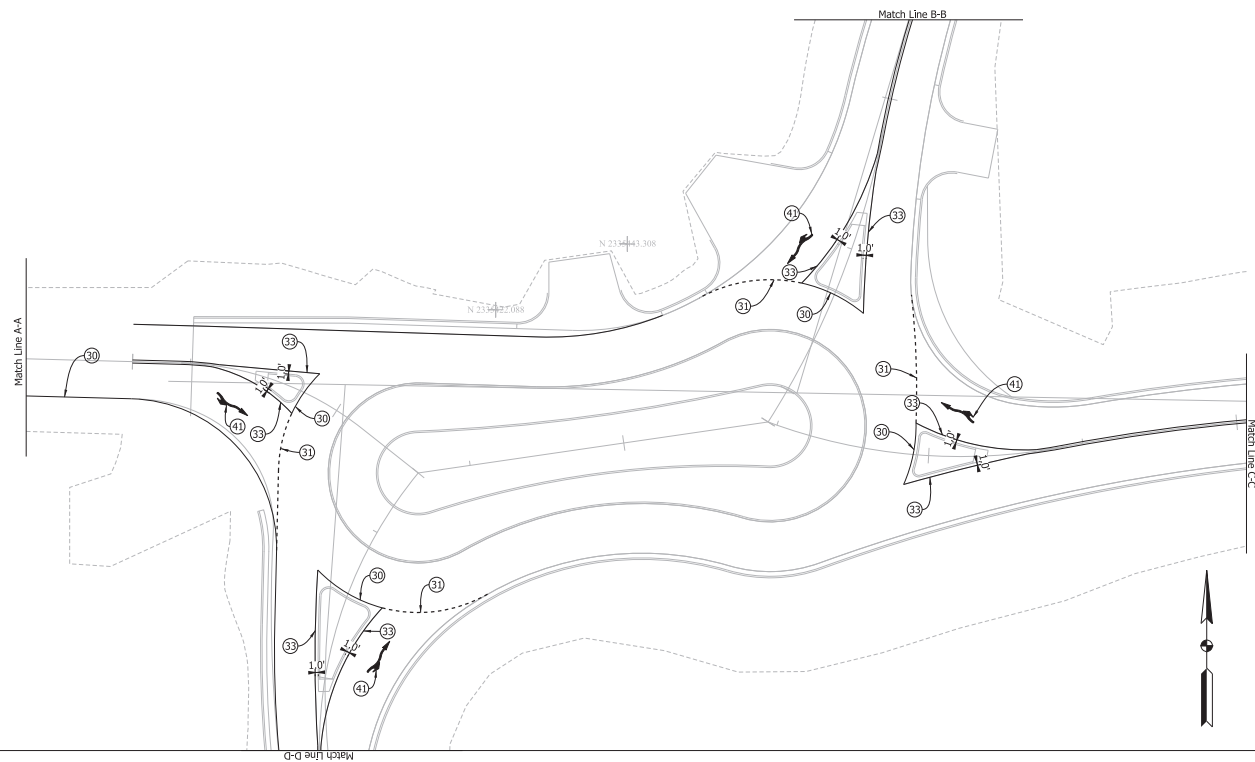
RECOMMENDED FOR APPROVAL	
DESIGNED: JSC	DRAWN: EJP
CHECKED: CLW	CHECKED: JSC

INDIANA
DEPARTMENT OF TRANSPORTATION
CR 18 AT CR 13/CR115 INTERSECTION IMPROVEMENT
Plan and Profile Line 'PR-C'

HORIZONTAL SCALE 1"=20'	BRIDGE FILE
VERTICAL SCALE 1"=5'	DESIGNATION
SURVEY BOOK ELECTRONIC CONTRACT	SHEETS 6 of 29 PROJECT

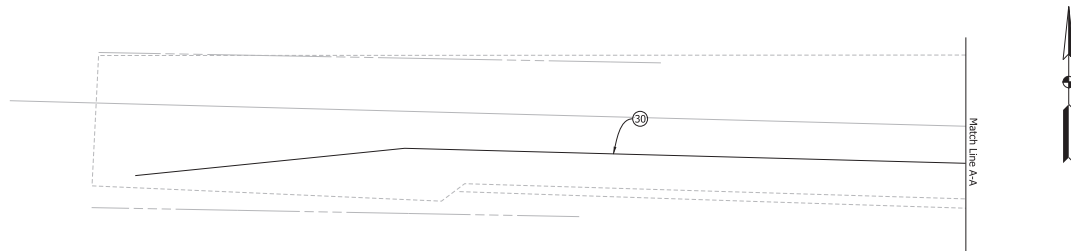


\\012012 - 3/9/2016 - pt C:\projects\18_Broad County\18114025\CR13\CR13_Survey\Sheet16.dwg



Legend

- (30) Line, Multi-Component, Solid, White, 4 in.
- (31) Line, Multi-Component, Dotted, White, 4 in.
- (33) Line, Multi-Component, Solid, Yellow, 4 in.
- (41) Transverse Marking, Solid, White, Multi-Component, Lane Indication Arrow



PRELIMINARY
NOT FOR CONSTRUCTION
DATE: 9/17/2015

RECOMMENDED FOR APPROVAL	
DESIGNED: JSC	DRAWN: EJP
CHECKED: CLW	CHECKED: JSC

INDIANA
DEPARTMENT OF TRANSPORTATION
CR 18 AT CR 13/CR115 INTERSECTION IMPROVEMENT
Pavement Markings

HORIZONTAL SCALE 1"=20'	BRIDGE FILE
VERTICAL SCALE 1"=5'	DESIGNATION
SURVEY BOOK ELECTRONIC CONTRACT	SHEETS 16 of 29 PROJECT

APPENDIX C

Early Coordination



September 10, 2019

Re: Early Coordination
Des. No. 1401749
CR 18 at CR 13/ CR 115 Intersection Improvement, Elkhart County

To Whom it May Concern:

Elkhart County, with funding from the Federal Highway Administration (FHWA), intends to proceed with a project to improve the intersection of County Road 18 at County Road 13/County Road 115 (Des. No. 1401749). FHWA is funding the project and is designated as the lead Federal agency. This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. **Please use the above designation number and description in your reply, and your comments will be incorporated into the formal environmental study.**

The project is located along County Road 18, east of the City of Elkhart, at the intersections with County Road 13 and County Road 115, in Elkhart County, Concord Township, Section 14, Township 37-N, Range 5-E. See attached USGS topographic map (Elkhart quadrangle) indicating the project location.

A red flag investigation was performed by Troyer Group in June, 2019. No major items of concern were identified. A Wetland Delineation Report was prepared by Orbis Environmental Consulting in November 2018. According to Orbis, the nearby Elkhart River and a small adjacent wetland east of the river and south of CR 18 are the only potential jurisdictional features within the project area. A stormwater outlet will be reconstructed near the delineated wetland. Design will seek to avoid or minimize impact to these resources.

The need of the project stems from operational safety issues that currently exist at this intersection. Frequent crashes have occurred, the probable causes for which are excessive speed, inadequate advanced warning signs for intersection, and a large total intersection traffic volume. The purpose of the project is to increase the operation safety at the intersection by replacing the exiting stop-controlled intersection with a safer alternative while still maintaining, or improving, operation capacity for the projected traffic volume demand.

The existing intersections are offset by approximately 130 ft. CR 13 is also skewed to the mainline roadway (CR 18). CR 18 is an east-west roadway. Its west approach has a dedicated left-turn lane extending from the Elkhart River bridge to CR 13, and one shared through-right-turn lane. CR 18 is stop controlled with CR 13 and free at CR 115. CR 115 and CR 13 are north-south roadways that form two T-intersections with CR 18. CR 115 and CR 13 are stop controlled at CR 18.

The proposed scope for improvement involves construction of an elongated, "peanut-shaped" roundabout. Improvements to the intersection are needed to reduce the frequency and severity of vehicular accidents that occur at the intersection. This will provide a facility capable of handling the anticipated traffic growth as well as reducing the number of stop-controlled intersections along arterial corridors.

It comes down to people and their experiences | **Together, We Will**

550 Union St. | Mishawaka, IN 46544 | p 574.259.9976 | www.troyergroup.com

Roadway approach reconstruction and adjacent roadside grading will extend approximately 200 ft. north of the roundabout, 400 ft. east, and 600 ft. south. A modular block retaining wall will run along the east side of CR 115 for approximately 100 ft. and proceed eastward along the south side of the roundabout for approximately another 400 ft. The Elkhart River bridge is located immediately west of the roundabout. The only bridge work to be included with the project is the addition of guardrail on the bridge deck and on CR 18's western bridge approach.

In total, approximately 0.5 acres of new permanent right-of-way will be necessary to accommodate the proposed roundabout footprint and 0.7 acres of temporary right-of-way will be necessary for grading and construction activities. The attached Exhibit 3 illustrates the currently anticipated right-of-way areas as well as an environmental review area inside of which project impacts may occur.

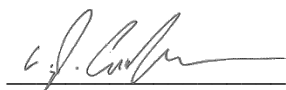
During construction, the intersection will be closed and through traffic will be detoured utilizing Toledo Rd. to the north, CR 17 to the east, and CR 45 to the south and west.

We are seeking comment on the proposed improvements, including any concerns your agency may have within the proposed project limits.

The Troyer Group is investigating the areas near the project for archaeological and historic resources for compliance with Section 106. The results of this investigation will be forwarded to the INDOT's Cultural Resources Office for review and concurrence.

If we do not receive your response **within thirty (30) calendar days** from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, if you feel an extension to the response time is necessary; a reasonable amount may be granted upon request. If a questionnaire follows this letter, please complete and return it. If you have any questions regarding this matter, please feel free to contact C.J. Cunningham at Troyer Group, at (574) 259-9976 or cjc@troyergroup.com or Kent Schumacher, Elkhart County Engineer, at (574) 533-0538 or kschumacher@elkcohw.org. Thank you in advance for your input.

Sincerely,



C.J. Cunningham
Manager – Environmental Services
The Troyer Group

Attachments: Exhibit 1 – Project Location Map
Exhibit 2 – USGS Map
Exhibit 3 – Aerial Image with Proposed Project Scope
Project Area Photos

cc: Indiana Department of Environmental Management
Indiana Geological Society
Federal Highway Administration
Indiana Department of Natural Resources, Division of Fish and Wildlife
US Fish and Wildlife Service



Army Corps of Engineers - Detroit District
INDOT, Fort Wayne District
INDOT, Environmental Services Division
INDOT, Public Involvement Office
USDA, Natural Resource Conservation Service
US Department of Housing & Urban Development
National Park Service
USEPA, Sole Source Aquifer Coordinator
Michiana Area Council of Governments
Elkhart County Surveyor
Elkhart County Highway Department
Elkhart County Board of Commissioners
Elkhart County Council
Concord Township Fire Department
Concord Community Schools





September 10, 2019

TO: John Heiliger, MS4 Supervisor
Elkhart County
4230 Elkhart Road
Goshen, IN 46526

FROM: C.J. Cunningham
Troyer Group
550 Union Street
Mishawaka, IN 46544

RE: Early Notification
INDOT DES Number: 1401749
Location: County Road 18 at County Road 13 & County Road 115
Description: Intersection Improvement

The Indiana Department of Transportation (INDOT) and Federal Highway Administration (FHWA) intend to proceed with the above project. You are being notified because this project lies within an Urbanized Area Boundary (UAB). In accordance with 327 IAC 15-13 (Rule 13 - Municipal Separate Storm Sewer Systems), INDOT has developed a Storm Water Quality Management Plan (SWQMP).

As part of its implementation, projects falling within the UAB will be required to consider appropriate post construction storm water quality best management practices (BMPs). These BMPs should take into consideration the available space, pollutants of concern and receiving waters.

This letter is for notification purposes only, and no action is required by you; however, if you would like to provide your input on water quality concerns, please provide this information **within thirty (30) calendar days** from the date of this letter to the undersigned. Should we not receive your response within the specified timeframe, it will be assumed that your agency does not have additional concerns about water quality issues resulting from the proposed project. Should you find that an extension to the response time is necessary, a reasonable amount of time may be granted upon request. If you have any questions regarding this matter, please feel free to contact C.J. Cunningham of the Troyer Group, at (574) 259-9976 or cjc@troyergroup.com. Thank you in advance for your attention to this matter.

Sincerely,

C.J. Cunningham, Manager – Environmental Services
Troyer Group

It comes down to people and their experiences | Together, We Will

550 Union St. | Mishawaka, IN 46544 | p 574.259.9976 | www.troyergroup.com



Organization and Project Information

Project ID:

Des. ID:

Des. No. 1401749

Project Title:

CR 18 at CR 13/ CR 115 Intersection Improvement

Name of Organization:

Troyer Group

Requested by:

C.J. Cunningham

Environmental Assessment Report

1. Geological Hazards:

- Moderate liquefaction potential
- Floodway

2. Mineral Resources:

- Bedrock Resource: Moderate Potential
- Sand and Gravel Resource: High Potential

3. Active or abandoned mineral resources extraction sites:

- None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

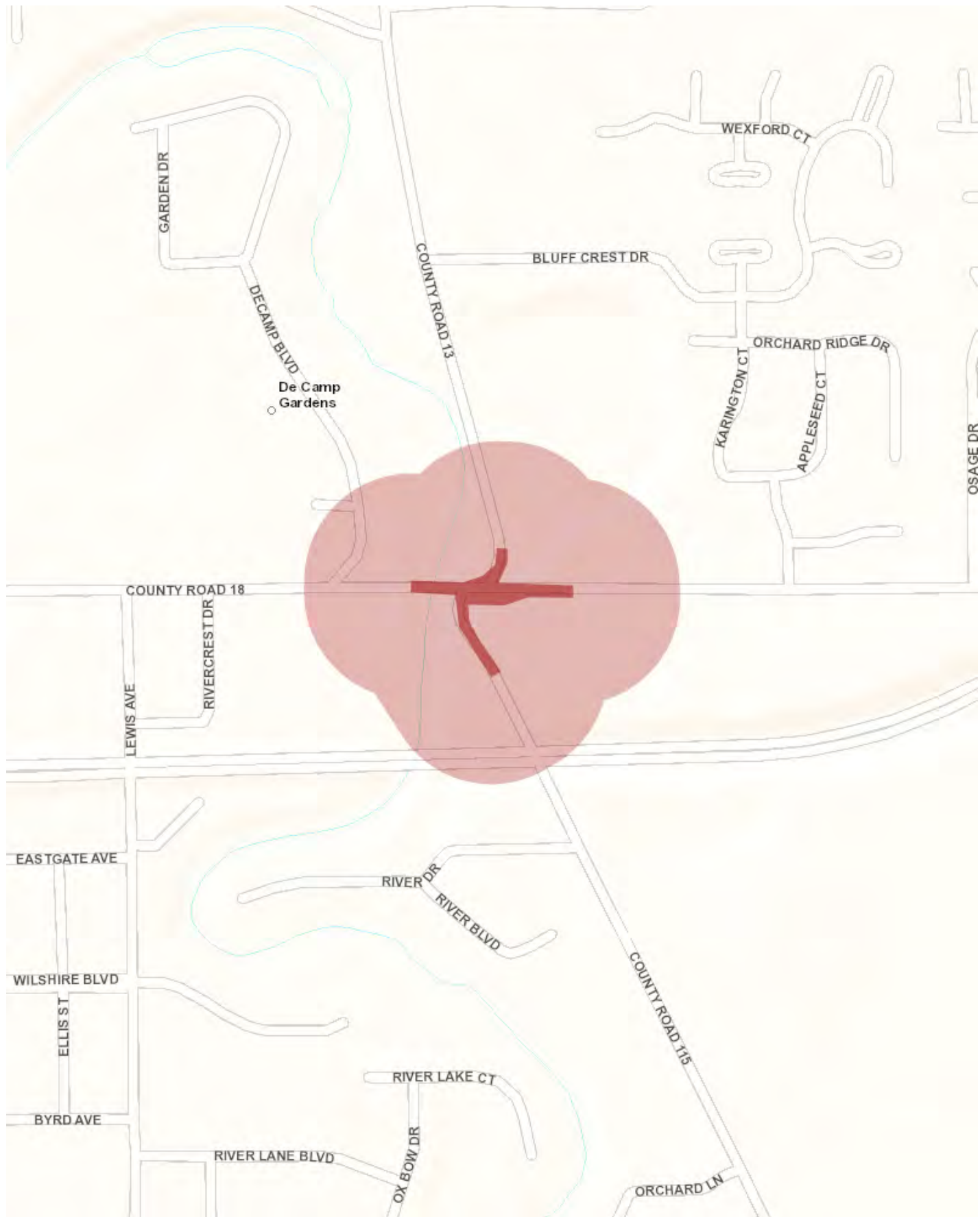
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: September 10, 2019



Metadata:

- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

Elkhart County Highway
Kent Schumacher
610 Steury Ave
Goshen , IN 46528
Date: 9/10/19

Troyer Group
C.J. Cunningham
550 Union Street
Mishawaka , IN 46544

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: Des. No. 1401749, CR 18 at CR 13/ CR 115 Intersection Improvement - The proposed scope for improvement involves construction of an elongated, "peanut-shaped" roundabout. Improvements to the intersection are needed to reduce the frequency and severity of vehicular accidents that occur at the intersection. This will provide a facility capable of handling the anticipated traffic growth as well as reducing the number of stop-controlled intersections along arterial corridors. Roadway approach reconstruction and adjacent roadside grading will extend approximately 200 ft. north of the roundabout, 400 ft. east, and 600 ft. south. A modular block retaining wall will run along the east side of CR 115 for approximately 100 ft. and proceed eastward along the south side of the roundabout for approximately another 400 ft. The Elkhart River bridge is located immediately west of the roundabout. The only bridge work to be included with the project is the addition of guardrail on the bridge deck and on CR 18's western bridge approach.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers,

lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana. A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the following statutes:
 - IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code

- IC 14-28-1 Flood Control Act 310 IAC 6-1
- IC 14-29-1 Navigable Waterways Act 312 IAC 6
- IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
- IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq> (<http://www.in.gov/idem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post

construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for additional project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana, contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm>) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm>.)

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf (http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf)). It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit:

<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit:

<http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).

5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2 , Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF> (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm> (<http://www.in.gov/idem/5284.htm>), is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

Des. No. 1401749, CR 18 at CR 13/ CR 115 Intersection Improvement - The proposed scope for improvement involves construction of an elongated, "peanut-shaped" roundabout. Improvements to the intersection are needed to reduce the frequency and severity of vehicular accidents that occur at the intersection. This will provide a facility capable of handling the anticipated traffic growth as well as reducing the number of stop-controlled intersections along arterial corridors. Roadway approach reconstruction and adjacent roadside grading will extend approximately 200 ft. north of the roundabout, 400 ft. east, and 600 ft. south. A modular block retaining wall will run along the east side of CR 115 for approximately 100 ft. and proceed eastward along the south side of the roundabout for approximately another 400 ft. The Elkhart River bridge is located immediately west of the roundabout. The only bridge work to be included with the project is the addition of guardrail on the bridge deck and on CR 18's western bridge approach.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 9-11-19

Signature of the INDOT

Project Engineer or Other Responsible Agent




Kent Schumacher

Date: 9/10/19

Signature of the

For Hire Consultant



C.J. Cunningham

CJ Cunningham

From: Wright, Mary <MWRIGHT@indot.IN.gov>
Sent: Wednesday, September 11, 2019 8:43 AM
To: CJ Cunningham
Subject: RE: Early Coordination, INDOT Des. No. 1401749 - CR 18 At CR 13/ CR 115 Intersection Improvement, Elkhart County (LPA)

Follow Up Flag: Follow up
Flag Status: Flagged

Early Coordination and Creating a Public Involvement Plan (PIP)

We have received your early coordination notification packet for the above referenced project(s). Our office prefers to be notified at the early coordination stage in order to encourage early and ongoing public involvement aside from the specific legal requirements as outlined in our Public Involvement Manual <http://www.in.gov/indot/2366.htm>. Seeking the public's understanding of transportation improvement projects early in the project development stage can allow the opportunity for the public to express their concerns, comments, and to seek buy-in. Early coordination is the perfect opportunity to examine the proposed project and its impacts to the community along with the many ways and or tools to inform the public of the improvements and seek engagement. A good public involvement plan, or PIP, should consider the type, scope, impacts, and the level of public awareness that should, or could, be implemented. In other words, although there are cases where no public involvement is legally required, sometimes it is simply the right thing to do in order to keep the public informed.

The public involvement office is always available to provide support and resources to bolster any public involvement activities you may wish to implement or discuss. Please feel free to contact our office anytime should you have any questions or concerns. Thank you for notifying our office about your proposed project. We trust you will not only analyze the appropriate public involvement required, but also consider the opportunity to do go above and beyond those requirements in creating a good PIP.

Rickie Clark, Manager

100 North Senate Avenue, Room N642

Indianapolis, IN 46204

Phone: 317-232-6601

Email: rclark@indot.in.gov



United States Department of the Interior

Fish and Wildlife Service



Indiana Field Office (ES)
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273

September 23, 2019

Mr. C.J. Cunningham
The Troyer Group
550 Union Street
Mishawaka, Indiana 46544

Project No.: Des. 1401749
Project: Intersection Improvements CR 18 at CR 13/CR 115
Location: Elkhart, Elkhart County

Dear Mr. Cunningham:

This responds to your letter dated September 10, 2019, requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The proposed project consists of the reconstruction of the intersections of CR 13 and CR 115 with CR 18 just east of the CR 18 Elkhart River crossing. CR 13 and CR 115 are offset about 130 feet, with CR 115 being right at the end of the bridge on the south and CR 13 being about 180 feet east of the bridge on the north. It is proposed to construct an elongated roundabout along CR 18 to encompass both CR 13 and CR 115. A slight realignment of part of CR 115 would be required to improve the curvature and sight distance approaching the roundabout. Additional permanent and temporary right-of-way would be required in all quadrants, with a retaining wall being used in the southeastern quadrant to reduce impacts to the adjacent property.

Most of the required additional permanent and temporary right-of-way is currently wooded, containing a variety of tree sizes and species. The adjacent Elkhart River corridor increases the value of the woodlands for wildlife. Therefore, we request that the loss of these trees be mitigated as close to the project impact site as possible, preferably along the Elkhart River or a nearby tributary. We support the woodland mitigation guidelines of the Indiana Department of Natural Resources contained in their Information Bulletin #17

(<http://www.in.gov/legislative/iac/20190130-IR-312190041NRA.xml.pdf>) which states that the mitigation ratio for non-wetland floodway forest losses of more than 1 acre is to be 2:1 (2 acres replanted for every acre destroyed), planted as close to the impact site as possible; loss of less than an acre is to be mitigated at a 1:1 ratio or 5 trees for each lost tree of 10 inches dbh or greater. If any of the woodland that would be removed is forested wetland, the mitigation ratio is 4:1. This tree replacement requirement is not related to any possible mitigation needed for potential impact to the Indiana bat or northern long-eared bat under the range-wide programmatic informal consultation process.

ENDANGERED SPECIES

The proposed project is within the range of the Federally endangered Indiana bat (*Myotis sodalis*) and the threatened northern long-eared bat (*Myotis septentrionalis*) and eastern massasauga rattlesnake (*Sistrurus catenatus*). The impacts to the 2 bat species will be evaluated utilizing the Section 7 Range-wide Programmatic Consultation process. There is no habitat for the eastern massasauga within the proposed project area, so we agree that the proposed project is not likely to adversely affect this threatened species.

This precludes the need for further consultation on the eastern massasauga for this project as required under Section 7 of the Endangered Species Act of 1973, as amended. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinitiate consultation.

We appreciate the opportunity to comment on this proposed project. If project plans change, please recoordinate with our office as soon as possible. For further discussion, please contact Elizabeth McCloskey at (219) 983-9753 or elizabeth_mccloskey@fws.gov.

Sincerely yours,

/s/ *Elizabeth S. McCloskey*

for Scott E. Pruitt
Supervisor

Sent via email September 23, 2019; no hard copy to follow.

cc: Christie Stanifer, Environmental Coordinator, Division of Fish and Wildlife, Indianapolis, IN



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Joe McGuinness, Commissioner

C.J. Cunningham
Manager – Environmental Services
The Troyer Group
(574) 259-9976
cjc@troyergroup.com

September 30, 2019

Re: Early Coordination Review, Des. 1401749
CR 18 at CR 13/ CR 115 Intersection Improvement, Elkhart County

Dear Mr. Cunningham:

The Indiana Department of Transportation (INDOT) Environmental Services Division (ESD) appreciates the opportunity to assist you on the project referenced above. Pursuant to your early coordination request for an environmental review, we have performed a preliminary search of the project area.

There are several INDOT projects planned in the general area of your project that you should be aware of. These projects are either under construction or have letting dates that may coincide with your project. These projects are listed under their corresponding INDOT Project Managers (PM). Please contact these PMs for more project-specific information.

Jenny Bass, jbass@indot.in.gov, 260-969-8252

- Contract: B-40080; DES: 1600216, 1600217 - Bridge Deck Overlay on Route US 20; Bridge over Elkhart River, EB/WB Lane, 1.35 miles E of US 33.
- Contract: B-40080; DES: 1701350 - Bridge Thin Deck Overlay on Route US 20; US 20 EB/WB, WB Ramp, 2.74 miles E of US 33.

Karen James, kjames1@indot.in.gov, 260-969-8264

- Contract: B-42381; DES: 1900270 - Small Structure Pipe Lining on Route US 20; Carries UNT of Elkhart River, 01.46 E US33.
- Contract: RS-41820; DES: 1701356 - Bridge Deck Overlay on Route US 33; Bridge over Yellow Creek, 0.66 miles S of US 20.
- Contract: RS-41820; DES: 1701337 - HMA Overlay, Preventive Maintenance on Route US 33; From 4.57 miles N of SR 15 N Jct. (CR 15) to US 20.

Matthew Yarian, myarian@indot.in.gov, 260-969-8234

- Contract: R-41822; DES: 1500839 - Concrete Pavement Restoration (CPR) on Route US 20; From 2.81 miles W of SR 19 to 3.18 miles W of SR 15.

Matthew Witt, mwitt@indot.in.gov, 260-399-7320

- Contract: B-41020; DES: 1801159 - Repair Or Replace Joints on Route US 20; CR 115 over US 20, 1.47 Miles of East of US 33.
- Contract: B-41020; DES: 1801160 - Repair Or Replace Joints on Route US 20; CR 18 over US 20 EB/WB, 2.06 Miles East of US 33.

Andrew Wortkoetter, awortkoetter@indot.in.gov, 317-467-3965

- Contract: B-41845; DES: 1900821 - New Bridge, Other on Route ST 1050; Hively Avenue, east of Main Street, Crossing the Norfolk Southern Railway.
- Contract: B-41845; DES: 1801933 - New Bridge, Other on Route ST 2350; Hively Avenue, east of Main Street, crossing the Norfolk Southern Railway.
- Contract: B-41846; DES: 1900836 - New Bridge, Other on Route ST 1015; Sunnyside Ave / Mall Dr. at US 33(Main St) over Norfolk Southern Railroad.

During the cursory review of your project area, it was noted that Concord Fire Department Station 1 is located approximately 1,200 feet to the NW. Considering the number of active projects in the area, it's recommended that coordination with the fire station be maintained throughout the project to address any possible MOT issues. Concord Fire Department Station 1: 23625 Co Rd 18, Elkhart, IN 46516, (574) 875-9644.

Appropriate hazardous materials investigations should be conducted in areas of excavation. If during the Hazardous Material investigation sites are identified that have a reasonable potential to impact the project area(s), ESD recommends that the Indiana Department of Environmental Management's (IDEM) Virtual File Cabinet (VFC) be consulted. The VFC will provide information that is useful in assessing the risk of impacts.

If your project will require the use of state right-of-way please contact the In-House Services Manager at the INDOT Fort Wayne District Office.

As always, be sure to follow all applicable processes as well as federal and state laws and local requirements. Thank you for the opportunity to assist you with your project. If you have any questions, please contact a member of my staff, Tomas A. Beauchamp at 317-234-5071 or TBeauchamp@indot.IN.gov.

Sincerely,



Ron Bales
Environmental Policy Manager,
Environmental Services Division

September 30, 2019

C. J. Cunningham
The Troyer Group
550 Union Street
Mishawaka, Indiana 46544

Dear Mr. Cunningham:

The proposed project to make intersection improvements on County Road 18 at County Road 13/County Road 115 in Elkhart County, Indiana (Des No 1401749), as referred to in your letter received September 10, 2019, will not cause a conversion of prime farmland.

If you need additional information, please contact Daniel Phillips at 317-295-5871.

Sincerely,

JERRY RAYNOR Digitally signed by JERRY RAYNOR
Date: 2019.10.01 08:46:00 -04'00'

JERRY RAYNOR
State Conservationist



State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-21815

Request Received: September 10, 2019

Requestor: The Troyer Group Inc.
CJ Cunningham
550 Union Street
Mishawaka, IN 46544-2340

Project: CR 18 and CR 13/CR 115 intersection peanut-shaped roundabout construction, east of Elkhart; Des #1401749

County/Site info: Elkhart

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: This proposal may require the formal approval of our agency pursuant to the Flood Control Act (IC 14-28-1) for any proposal to construct, excavate, or fill in or on the floodway of the Elkhart River. Please submit more detailed plans to the Division of Water's Technical Services Section if you are unsure whether or not a permit will be required.

Natural Heritage Database: The Natural Heritage Program's data have been checked. The state endangered Greater Redhorse (*Moxostoma valenciennesi*) has been documented in the Elkhart River within 1/2 mile of the project area.

Fish & Wildlife Comments: As long as standard erosion control measures are implemented, we do not foresee any impacts to the Greater Redhorse as a result of this project.

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Riparian Habitat:

Avoid impacting the Elkhart River's wooded riparian corridor to the extent possible. For any unavoidable habitat impacts that will occur, we recommend a mitigation plan be developed (and submitted with the permit application, if required). The DNR's Floodway Habitat Mitigation guidelines (and plant lists) can be found online at: <http://www.in.gov/legislative/iac/20190130-IR-312190041NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees).

2) Wetland Habitat:

Due to the presence or potential presence of wetland habitat on site, we recommend contacting and coordinating with the Indiana Department of Environmental Management (IDEM) 401 program and also the US Army Corps of Engineers (USACE) 404 program.

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

Impacts to wetland habitat should be mitigated at the appropriate ratio according to the 1991 INDOT/IDNR/USFWS Memorandum of Understanding.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas within the project area using a mixture of grasses (excluding all varieties of tall fescue), sedges, wildflowers, shrubs and hardwood trees native to Northern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion.
2. Minimize and contain within the project limits all tree and brush clearing.
3. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
4. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
5. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.
6. Do not excavate or place fill in any riparian wetland.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

Date: October 10, 2019



DEPARTMENT OF THE ARMY
U.S. ARMY CORPS OF ENGINEERS, DETROIT DISTRICT
477 MICHIGAN AVE.
DETROIT, MICHIGAN 48226-2550

October 15, 2019

C.J. Cunningham
The Troyer Group
550 Union Street
Mishawaka, IN 46544

Dear Mr. Cunningham:

This is in response to your September 10, 2019, letter requesting comments on a proposed project to improve the intersection of County Road (CR) 18 at CR 13/CR 115 Elkhart County, Indiana (Des. No. 1401749). The proposed action will convert two T-intersections, which are offset about 130 feet apart, into an elongated, peanut-shaped roundabout. The following information is provided in accordance with our responsibilities under our Regulatory and Civil Works Programs.

Your project may require a Department of the Army Permit, pursuant to Section 404 of the Clean Water Act and/or Section 10 of the Rivers and Harbors Act of 1899. Any of the proposed work that occurs within a water of the United States or adjacent wetlands, will likely require prior authorization through our regulatory permit process. For further information on permit requirements and the application process, please contact the Michiana Branch, Regulatory Office, South Bend, Indiana, at 574-232-1952.

There are no current plans under our Civil Works Program to develop waterways in the vicinity of your project; nor do we have any current or proposed flood risk management studies for the area described in your letter.

Comparing the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map data with the supplied project plan, it appears the actual road construction extends about 20 feet into the Federally mapped 1-percent-annual-chance floodplain (Enclosure). Additionally, the proposed temporary and permanent rights-of-way for the project also extend into the floodway and guardrails are to be installed on the bridge deck and along the roadway within the floodway. The project should be evaluated for potential to impact the floodway from guardrails and any changes to the surface elevations within the floodplain.

We recommend that you coordinate with local officials and with the Indiana Department of Natural Resources regarding the applicability of a floodplain permit prior to construction. This coordination would help ensure compliance with local and state floodplain management regulations and acts, such as the Indiana Flood Control Act (IC 13-2-22). If you obtain information that any part of your project would impact the

floodplain, you should consider other alternatives that, to the extent possible, avoid or minimize adverse impacts associated with use of the floodplain.

We appreciate the opportunity to comment on the proposed project to improve the intersection of CR 18 at CR 13/CR 115 Elkhart County, Indiana. Questions regarding our regulatory program should be directed to Mr. Don Reinke, Regulatory Office, at 313-226-6812. Any other questions may be directed to Mr. Paul Allarding of my staff at 313-226-7590 or me at 313-226-2476.

Sincerely,

Original signed

Charles A. Uhlarik, Chief
Environmental Analysis Branch

Enclosure

Copies furnished:

Don Reinke, Corps, Regulatory Office, Detroit
Mary Weidel, Corps, Floodplain Management Services, Detroit



POWERED BY
Esri, USDA Farm Service Agency

esri

MAP PANELS		Selected FloodMap Boundary
		Digital Data Available
		No Digital Data Available
		Unmapped
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
		Area of Undetermined Flood Hazard Zone D
		Otherwise Protected Area
		Coastal Barrier Resource System Area

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth
		Regulatory Floodway Zone AE, AO, AH, VE, AR
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
		Area with Flood Risk due to Levee Zone D

OTHER FEATURES		Cross Sections with 1% Annual Chance Water Surface Elevation
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
GENERAL STRUCTURES		Jurisdiction Boundary
		Coastal Transect Baseline
		Profile Baseline
		Hydrographic Feature
GENERAL STRUCTURES		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall

Appendix C-25

SOURCE: FEMA Flood Map Service Center, <https://msc.fema.gov/portal/home>

CJ Cunningham

From: McElhinney, Cary <mcelhinney.cary@epa.gov>
Sent: Monday, October 21, 2019 4:53 PM
To: CJ Cunningham
Cc: Bosscher, Valerie
Subject: RE: Early Coordination, INDOT Des. No. 1401749 - CR 18 At CR 13/ CR 115 Intersection Improvement, Elkhart County (LPA)

Follow Up Flag: Follow up
Flag Status: Flagged

C.J.-

As described, the intersection improvement project in Elkhart County, Indiana is not likely to contaminate the St. Joseph Sole Source Aquifer, designated under the authority of the Safe Drinking Water Act, Section 1424(e), so as to create a significant hazard to public health. Unless future developments change the status of the proposal, no modifications or further review under the Sole Source Aquifer Program should be necessary.

As always, we suggest that during construction, demolition and maintenance, appropriate safeguards and best management practices are in place to ensure that local ground water supplies and neighboring drinking water wells are not endangered. Such precautions could include notifying general contractors that the site is sensitive, using “green infrastructure” practices where possible to reduce potential impacts of stormwater run-off, securing adequate precautions for fueling/servicing large equipment, and developing contingency plans to handle the release of any hazardous materials.

- Cary

Cary McElhinney
(312)886-4313



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

February 12, 2020

Consultation Code: 03E12000-2020-SLI-0026

Event Code: 03E12000-2020-E-03678

Project Name: Des. No. 1401749 - CR 18 & 13 & 115, Roundabout, Elkhart County

Subject: Updated list of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Project Summary

Consultation Code: 03E12000-2020-SLI-0026

Event Code: 03E12000-2020-E-03678

Project Name: Des. No. 1401749 - CR 18 & 13 & 115, Roundabout, Elkhart County

Project Type: TRANSPORTATION

Project Description: The Elkhart County sponsored project, Des. No. 1401749, takes place in Concord Township, Elkhart County, Section 14, Township 37N, Range 5E, Indiana, and involves creating a safer and more efficient intersection for County Roads 18, 115, and 13. The proposed scope for improvement involves construction of an elongated roundabout. Improvements to the intersection are needed to reduce the frequency and severity of vehicular accidents that occur at the intersection. The probable cause of these crashes in general is excessive speed, inadequate advanced warning signs for intersection, or a large total intersection traffic volume. No recorded right-of-way exists within parts of the project limits. The project requires approximately 0.5 acres of permanent right-of-way to accommodate the proposed roundabout footprint. The project also requires more than a half-acre of temporary right-of-way for grading and construction activities.

Excavation work will be necessary for grading of the roundabout, its approaches, and roadside drainage. Excavation will take place in the temporary right-of-way. Roadway approach reconstruction and adjacent roadside grading will extend approximately 200 ft north, 400 ft east, and 600 ft south of the roundabout. A modular block retaining wall will run along the east side of CR 115 for approximately 100 ft and proceed eastward along the south side of the roundabout for approximately 400 ft. All work will take place within 100 feet from the existing pavement. There will be approximately 1.1 acres of tree removal that will take place for construction.

This project will also include structure identified as NBI No. 20-00156. The 220.2 ft long bridge is a three span, prestressed concrete continuous bridge structure that spans over Elkhart River. The bridge work included in this project is the addition of guardrail on the bridge deck and on CR 18's western bridge approach.

The October 31, 2019 Bat Inspection for NBI 20-00156 states that no evidence of bats or birds was seen or heard in the structure.

The project is scheduled to be let in February of 2021 and construction is

expected to begin in spring and summer of 2021. The addition of street lights is anticipated; therefore permanent lighting is expected. Temporary lighting is anticipated to be utilized during construction.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/place/41.6590702573092N85.91569493855432W>



Counties: Elkhart, IN

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location is outside the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none">▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

April 20, 2020

Consultation Code: 03E12000-2020-I-0026

Event Code: 03E12000-2020-E-05879

Project Name: Des. No. 1401749 - CR 18 & 13 & 115, Roundabout, Elkhart County

Subject: Concurrence verification letter for the 'Des. No. 1401749 - CR 18 & 13 & 115, Roundabout, Elkhart County' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **Des. No. 1401749 - CR 18 & 13 & 115, Roundabout, Elkhart County** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Des. No. 1401749 - CR 18 & 13 & 115, Roundabout, Elkhart County

Description

The Elkhart County sponsored project, Des. No. 1401749, takes place in Concord Township, Elkhart County, Section 14, Township 37-N, Range 5-E. The proposed scope for improvement involves construction of an elongated roundabout for County Roads 18, 115, and 13.

No recorded right-of-way exists within parts of the project limits. The project requires approximately 0.5 acres of permanent right-of-way to accommodate the proposed roundabout footprint. The project also requires more than 0.5 acre of temporary right-of-way for grading and construction activities.

Excavation work will be necessary for grading of the roundabout, its approaches, and roadside drainage. Excavation will take place in the temporary right-of-way. Roadway approach reconstruction and adjacent roadside grading will extend approximately 200 ft north, 400 ft east, and 600 ft south of the roundabout. A modular block retaining wall will run along the east side of CR 115 for approximately 100 ft and proceed eastward along the south side of the roundabout for approximately 400 ft.

There will be approximately 1.1 acres of tree removal that will take place for construction. The dominant tree species that are included in the tree removal are *Acer saccharinum* (silver maple), *Juglans nigra* (black walnut), *Morus alba* (white mulberry), and *Robinia pseudoacacia* (black locust). All work and tree removal or trimming will take place within 100 feet from the existing pavement. Mitigation is not required for this project. No trees will be removed from 100-300 ft from existing roadway. All tree removal and trimming will occur within the bat inactive season from 10/1/2020 to 3/31/2021. The addition of street lights is anticipated; therefore permanent lighting is expected. Temporary lighting is anticipated to be utilized during construction.

This project will also include a bridge identified as NBI No. 20-00156 that spans over Elkhart River. The bridge work included in this project is the addition of guardrail on the bridge deck and on CR 18's western bridge approach.

This project is within 1000 ft of suitable summer bat habitat. Troyer Group performed a bat inspection on October 31, 2019 for NBI 20-00156 and evidence of bats were not seen or heard on the structure. A review of the USFWS database was performed by INDOT on January 16, 2019 and did not indicate the presence of endangered bat species within 0.5 mi of the project area.

A Red Flag Investigation has been prepared; however, due to the Local Public Agency (LPA) sponsor the RFI will not be submitted in advance of the environmental document.

Construction is anticipated to begin Spring 2021.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will the tree removal alter *any* **documented** Indiana bat or NLEB roosts and/or alter any surrounding summer habitat **within** 0.25 mile of a documented roost?

No

20. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

21. Are *all* trees that are being removed clearly demarcated?

Yes

22. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

Yes

23. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

24. Does the project include slash pile burning?

No

25. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

26. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

27. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *Bat inspection 10-19.pdf* <https://ecos.fws.gov/ipac/project/M5FYB7OFA5HR3OXS2BUR6YJ2HE/projectDocuments/20271079>

28. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

29. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

30. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

31. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

32. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

33. Will the project install *any* new or replace any existing **permanent** lighting in addition to the lighting already indicated for habitat removal (including the removal or trimming of trees) or bridge/structure removal, replacement or maintenance activities?

No

34. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

Yes

35. Will the activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the active season^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Yes

36. Will *any* activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the inactive season^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Yes

37. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

No

38. Will the project raise the road profile **above the tree canopy**?

No

39. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the active season within undocumented habitat.

40. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) and/or increase noise levels above existing traffic/background levels consistent with a No Effect determination in this key?

Automatically answered

Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the inactive season

41. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

42. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

43. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

44. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

45. **Tree Removal AMM 1**

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

46. **Tree Removal AMM 3**

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

47. **Tree Removal AMM 4**

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

48. **Lighting AMM 2**

Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society^{[1][2]} to rate the amount of light emitted in unwanted directions?

[1] Refer to [Fundamentals of Lighting - BUG Ratings](#)

[2] Refer to [The BUG System—A New Way To Control Stray Light](#)

Yes

49. **Lighting AMM 2**

Will the **permanent** lighting used during removal of suitable habitat and/or the removal/trimming of trees within suitable habitat be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

Yes

50. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

Yes

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

No

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

1.1

4. Please describe the proposed bridge work:

Addition of guardrail on the bridge and approaches.

5. Please state the timing of all proposed bridge work:

Spring/Summer of 2021

6. Please enter the date of the bridge assessment:

10/31/2019

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

LIGHTING AMM 2

When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on December 02, 2019. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

APPENDIX D

Section 106 of the NHPA

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECT FINDING
PROPOSED CR 18 AT CR 13/CR 115, INTERSECTION IMPROVEMENT PROJECT,
ELKHART, CONCORD TOWNSHIP, ELKHART COUNTY, INDIANA
DES. NO.: 1401749**

AREA OF POTENTIAL EFFECTS

(Pursuant to 36 CFR Section 800.4(a)(1))

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. Appendix A and B shows the APE for this project. The APE consists of an irregularly shaped area surrounding the intersection of CR 18 at CR 13/CR 115. The width of the APE is generally one property deep. The APE was determined by adjacent properties to the proposed construction limits and line of sight to and from the proposed construction project limits.

ELIGIBILITY DETERMINATIONS

(Pursuant to 36 CFR 800.4(c)(2))

The APE contains no resources listed in the National Register of Historic Places (NRHP). The APE contains no resources eligible to be listed in the NRHP.

EFFECT FINDING

INDOT, acting on FHWA's behalf has determined a "No Historic Properties Affected" finding is appropriate for this undertaking. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

This undertaking will not convert property from any Section 4(f) historic property to a transportation use; the INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Historic Properties Affected"; therefore, no Section 4(f) evaluation is required.

Anuradha Kumar V.

Anuradha V. Kumar, for FHWA
Manager
INDOT Cultural Resources

04/24/2020

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO HISTORIC PROPERTIES AFFECTED
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR Section 800.4(d)(1)
PROPOSED CR 18 AT CR 13/CR 115, INTERSECTION IMPROVEMENT PROJECT, ELKHART,
CONCORD TOWNSHIP, ELKHART COUNTY, INDIANA
DES. NO.: 1401749**

1. DESCRIPTION OF THE UNDERTAKING

Elkhart County, with funding from Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the intersection improvements at the intersection of County Road (CR) 18 at CR 13/CR 115 in Concord Township, Elkhart County, Indiana.

The undertaking is on CR 18 from the intersection of CR 18 and CR 13 and CR 115 in Elkhart County, Indiana. It is within Concord Township, Elkhart, Indiana USGS Topographic Quadrangle, in Section 14, Township 37N, Range 5E (Appendix A). The project proposes intersection improvements in the aforementioned intersection.

The Area of Potential Effects (APE) consists of an irregularly shaped area surrounding the intersection of CR 18 at CR 13/CR 115. The width of the APE is generally one property deep. The APE was determined by adjacent properties to the proposed construction limits and line of sight to and from the proposed construction project limits (Appendix B).

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

Efforts, pursuant to 36 CFR Section 800.4 (b), to identify historic properties in the APE included a check of records available at the Indiana Department of Natural Resources-Division of Historic Preservation and Archaeology (IDNR-DHPA), historical/architectural and archaeological fieldwork, and communication with consulting parties. Sources of information examined at DHPA included National Register of Historic Places (NRHP) listings, Indiana Register of Historic Places listings, the SHAARD Database, the Indiana Historic Bridges, Buildings, and Cemeteries Map, the *Elkhart County Interim Report*, the Indiana Historic Bridge Inventory, archaeological site maps, cultural resources management reports, and cemetery records. There are no NRHP-listed or Indiana Register of Historic Places-listed properties within the APE. No previously identified archaeological sites or cemeteries have been recorded in the project area (Appendix C).

The results of the field surveys were reported in a Historic Property Short Report (HPSR) (Heimlich 2019) and a Phase Ia Archaeological Records Check and Reconnaissance Survey Short Report (Giedd 2019). There were no properties previously identified within the APE. The HPSR documented ten above-ground properties constructed before 1971, 50 years of age by the project letting date, that warranted a "contributing" rating within the APE. No properties were recommended eligible for the NRHP. The archaeological survey did not identify any sites listed in or eligible for the NRHP within the project area (Appendix C).

The following individuals and organizations were invited, in the early coordination letter (ECL) dated April 16, 2019 to be consulting parties (Appendix D):

- Indiana State Historic Preservation Officer (IN SHPO)
- Eastern Shawnee Tribe of Oklahoma
- Elkhart County, Indiana Commissioners
- Elkhart County, Indiana County Council
- Elkhart County, Indiana Highway Department
- Elkhart County, Indiana Historian
- Forest County Potawatomi Community
- Indiana Landmarks, Northern Regional Office
- Miami Tribe of Oklahoma
- Michiana Area Council of Governments
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians

Of the above-listed parties, IN SHPO, the Forest County Potawatomi Community, the Miami Tribe of Oklahoma, and the Pokagon Band of Potawatomi Indians accepted the invitation to be a consulting party while the Indiana Landmarks-Northern Regional Office responded but declined (Appendix D).

The Indiana Landmarks, Northern Regional Office, in a letter dated April 16, 2019, responded:

Thank you for the information about the above listed project and invitation to participate as a consulting party. We do not wish to participate as there appear no historic buildings in the area impacted by the project. We do not wish to receive any further information about the project.

The Pokagon Band of Potawatomi Indians, in a letter dated April 24, 2019, responded:

I am writing to inform you that I have reviewed the details for the project referenced above. I have made the determination that this undertaking will have No Adverse Effect on any historic, religious, or culturally significant resources to the Pokagon Band of Potawatomi Indians. The proposed work is within 0.5 miles of a recorded Pokagon archaeological site. Since the work is occurring within an existing roadway, a No Adverse Effect determination has been issued.

The IN SHPO, in a letter dated May 9, 2019, responded:

We are not aware of anyone who should be invited to become a consulting party for the purposes of the review of this project under Section 106, beyond those whom INDOT already has invited. In your next regular correspondence on this project, please advise us as to which of the invited consulting parties has accepted the invitation.

The Forest County Potawatomi Community, in a letter dated May 14, 2019, responded:

This response is regarding the projects mentioned above. This project falls within the current geographic area of interest of the Forest County Potawatomi Community. Therefore, the Tribal Historic Preservation Office requests a copy of the archaeological report and SHPO comments related to the project.

The Miami Tribe, in a letter dated May 14, 2019, responded:

The Miami Tribe offers no objection to the above-mentioned project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, as this site is within the aboriginal homelands of the Miami Tribe, if any human remains or Native American cultural items falling under the Native American Graves Protection

and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery... The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project.

The following individuals and organizations were invited by letter dated June 7, 2019 to review and respond to the Historic Property Short Report (HPSR) and Archaeology Short Report (Tribes only) (Appendix D):

- Indiana State Historic Preservation Officer (IN SHPO)
- Forest County Potawatomi Community
- Pokagon Band of Potawatomi Indians

The above-listed parties have been provided with access to copies of the HPSR and the Archaeological Short Report (Tribes only). Of the above-listed parties the IN SHPO, Forest County Potawatomi Community, and the Pokagon Band of Potawatomi Indians have responded (Appendix D).

The Pokagon Band of Potawatomi, in a letter dated June 13, 2019, responded:

I am writing to inform you that I have reviewed the details for the project referenced above. The proposed work is occurring within .35 miles of a known historic site that is recorded in the Pokagon Band Registry of Historic Places. I have made the determination that this undertaking will have No Adverse Effect on any historic, religious, or culturally significant resources to the Pokagon Band of Potawatomi Indians.

The IN SHPO, in a letter dated July 9, 2019, responded:

In the review request submittal form and INDOT's letter, the project description states that "improvement alternatives being considered include a 'peanut-shaped' roundabout..." This wording implies that other alternatives are being considered, yet, no others are discussed. Is this the preferred alternative chosen for the project, or are there indeed other alternatives? Furthermore, INDOT's letter and the review request submittal form also indicate that up to 1 acre of permanent and up to .2 acres of temporary right-of-way may be needed for the proposed project. If there are any other alternatives being considered, will this affect the amount of right-of-way needed?

If the "peanut-shaped" roundabout alternative is indeed the chosen alternative for this project, the area of potential effects ("APE") proposed in the historic property short report ("HPSR"; Heimlich 6/6/2019) appears to be of appropriate size to encompass the geographic area in which foreseeable effects of this project on historic properties could occur. However, other alternatives may result in different APEs depending on its size and scope.

Moreover, in regards to buildings and structures, for the purpose of the Section 106 review of this undertaking, we agree that the "peanut-shaped" roundabout alternative contains no above-ground properties listed in or eligible for inclusion in the National Register of Historic Places ("NRHP").

Regarding the archaeology, the Indiana SHPO, in the same letter dated July 9, 2019 (Appendix D), stating:

In terms of potential impact on archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the portions of the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Giedd, 05/23/2019), that no further archaeological investigations appear necessary at the proposed project area.

The Forest County Potawatomi Community, in a letter dated July 16, 2019, responded:

Based on information you provided it does not appear that the proposed work will impact any historic properties of concern to the Tribe. Therefore, the Tribal Historic Preservation office, on behalf of the Tribe, is pleased to offer a finding of no historic properties affected, with two conditions. First, should the SHPO finding differ the Tribe reserves the right to reconsider based on new evidence. Second, in the event that human remains or archaeological materials are exposed as a result of project activities then work must halt and the Tribe must be included in any further discussion regarding treatment and disposition of the find prior to its removal.

Orbis Environmental Consulting responded to the IN SHPO, in an electronic mailing (email) dated July 23, 2019:

To answer your inquires, the “peanut-shaped roundabout” is the preferred alternative at this time. This is based upon the abbreviated Engineers report, which indicates that due to the number of vehicular accidents, the limited sight distance, and the unusual road configuration, that this alternative is the only one that will serve the project need. There are no other alternatives being considered for this reason. Since there are no other alternatives being considered at this time, there is no known additional amounts of right-of-away needed other than what was stated in the preferred alternative.

I believe this addresses your comments from you letter.

The following individuals and organizations were invited by letter dated January 27, 2020 to review and respond to the Historic Property Short Report Addendum and an Archaeology Short Report (Tribes only) (Appendix D):

- Indiana State Historic Preservation Officer (IN SHPO)
- Forest County Potawatomi Community
- Pokagon Band of Potawatomi Indians

The above-listed parties have been provided with access to copies of the HPSR and the Archaeological Short Report (Tribes only). Of the above-listed parties the IN SHPO and the Pokagon Band of Potawatomi Indians have responded (Appendix D).

The Pokagon Band of Potawatomi Indians, in a letter dated January 28, 2020, responded:

I am writing to inform you that I have reviewed the details for the project referenced above. The proposed work is occurring within the details for the project referenced above. The proposed work is occurring within a mile of a known historic site that is recorded in the Pokagon Band Registry of Historic Places. I have made the determination that this undertaking will have No Adverse Effect on any historic, religious, or culturally significant resources to the Pokagon Band of Potawatomi Indians.

The IN SHPO, in a letter dated March 3, 2020, responded:

The area of potential effects (“APE”) proposed in the addendum historic property short report (“HPSR”; Heimlich, 1/27/2020) appears to be of appropriate size to encompass the added geographic area in which foreseeable effects of this project on historic properties could occur.

In regard to buildings and structures, for the purpose of the Section 106 review of this undertaking, we agree that there are no above-ground properties listed in or eligible for inclusion in the National Register of Historic Places (“NRHP”) within the expanded area of potential effects.

Additionally, in terms of potential impact on archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified

any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the portions of the proposed project area and we concur with the opinion of the archaeologist, as expressed in the addendum Indiana archaeological short report (Giedd and Duddleson, 12/09/2019), that no further archaeological investigations appear necessary at the proposed project area.

A public notice regarding the APE and “No Historic Properties Affected” finding will be issued for this project in a local newspaper and public comments will be accepted for 30 days. This document will be revised, if necessary, after the public notice to reflect any comments received.

3. BASIS FOR FINDING

No properties listed in or determined eligible for listing in the NRHP were identified within the APE. No properties within the APE were determined eligible for listing in the NRHP through the Section 106 coordination for this undertaking. No historic properties are present within the area of potential effects; therefore, the finding for this project is “No Historic Properties Affected”.

APPENDICES

- A. PROJECT LOCATION**
- B. PROJECT AREA AND AREA OF POTENTIAL EFFECTS**
- C. REPORT ABSTRACTS AND CONCLUSIONS**
- D. CONSULTING PARTIES CORRESPONDENCES**
- E. PHOTOGRAPHS OF THE APE**

Appendix A

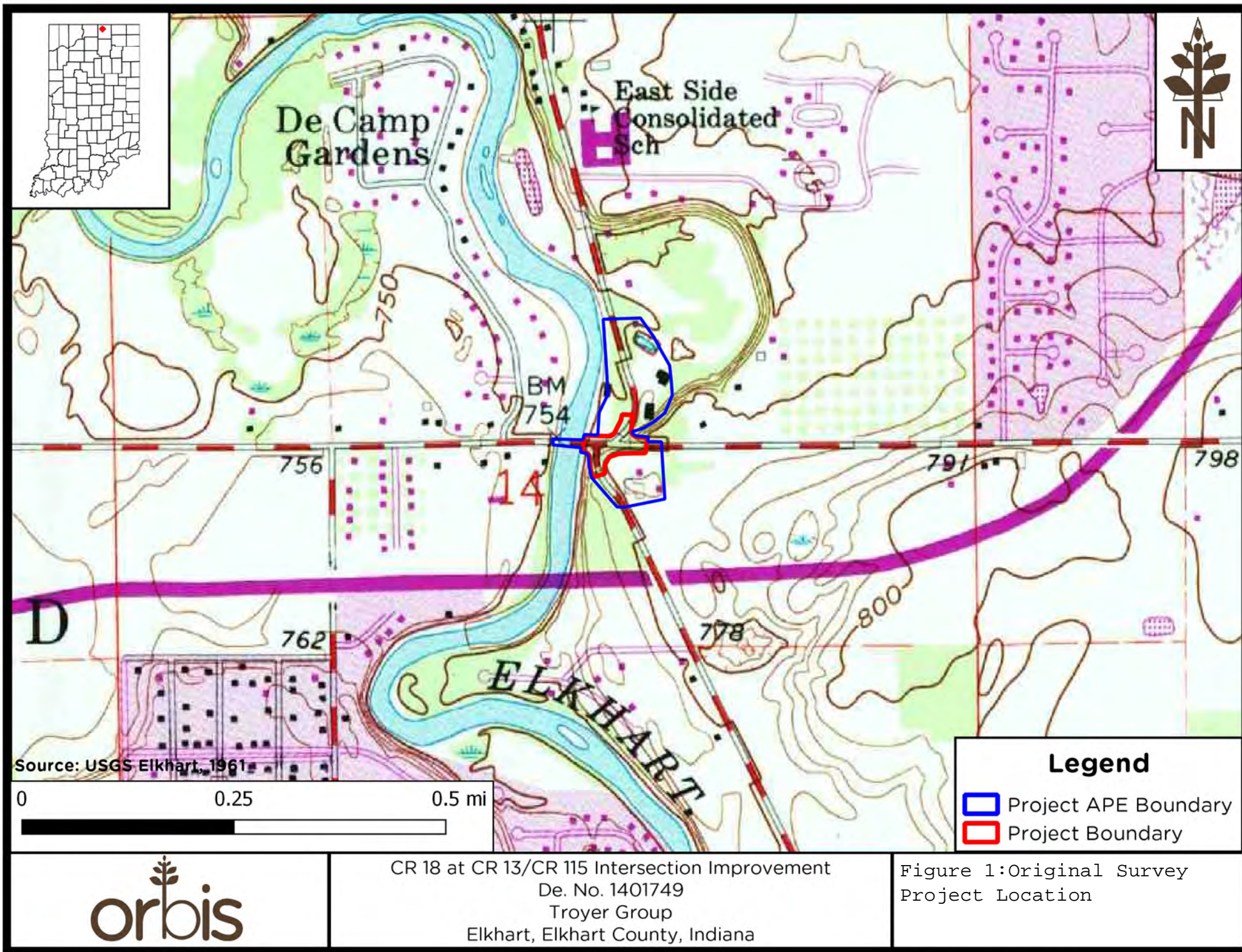
Project Location

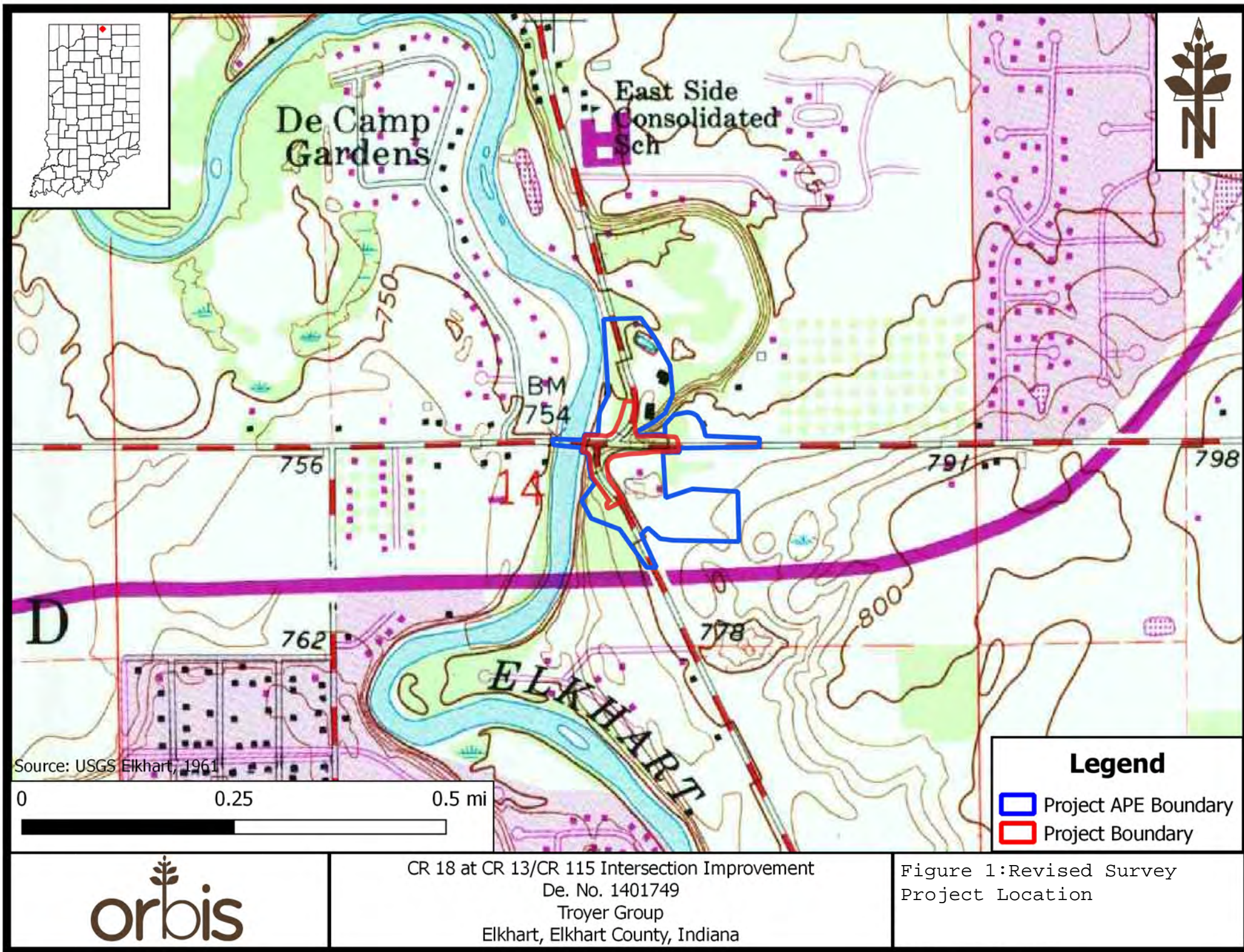
FHWA Documentation of Section 106 Findings of No Historic Properties Affected

Proposed CR 18 at CR 13/CR 115, Intersection
Improvement Project

Concord Township, Elkhart County, Indiana





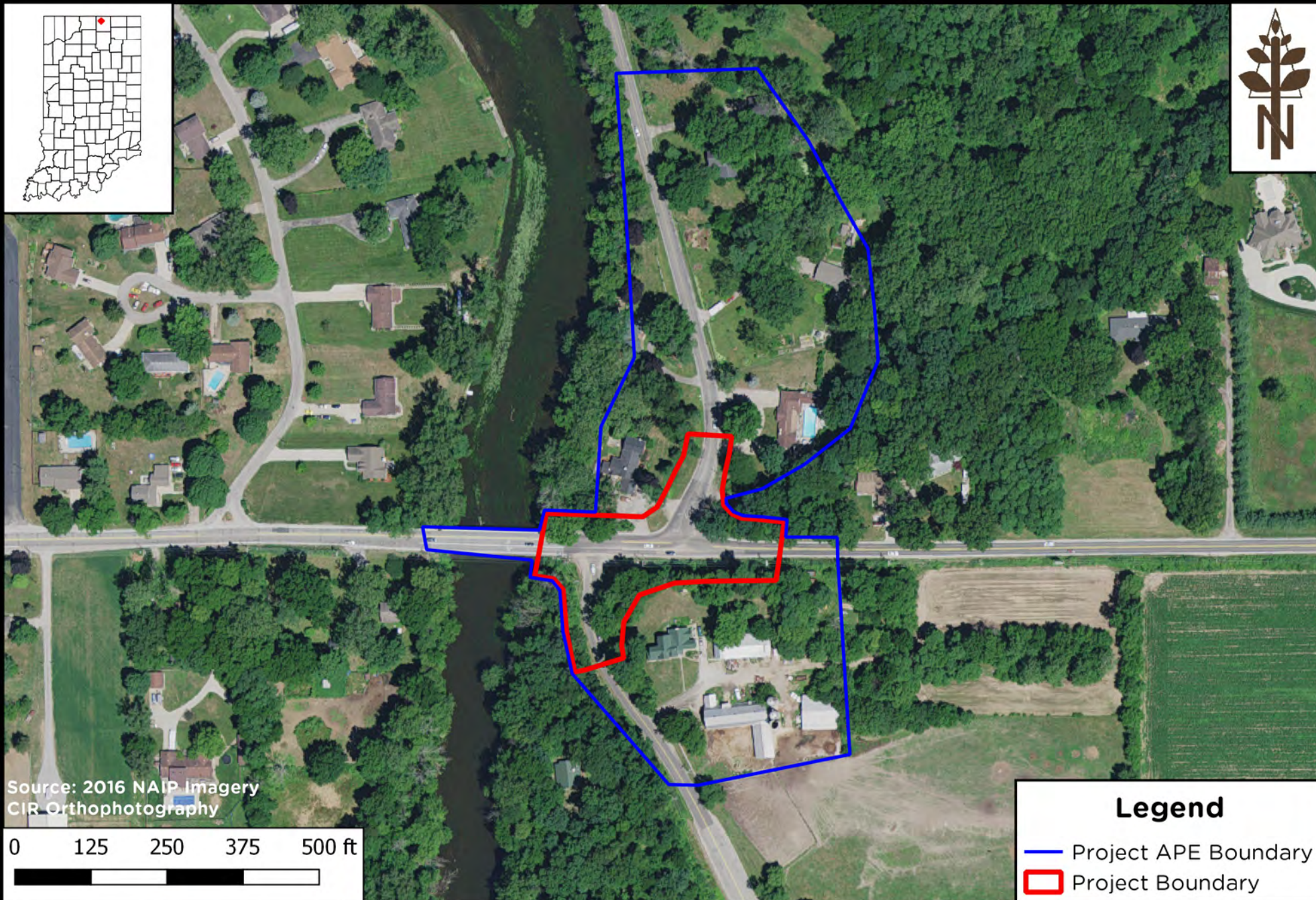


FHWA Documentation of Section 106 Findings of No Historic Properties Affected

Proposed CR 18 at CR 13/CR 115, Intersection
Improvement Project

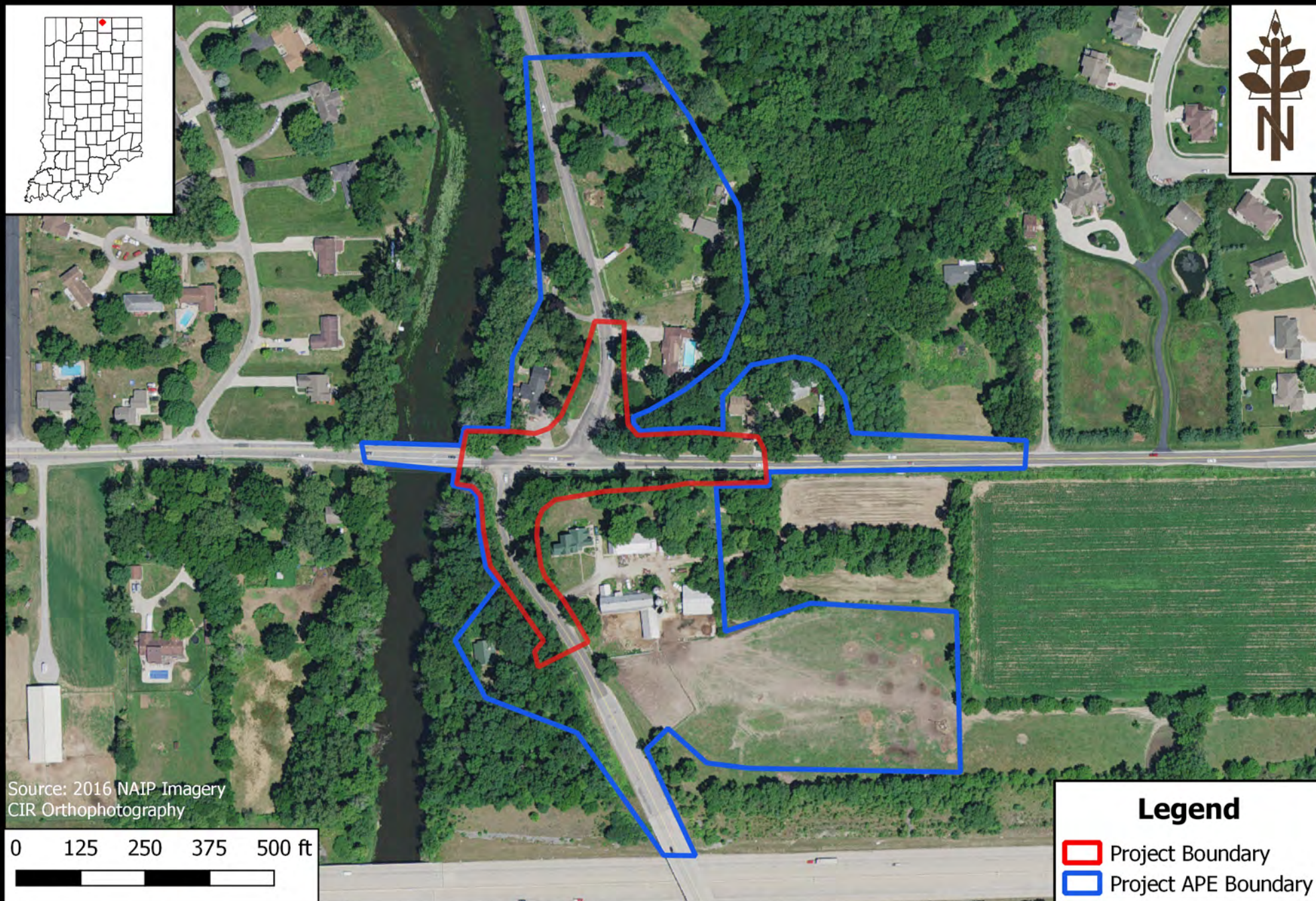
Concord Township, Elkhart County, Indiana





CR 18 at CR 13/CR 115 Intersection Improvement
De. No. 1401749
Troyer Group
Elkhart, Elkhart County, Indiana

Figure 1: Original Survey
Project Area and APE



CR 18 at CR 13/CR 115 Intersection Improvement
De. No. 1401749
Troyer Group
Elkhart, Elkhart County, Indiana

Figure 1: Revised Survey
Project Area and APE

FHWA Documentation of Section 106 Findings of No Historic Properties Affected

Proposed CR 18 at CR 13/CR 115, Intersection
Improvement Project

Concord Township, Elkhart County, Indiana



Phase Ia Reconnaissance Survey



CR 18 at CR 13/CR 115 Intersection Improvement Project Located
in Elkhart, Elkhart County, Indiana (Des. No. 1401749)

May 23, 2019



Orbis Environmental Consulting
P.O. Box 10235 • South Bend, Indiana 46680
Phone: (574) 635-1338

www.orbisec.com

Surface Visibility:

Factors Affecting Visibility:

Visual Walkover ☒ Pedestrian Survey ☐ Shovel Test ☒ Screened ☒ Mesh Size

Interval 5 m ☐ 10 m ☒ 15 m ☒ Other (describe below) ☐

Number of Shovel Test Units Excavated:

Describe Methods:

Attach photographs documenting disturbances below

Describe Disturbances:

Comments:

Results

- ☐ Archaeological records check has determined that the project area does not have the potential to contain archaeological resources.
- ☐ Archaeological records check has determined that the project area has the potential to contain archaeological resources.
- ☒ Phase Ia reconnaissance has located no archaeological resources in the project area.
- ☐ Phase Ia reconnaissance has identified landforms conducive to buried archaeological deposits.

Actual Area Surveyed hectares: acres:

Comments:

Recommendation

- ☐ The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
- ☐ The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.
- ☒ The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.

☐ The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.

☐ The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Other Recommendations/Commitments:

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

Historic Property Short Report for the Proposed CR 18 at CR 13/CR 115, Intersection Improvement Project



/

Des. No. 1401749
Concord Township, Elkhart County, Indiana

June 6, 2019

Prepared for:



Orbis Environmental Consulting
P.O. Box 10235 • South Bend, Indiana 46680
Phone: (574) 635-1338

www.orbisec.com

historian. However, in recognition of the change to IHSSI methodology, “contributing” properties that are located outside of a historic district will not receive an individual NRHP-eligibility evaluation within the text of the HPR. Instead, they will be documented in a table in an appendix, which will include photographs. As before, the IHSSI will serve as an aid in rating properties, but the historian will continue to be responsible for confirming or adjusting this rating—using the IHSSI criteria—based on their own fieldwork and research. Likewise, the historian will continue to be responsible for identifying previously un-surveyed individual resources and historic districts.

With the exception of resources already listed in the NRHP (either individually and/or as part of a historic district), the text of the HPR included NRHP-eligibility evaluations of all potential historic districts and all properties that the historian rated “notable” or “outstanding,” whether previously surveyed or not. The historian who prepared the HPR considered the potential NRHP eligibility of every above-ground resource within the APE.



6.0 RESULTS

Orbis conducted an above-ground properties survey on November 14, 2018. The day was mostly sunny with good-to-excellent visibility. Orbis identified seven above-ground properties constructed before 1971 in the APE. Of these seven above-ground properties, five are rated as “contributing”, and two are rated as “non-contributing” (Appendix A: Survey Table Results, INDOT 1- 7, Photo Locations of Contributing and Non-Contributing Properties, INDOT 1-7).

The identified housing along CR 13 consisted of postwar housing stock, primarily ranch and split-level styles from the mid to late-1950s and the 1960’s. There are five above-ground structures along CR 13 and all warrant a “contributing” rating (Appendix A: Survey Table Results, INDOT 1- 5). The identified above-ground structures along CR 115 is a farmstead. There is a c. 1900 Gable-Front, T-Plan style house and several barns and outbuildings that date from the 1930’s through the 1960’s. The house warrants a “non-contributing” rating (Appendix A: Survey Table Results, INDOT 6). Only one of these barns can be fully observed and it is a c. 1950 English Barn and warrants a “non-contributing” rating (Appendix A: Survey Table Results, INDOT 7). There were no other structures identified in the APE.

7.0 CONCLUSION

The APE contains no properties listed in the National Register. As a result of identification and evaluation efforts for this project, no properties are recommended eligible for listing in the National Register.

Addendum

Cultural Resource Desktop Review and Phase Ia Reconnaissance Survey



CR 18 at CR 13/CR 115 Intersection Improvement Project Located
in Elkhart, Elkhart County, Indiana (Des. No. 1401749)

November 26, 2019



Orbis Environmental Consulting
P.O. Box 10235 • South Bend, Indiana 46680
Phone: (574) 635-1338

www.orbisec.com

Field Supervisor: J Ryan Duddleson

Field Crew: James Heimlich

Surface Visibility: 0 Percent

Factors Affecting Visibility: Vegetation, pavement.

Visual Walkover ☒ Pedestrian Survey ☐ Shovel Test ☒ Screened ☒ Mesh Size 1/4 in

Interval 5 m ☐ 10 m ☒ 15 m ☒ Other (describe below) ☐

Number of Shovel Test Units Excavated: 11

Describe Methods: See Appendix B - Field Methods and Figure 3

Attach photographs documenting disturbances below

Describe Disturbances: Utilities

Comments: One historic house was observed adjacent to Area 4 (Figures 4-6). It is most likely the same structure that appears on the 1874, 1915, and 1920 historic maps (Geo. A. Ogle 1915, Higgins, Belden & Co 1874, and Hixson 1920). However, this structure does not appear on the IBBC. See Heimlich 2019 for further information.

Results

- ☐ Archaeological records check has determined that the project area does not have the potential to contain archaeological resources.
- ☐ Archaeological records check has determined that the project area has the potential to contain archaeological resources.
- ☒ Phase Ia reconnaissance has located no archaeological resources in the project area.
- ☐ Phase Ia reconnaissance has identified landforms conducive to buried archaeological deposits.

Actual Area Surveyed hectares: 00.4 acres: 00.9

Comments: Figure 3 shows the areas surveyed by Orbis archaeologists during this addendum Phase Ia reconnaissance. Appendix B contains descriptions of these areas.

Recommendation

- ☐ The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
- ☐ The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.
- ☒ The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.
- The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which
- ☐ have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.
- ☐ The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Other Recommendations/Commitments:

January 27, 2020

RE: Proposed County Road 18 at County Road 13/County Road 115,
Intersection Improvement Project, Historic Property Short Report (HPSR) Addendum
in Elkhart, Concord Township, Elkhart County, Indiana, Des. No. 1401749

Dear Consulting Party,

Elkhart County, with funding from Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the intersection improvements at the intersection of County Road (CR) 18 at CR 13/CR 115, Des. No. 1401749. Orbis Environmental Consulting and Troyer Group are under contract with Elkhart County to advance the environmental documentation for the referenced project.

A Section 106 early coordination letter was distributed on April 16, 2019. In addition, a letter distributed on June 7, 2019 notified consulting parties that a historic property short report and archaeology short report were available for review and comment.

The proposed undertaking is on CR 18 from the intersection of CR 18 and CR 13 and CR 115 in Elkhart County, Indiana. It is within Concord Township, Elkhart, Indiana USGS Topographic Quadrangle, in Section 14, Township 37N, Range 5E (Appendix A). The project proposes intersection improvements in the aforementioned intersection.

Since the last correspondence, the project area has expanded, requiring further historic structures survey work. Troyer Group notified Orbis Environmental Consulting about these changes on September 23, 2019. Changes to the project area involve an increase of road work. This increased road work entails a realignment of CR 18 and CR 115 in order to have it align with the proposed intersection improvements. Along with the road realignment there is a proposed increase of temporary and permanent right-of-way on the southwest side of the intersection, along CR 115 and along CR 18 on the southeast and northeast sides of the road (Appendix B).

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE was increased due to the expansion of the project area. The APE is generally one property deep and established through view shed from the project boundary limits. The APE was expanded along the northeast and eastern end of CR 18 to include two additional other above-ground structures and the view shed from the project area. It was also expanded along the southwestern and southern end of CR 115 to include one additional above-ground structure and the view shed from the project area (Appendix C and Appendix D). The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. A historic structures survey was conducted on October 10, 2019 to include any above-ground structures in the revised APE (Appendix C and Appendix D). Results of the additional survey are reported here. The expanded APE resulted in three additional above-ground structures being evaluated as warranting a "contributing" rating. These additional structures are identified as INDOT 8-10 (Appendix E). An updated table and photo location of the above-ground structures in the APE can be found in Appendix E. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP.

Enclosures:

Appendix A: Project Location Figure

Appendix B: Proposed Intersection Improvements Figure

Appendix C: Past Project APE and Revised Project APE

Appendix D: Project Photographs

Appendix E: Above-Ground Structures Evaluation Table and Location within APE

Appendix D

Consulting Parties Correspondences

FHWA Documentation of Section 106 Findings of No Historic Properties Affected

Proposed CR 18 at CR 13/CR 115, Intersection
Improvement Project

Concord Township, Elkhart County, Indiana



DES. NO. 1401749
CONSULTING PARTY LIST

Name	How Contacted	Invite Sent	Response to Invitation	Date	Hard Copy Sent	Notes
Indiana SHPO	DHPA, 402 W. Washington St. Indianapolis, IN 46204	4/16/2019; 6/7/2019; 1/27/2020	Accepted	N/A	4/16/2019; 6/7/2019; 1/27/2020	
Eastern Shawnee Tribe of Oklahoma	INDOT Emailed: thpo@estoo.net	4/16/2019	No Response	N/A	N/A	
Elkhart County, Indiana Commissioners	Ccommissioners@elkhartcounty.com	4/16/2019	No Response	N/A	N/A	
Elkhart County, Indiana County Council	117 N. Second St., Elkhart, IN 46526	4/16/2019	No Response	N/A	N/A	
Elkhart County, Indiana Highway Department	kschumacher@elkcohw.org	4/16/2019	No Response	N/A	N/A	
Elkhart County, Indiana Historian	mbrenneman@maplenet.net	4/16/2019	No Response	N/A	N/A	
Forest County Potawatomi Community	INDOT Emailed: Michael.LaRonge@fcpotawatomi-nsn.gov	4/16/2019; 6/7/2019; 1/27/2020	Accepted	5/14/2019; 6/16/2019	N/A	
Indiana Landmarks, Northern Regional Office	tzeiger@indianalandmarks.org	4/16/2019	Declined	4/16/2019	N/A	
Miami Tribe of Oklahoma	INDOT Emailed: dhunter@miamination.com	4/16/2019	Accepted	4/16/2019	N/A	
Michiana Area Council of Governments	jturnwald@macog.com	4/16/2019	No Response	N/A	N/A	
Peoria Tribe of Indians of Oklahoma	INDOT Emailed: lpappenfort@peoritribe.com	4/16/2019	No Response	N/A	N/A	

Name	How Contacted	Invite Sent	Response to Invitation	Date	Hard Copy Sent	Notes
Pokagon Band of Potawatomi Indians	INDOT Emailed: Matthew.Bussler@pokagonband-nsn.gov	4/16/2019; 6/7/2019	Accepted	4/24/2019; 6/13/2019	N/A	



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

April 16, 2019

This letter was sent to the listed parties.

RE: Proposed County Road 18 at County Road 13/County Road 115,
Intersection Improvement Project in Elkhart, Concord Township,
Elkhart County, Indiana, Des. No. 1401749

Dear Consulting Party (see attached list),

Elkhart County, with funding from Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the intersection improvements at the intersection of County Road (CR) 18 at CR 13/CR 115, Des. No. 1401749. Orbis Environmental Consulting and Troyer Group are under contract with Elkhart County to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is on CR 18 from the intersection of CR 18 and CR 13. and CR 115 in Elkhart County, Indiana. It is within Concord Township, Elkhart, Indiana USGS Topographic Quadrangle, in Section 14, Township 37N, Range 5E.

The proposed undertaking is on CR 18 from the intersection of CR 18 and CR 13 and CR 115 in Elkhart County, Indiana. It is within Concord Township, Elkhart County, Indiana USGS Topographic Quadrangle, in Section 14, Township 37N, Range 5E.

Currently there are two separate T-intersections, one at CR 18 with 115, with CR 115 being stop-controlled, and CR 18 being free-flow. The other is at CR 18 with CR 13, both roads being stop-controlled. Improvement alternatives being considered include a "peanut shaped" roundabout that would act as a four-legged, single-lane roundabout, elongated to intersect with both CR 13 and CR 115. Ingress and egress access to all adjacent residential properties will be maintained by relocating and extending driveways where necessary. No pedestrian improvements are included with the project. Much of the improved roadway surface will be curbed and guttered and water will be detained before being outlet into the nearby Elkhart River. It is estimated that between 0.5 and 1.0 acres of additional permanent right-of-way will be necessary to accommodate the proposed improvements. Up to 0.2 acres of temporary right-of-way may also be necessary to accommodate incidental construction activities.

The purpose of the proposed project is to increase the operational safety and operational efficiency at the intersection of CR 18, CR 13 and CR 115. The project is needed because the unusual intersection geometry has resulted in frequent accidents. In a span of 29 months between 2014 and 2016, 17 crashes occurred at the intersection, including eight rear-end collisions. The causes of the rear-end collisions were attributed to excessive speed and inadequate advance warning signage. The project is also needed because the southern approach of the existing intersection experiences a Level of Service (LOS) D, which represent a very poor operating condition causing long user delays. Left untreated, this intersection approach is expected to operate at a LOS E by year 2035.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <http://www.achp.gov/citizensguide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no cultural resource investigations have occurred; however, the results of cultural resource identification and evaluation efforts, both above-ground and archaeological, will be forthcoming. Consulting parties will receive notification when these reports are completed.

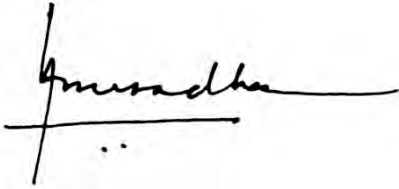
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact James Heimlich of Orbis Environmental Consulting at (574) 850-6003 or jheimlich@orbisec.com. All future responses regarding the proposed project should be forwarded to Orbis Environmental Consulting at the following address:

James L. Ingermann Heimlich
Archaeologist/Historian
Orbis Environmental Consulting
P.O. Box 10235
South Bend, IN 46680
jheimlich@orbisec.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

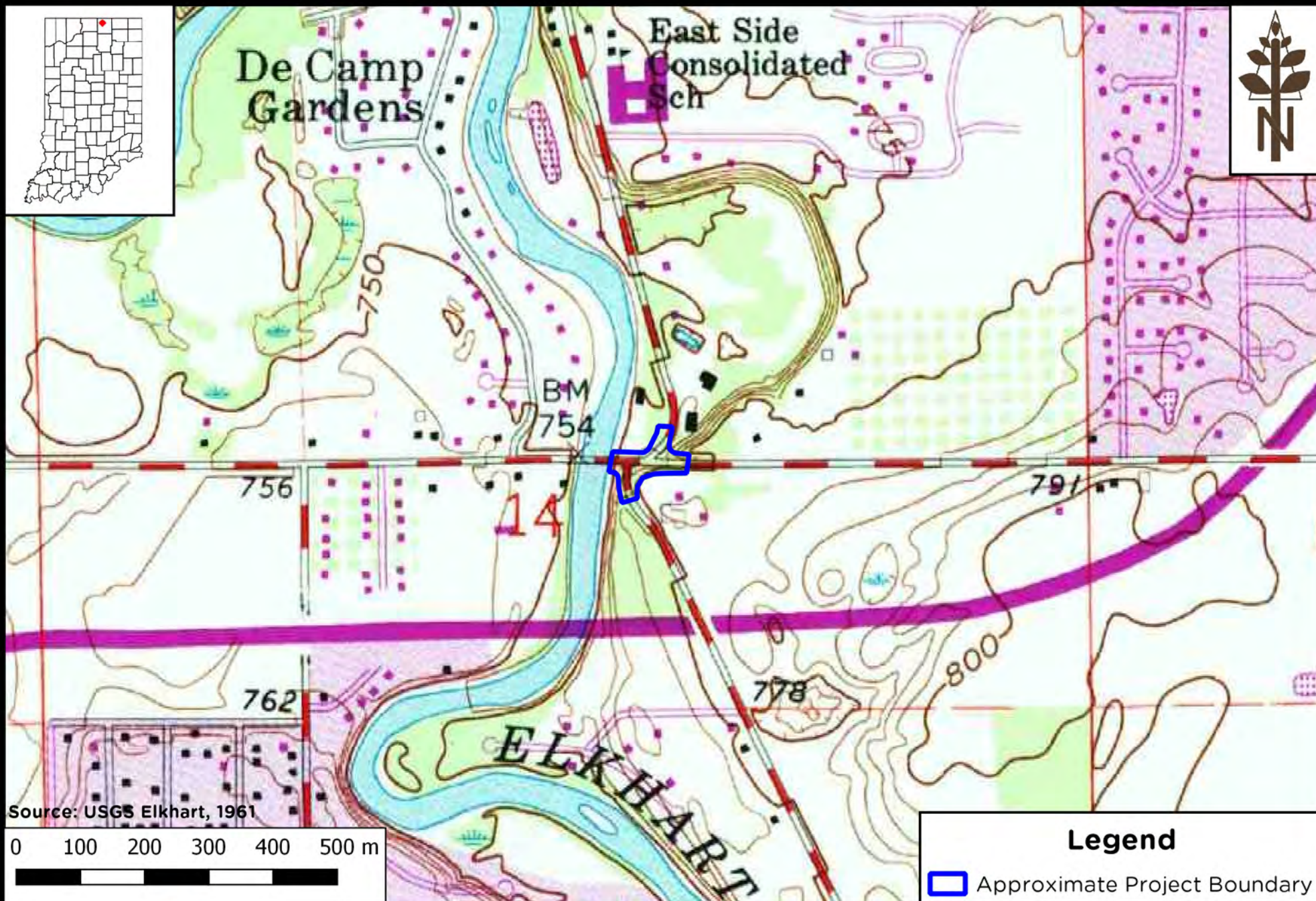
Sincerely,

A handwritten signature in black ink, appearing to read "Anuradha", with a horizontal line drawn underneath it.


Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:
Project Location Figure

Distribution List:
Department of Natural Resources, Division of Historic Preservation and Archaeology
Eastern Shawnee Tribe of Oklahoma
Elkhart County, Indiana Commissioners
Elkhart County, Indiana County Council
Elkhart County, Indiana Highway Department
Elkhart County, Indiana Historian
Forest County Potawatomi Community
Indiana Landmarks, Northern Regional Office
Miami Tribe of Oklahoma
Michiana Area Council of Governments
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians



Legend

 Approximate Project Boundary



Phase I Archaeological Reconnaissance Survey
CR 18 at CR 13/CR 115 Intersection Improvement
Des. No. 1401749
Elkhart, Elkhart County, Indiana

Figure 1
Project Location



FHWA Project: Des. No. 1401749; Proposed County Road 18 at County Road 13/County Road 115, Intersection Improvement Project in Elkhart County, Indiana

6 messages

James Heimlich <jheimlich@orbisec.com>

Tue, Apr 16, 2019 at 1:20 PM

To: tzeiger@indianalandmarks.org, Ccommissioners@elkhartcounty.com, mbrenneman@maplenet.net, kschumacher@elkcohw.org, jturnwald@macog.com, "Branigin, Susan" <SBranigin@indot.in.gov>, "Miller, Shaun (INDOT)" <smiller@indot.in.gov>, "Kumar, Anuradha" <akumar@indot.in.gov>, "Kelly, Clinton" <CKelly1@indot.in.gov>

Des. No.: 1401749
Project Description: CR 18 at CR 13/CR 115, Intersection Improvement
Location: Concord Township, Elkhart County, Indiana

Elkhart County, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with intersection improvements at the intersection of CR 18 at CR 13/CR 115, Des. No. 1401749.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

Department of Natural Resources, Division of Historic Preservation and Archaeology
Eastern Shawnee Tribe of Oklahoma
Elkhart County, Indiana Commissioners
Elkhart County, Indiana County Council
Elkhart County, Indiana Highway Department
Elkhart County, Indiana Historian
Forest County Potawatomi Community
Indiana Landmarks, Northern Regional Office
Miami Tribe of Oklahoma
Michiana Area Council of Governments
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the letter in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with the proposed design. Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344. Thank you in advance for your input,

James L. Ingermann Heimlich

--

From: Kelly, Clint [<mailto:CKelly1@indot.IN.gov>]
Sent: Tuesday, April 16, 2019 2:46 PM
To: thpo@estoo.net; dhunter@miamination.com; lpappenfort@peoriatribes.com; Matthew.Bussler@pokagonband-nsn.gov; Michael LaRonge
Cc: Kumar, Anuradha; Branigin, Susan; Miller, Shaun (INDOT); Yarian, Matthew; michelle.allen@dot.gov; Novak, Karen; kschumacher@elkcohw.org
Subject: FW: FHWA Project: Des. No. 1401749; Proposed County Road 18 at County Road 13/County Road 115, Intersection Improvement Project in Elkhart County, Indiana

Des. No.: 1401749
Project Description: CR 18 at CR 13/CR 115, Intersection Improvement
Location: Concord Township, Elkhart County, Indiana

Elkhart County, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with intersection improvements at the intersection of CR 18 at CR 13/CR 115, Des. No. 1401749.

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Eastern Shawnee Tribe of Oklahoma
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Forest County Potawatomi Community
Indiana Landmarks, Northern Regional Office
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Please review the letter in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. If we do not receive a response from an invited consulting party in the time allotted, the project will proceed consistent with the proposed design. Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344. Thank you in advance for your input,

Clint Kelly
Historian
Cultural Resources Office
Environmental Services
100 N. Senate Ave., Rm. 642
Indianapolis, IN 46204
Office: (317) 232-1349
Email: ckelly1@indot.in.gov



James Heimlich <jheimlich@orbisec.com>

FHWA Project: Des. No. 1401749; Proposed County Road 18 at County Road 13/County Road 115, Intersection Improvement Project in Elkhart County, Indiana

Todd Zeiger <TZeiger@indianalandmarks.org>

Tue, Apr 16, 2019 at 1:46 PM

To: James Heimlich <jheimlich@orbisec.com>

Thank you for the information about the above listed project and invitation to participate as a consulting party. We do not wish to participate as there appear no historic buildings in the area impacted by the project. We do not wish to receive any further information about the project.

Thank you

Todd

.....

Todd Zeiger

Director, Northern Regional Office

.....

Indiana Landmarks

801 W. Washington

South Bend, Indiana 46601

Ph. 574-232-4534 x 4

Cell: 574-286-5765

www.indianalandmarks.org

Indiana Landmarks revitalizes communities, reconnects us to our heritage, and saves meaningful places.

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Pokégnek Bodéwadmik • Pokagon Band of Potawatomi
Department of Language and Culture

59291 Indian Lake Road • Dowagiac, MI 49047 • www.PokagonBand-nsn.gov
(269) 462-4316 • (269) 782-2499 fax

4/24/2019

Shaun Miller
INDOT
Phone: 317-233-6795
Email: Smiller@indot.in.gov

FW: FHWA Project: Des. No. 1401749; Proposed County Road 18 at County Road 13/County Road 115, Intersection Improvement Project in Elkhart County, Indiana

Dear Shaun:

Migweth for contacting me regarding this project. As THPO, I am responsible for handling Section 106 Consultations on behalf of the tribe. I am writing to inform you that I have reviewed the details for the project referenced above. I have made the determination that this undertaking will have **No Adverse Effect** on any historic, religious, or culturally significant resources to the Pokagon Band of Potawatomi Indians. The proposed work is within 0.5 miles of a recorded Pokagon archaeological site. Since the work is occurring within an existing roadway, a No Adverse Effect determination has been issued.

If any cultural or archaeological resources are uncovered during construction, please stop work and contact me immediately. Should you have any other questions, please don't hesitate to contact me at your earliest convenience.

Sincerely,

Matthew J.N. Bussler
Tribal Historic Preservation Officer
Pokagon Band of Potawatomi Indians
Office: (269) 462-4316
Cell: (269) 519-0838
Matthew.Bussler@Pokagonband-nsn.gov

A proud, compassionate people committed to strengthening our sovereign nation.
A progressive community focused on culture and the most innovative opportunities for all of our citizens.

From: Michael LaRonge [<mailto:Michael.LaRonge@fcpotawatomi-nsn.gov>]

Sent: Tuesday, May 14, 2019 12:30 PM

To: Kelly, Clint <CKelly1@indot.IN.gov>

Subject: RE: FHWA Project: Des. No. 1401749; Proposed County Road 18 at County Road 13/County Road 115, Intersection Improvement Project in Elkhart County, Indiana

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Re: FHWA INDOT Des No. 1401749, CR 18 (intersection with CR 13/ CR 115), Concord Township, Elkhart County, Indiana.

Dear Mr. Kelly,

Pursuant to consultation under Section 106 of the National Historic Preservation Act (1966 as amended) the Forest County Potawatomi as a Federally Recognized Native American Tribe reserves the right to comment on Federal undertakings, as defined under the act. Thank you for your participation in the process.

This response is regarding the projects mentioned above. This project falls within the current geographic area of interest of the Forest County Potawatomi Community. Therefore, the Tribal Historic Preservation Office requests a copy of the archaeological report and SHPO comments related to the project.

Your interest in protecting cultural and historic properties is appreciated. If you have any questions or concerns, please contact me at the phone number or email address listed below.

Respectfully,

Michael LaRonge
Tribal Historic Preservation Officer
Natural Resources Department
Forest County Potawatomi Community
5320 Wensaut Lane
P.O. Box 340
Crandon, Wisconsin 54520
Phone: 715-478-7354
Fax: 715-478-7225
Email: Michael.LaRonge@FCPotawatomi-nsn.gov

From: Kelly, Clint [<mailto:CKelly1@indot.IN.gov>]

Sent: Tuesday, April 16, 2019 2:46 PM

To: thpo@estoo.net; dhunter@miamination.com; lpappenfort@peoriatribe.com; Matthew.Bussler@pokagonband-nsn.gov; Michael LaRonge

Cc: Kumar, Anuradha; Branigin, Susan; Miller, Shaun (INDOT); Yarian, Matthew; michelle.allen@dot.gov;



Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355

Ph: (918) 541-1300 • Fax: (918) 542-7260

www.miamination.com



May 14, 2019

Shaun Miller
Archaeological Team Lead
Cultural Resources Office
Indiana DOT
575 North Pennsylvania Street
Indianapolis, IN 46204

Re: Des. No. 1401749; County Road 18 at County Road 13-County Road 115 Intersection
Improvement, Elkhart County, Indiana – Comments of the Miami Tribe of Oklahoma

Dear Mr. Miller:

Aya, kikwehsitoole – I show you respect. My name is Diane Hunter, and I am the Tribal Historic Preservation Officer for the Federally Recognized Miami Tribe of Oklahoma. In this capacity, I am the Miami Tribe's point of contact for all Section 106 issues.

The Miami Tribe offers no objection to the above-mentioned project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, as this site is within the aboriginal homelands of the Miami Tribe, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-8966 or by email at dhunter@miamination.com to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

Diane Hunter
Tribal Historic Preservation Officer



May 9, 2019

James L. Ingermann Heimlich
Archaeologist/Historian
Orbis Environmental Consulting
P.O. Box 10235
South Bend, Indiana 46680

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration ("FHWA")

Re: Early coordination letter for the proposed County Road 18 at County Road
13/County Road 115 intersection improvement project, Concord
Township, Elkhart County, Indiana (Des. No. 1401749; DHPA No. 23797)

Dear Mr. Heimlich:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO staff" or "INDNR-DHPA") has reviewed INDOT's early coordination letter dated April 12, 2019, which we received on April 17, 2019.

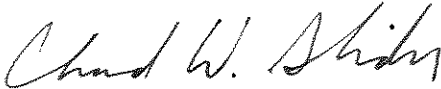
We are not aware of anyone who should be invited to become a consulting party for the purposes of the review of this project under Section 106, beyond those whom INDOT already has invited. In your next regular correspondence on this project, please advise us as to which of the invited consulting parties has accepted the invitation.

It is our understanding that additional information regarding historic and archaeological resources in the area of potential effects will be forthcoming. Once the indicated information is received, the Indiana SHPO will resume identification and evaluation procedures for this project. Please keep in mind that additional information may be requested in the future.

The structures reviewer on the Indiana SHPO staff for this project is Danielle Kauffmann, and the archaeological reviewer is Wade Tharp. However, if you have a question about our comments or about the review process, we ask that you initially direct your question to a staff member of the INDOT Cultural Resources Office who has been assigned to this project.

In all future correspondence regarding the proposed County Road 18 at County Road 13/County Road 115 intersection improvement project, located in Concord Township, Elkhart County (Des. No. 1401749), please refer to DHPA No. 23797.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Anuradha Kumar, INDOT
Susan Branigin, INDOT
Shaun Miller, INDOT
Shirley Clark, INDOT
James Heimlich, Orbis Environmental Consulting
Wade Tharp, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

June 7, 2019

This letter was sent to the listed parties.

RE: Proposed County Road 18 at County Road 13/County Road 115,
Intersection Improvement Project in Elkhart, Concord Township,
Elkhart County, Indiana, Des. No. 1401749

Dear Consulting Party (see attached list),

Elkhart County, with funding from Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the intersection improvements at the intersection of County Road (CR) 18 at CR 13/CR 115, Des. No. 1401749. Orbis Environmental Consulting and Troyer Group are under contract with Elkhart County to advance the environmental documentation for the referenced project.

This letter is part of the Section 106 review process for this project. A Section 106 early coordination letter was distributed on April 16, 2019.

The proposed undertaking is on CR 18 from the intersection of CR 18 and CR 13 and CR 115 in Elkhart County, Indiana. It is within Concord Township, Elkhart County, Indiana USGS Topographic Quadrangle, in Section 14, Township 37N, Range 5E.

Currently there are two separate T-intersections, one at CR 18 with 115, with CR 115 being stop-controlled, and CR 18 being free-flow. The other is at CR 18 with CR 13, both roads being stop-controlled. Improvement alternatives being considered include a "peanut shaped" roundabout that would act as a four-legged, single-lane roundabout, elongated to intersect with both CR 13 and CR 115. Ingress and egress access to all adjacent residential properties will be maintained by relocating and extending driveways where necessary. No pedestrian improvements are included with the project. Much of the improved roadway surface will be curbed and guttered and water will be detained before being outlet into the nearby Elkhart River. It is estimated that between 0.5 and 1.0 acres of additional permanent right-of-way will be necessary to accommodate the proposed improvements. Up to 0.2 acres of temporary right-of-way may also be necessary to accommodate incidental construction activities.

The purpose of the proposed project is to increase the operational safety and operational efficiency at the intersection of CR 18, CR 13 and CR 115. The project is needed because the unusual intersection geometry has resulted in frequent accidents. In a span of 29 months between 2014 and 2016, 17 crashes occurred at the intersection, including eight rear-end collisions. The causes of the rear-end collisions were attributed to excessive speed and inadequate advance warning signage. The project is also needed because the southern

approach of the existing intersection experiences a Level of Service (LOS) D, which represent a very poor operating condition causing long user delays. Left untreated, this intersection approach is expected to operate at a LOS E by year 2035.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have accepted consulting party status are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <http://www.achp.gov/citizensguide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP.

With regards to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the project area. As a result of these efforts, no sites were recommended as eligible for listing in the NRHP and no further work is recommended.

The Historic Property Report and Archaeology Report (Tribes only) are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard copy of this material, please respond to this email with your request within seven (7) days.

Please review the information and comment within thirty (30) calendar days of receipt.

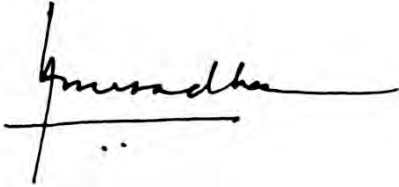
For questions concerning specific project details, you may contact James Heimlich of Orbis Environmental Consulting at (574) 850-6003 or jheimlich@orbisec.com. All future responses regarding the proposed project should be forwarded to Orbis Environmental Consulting at the following address:

James L. Ingermann Heimlich
Archaeologist/Historian
Orbis Environmental Consulting
P.O. Box 10235
South Bend, IN 46680

jheimlich@orbisec.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

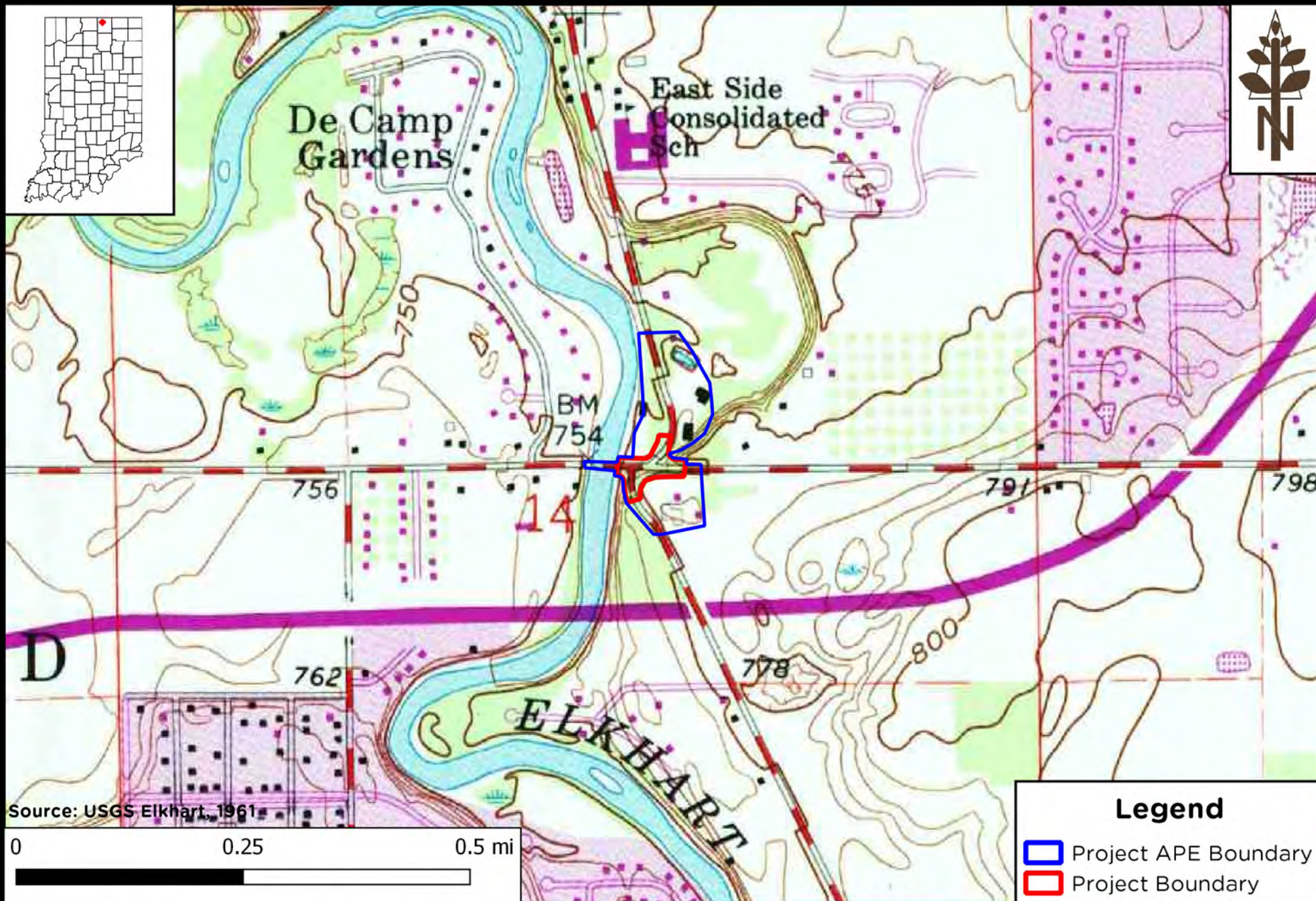
Sincerely,

A handwritten signature in black ink, appearing to read "Anuradha", written over a horizontal line.

Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:
Project Location Figure

Distribution List:
Department of Natural Resources, Division of Historic Preservation and Archaeology
Forest County Potawatomi Community



CR 18 at CR 13/CR 115 Intersection Improvement
De. No. 1401749
Troyer Group
Elkhart, Elkhart County, Indiana

Figure 1
Project Location
Project #1809002



FHWA Project: Des. No. 1401749; Proposed County Road 18 at County Road 13/County Road 115, Intersection Improvement Project in Elkhart County, Indiana

1 message

James Heimlich <jheimlich@orbisec.com> Fri, Jun 7, 2019 at 1:21 PM
To: "Slider, Chad (DNR)" <cslider@dnr.in.gov>, "Kelly, Clinton" <CKelly1@indot.in.gov>, "Kumar, Anuradha" <akumar@indot.in.gov>, "Branigin, Susan" <SBranigin@indot.in.gov>, "Korzeniewski, Patricia J" <PKorzeniewski@indot.in.gov>, "Miller, Shaun (INDOT)" <smiller@indot.in.gov>, CJ Cunningham <cjc@troyergroup.com>, J Ryan Duddleson <ryan@orbisec.com>

Subject of email: FHWA Project: Des. No. 1401749; Proposed County Road 18 at County Road 13/County Road 115, Intersection Improvement Project in Elkhart County, Indiana

Des. No.: 1401749

Project Description: CR 18 at CR/CR 115, Intersection Improvement

Location: Concord Township, Elkhart County, Indiana

Elkhart County, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with intersection improvements at the intersection of CR 18 at CR 13/CR 115, Des. No. 1401749. The Section 106 Early Coordination Letter for this project was originally distributed on April 16, 2019.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report and an Archaeology Report have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

James L. Ingermann Heimlich

--



James Ingermann Heimlich
Historian-Principal Investigator/Archaeologist

Phone (574) 635-1338

Cell (574) 850-6003

Address P.O. Box 10235, South Bend, Indiana 46680

Email jheimlich@orbisec.com

www.orbisec.com



Virus-free. www.avast.com



Des. No. 1401749, County Road 18 at County Road 13/County Road 115 intersection improvement project, Concord Township, Elkhart County, Indiana

4 messages

Korzeniewski, Patricia J <PKorzeniewski@indot.in.gov> Fri, Jun 7, 2019 at 2:40 PM
To: "thpo@estoo.net" <thpo@estoo.net>, "dhunter@miamination.com" <dhunter@miamination.com>, "lpappenfort@peoriatribes.com" <lpappenfort@peoriatribes.com>, "Matthew.Bussler@pokagonband-nsn.gov" <Matthew.Bussler@pokagonband-nsn.gov>, "michael.laronge@fcpotawatomi-nsn.gov" <michael.laronge@fcpotawatomi-nsn.gov>
Cc: "Kelly, Clint" <CKelly1@indot.in.gov>, "patricia.korzeniewski@valpo.edu" <patricia.korzeniewski@valpo.edu>, "Miller, Shaun (INDOT)" <smiller@indot.in.gov>, "michelle.allen@dot.gov" <michelle.allen@dot.gov>, James Heimlich <jheimlich@orbisec.com>

Subject of email: FHWA Project: Des. No. 1401749; Proposed County Road 18 at County Road 13/County Road 115, Intersection Improvement Project in Elkhart County, Indiana

Des. No.: 1401749

Project Description: CR 18 at CR/CR 115, Intersection Improvement

Location: Concord Township, Elkhart County, Indiana

Elkhart County, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with intersection improvements at the intersection of CR 18 at CR 13/CR 115, Des. No. 1401749. The Section 106 Early Coordination Letter for this project was originally distributed on April 16, 2019.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report and an Archaeology Report have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Patricia Jo Korzeniewski

Archaeologist and Environmental Manager

INDOT, Cultural Resources Office

PKorzeniewski@indot.in.gov

(317) 233-2093

Korzeniewski, Patricia J <PKorzeniewski@indot.in.gov>
To: James Heimlich <jheimlich@orbisec.com>

Tue, Jul 16, 2019 at 8:01 AM



Pokégnek Bodéwadmik • Pokagon Band of Potawatomi
Department of Language and Culture

59291 Indian Lake Road • Dowagiac, MI 49047 • www.PokagonBand-nsn.gov
(269) 462-4316 • (269) 782-2499 fax

6/13/2019

Shaun Miller
INDOT
Cultural Resources Office
Archaeology Team Lead
Phone: 317-233-6795
Email: Smiller@indot.in.gov

**Des. No. 1401749, County Road 18 at County Road 13/County Road 115 intersection
improvement project, Concord Township, Elkhart County, Indiana**

Dear Responsible Party:

Migweth for contacting me regarding this project. As THPO, I am responsible for handling Section 106 Consultations on behalf of the tribe. I am writing to inform you that I have reviewed the details for the project referenced above. The proposed work is occurring within .35 miles of a known historic site that is recorded in the Pokagon Band Registry of Historic Places. I have made the determination that this undertaking will have **No Adverse Effect** on any historic, religious, or culturally significant resources to the Pokagon Band of Potawatomi Indians.

If any cultural or archaeological resources are uncovered during construction, please stop work and contact me immediately. Should you have any other questions, please don't hesitate to contact me at your earliest convenience.

Sincerely,

Matthew J.N. Bussler
Tribal Historic Preservation Officer
Pokagon Band of Potawatomi Indians
Office: (269) 462-4316
Cell: (269) 519-0838
Matthew.Bussler@Pokagonband-nsn.gov

A proud, compassionate people committed to strengthening our sovereign nation.
A progressive community focused on culture and the most innovative opportunities for all of our citizens.

Korzeniewski, Patricia J <PKorzeniewski@indot.in.gov>
To: James Heimlich <jheimlich@orbisec.com>
Cc: "Kelly, Clint" <CKelly1@indot.in.gov>

Tue, Jul 16, 2019 at 8:01 AM

Good Morning James,

I wanted to forward you the response from Michael LaRonge of the Forest County Potawatomi Community, from the Archaeology Report and the Historic Property Report.

RE: FHWA Project: Des. No. 1401749 Proposed CR-18 at CR13/CR-115 intersection improvement, Elkhart, Elkhart County, Indiana.

Dear Ms. Korzeniewski,

Pursuant to consultation under Section 106 of the National Historic Preservation Act (1966 as amended) the Forest County Potawatomi Community, a Federally Recognized Native American Tribe, reserves the right to comment on Federal undertakings, as defined under the act.

Thank you for providing additional information regarding this project. Based on information you provided it does not appear that the proposed work will impact any historic properties of concern to the Tribe. Therefore, the Tribal Historic Preservation office, on behalf of the Tribe, is pleased to offer a finding of no historic properties affected, with two conditions. First should the SHPO finding differ the Tribe reserves the right to reconsider based on new evidence. Second, In the event that human remains or archaeological materials are exposed as a result of project activities then work must halt and the Tribe must be included in any further discussion regarding treatment and disposition of the find prior to its removal.

Your interest in protecting cultural and historic properties is appreciated. If you have any questions or concerns, please contact me at the email address or phone number listed below.

Respectfully,

Michael LaRonge
Tribal Historic Preservation Officer
Natural Resources Department
Forest County Potawatomi Community
5320 Wensaut Lane
P.O. Box 340
Crandon, Wisconsin 54520
Phone: 715-478-7354



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



July 9, 2019

James L. Ingermann Heimlich
Archaeologist/Historian
Orbis Environmental Consulting
Post Office Box 10235
South Bend, Indiana 46680

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration ("FHWA")

Re: Historic property short report (Heimlich, 6/6/2019, and Indiana
archaeological short report (Giedd, 05/23/2019), for the proposed County
Road 18 at County Road 13/County Road 115 Intersection Improvement
Project, Concord Township, Elkhart County, Indiana (Des. No. 1401749;
DHPA No. 23797)

Dear Mr. Heimlich:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO staff" or "INDNR-DHPA") has reviewed Orbis Environmental Consulting's June 8, 2019, review request submittal form, with the aforementioned enclosures, all of which we received on June 11, 2019.

In the review request submittal form and INDOT's letter, the project description states that "improvement alternatives being considered include a 'peanut-shaped' roundabout..." This wording implies that other alternatives are being considered, yet, no others are discussed. Is this the preferred alternative chosen for the project, or are there indeed other alternatives? Furthermore, INDOT's letter and the review request submittal form also indicate that up to 1 acre of permanent and up to .2 acres of temporary right-of-way may be needed for the proposed project. If there are any other alternatives being considered, will this affect the amount of right-of-way needed?

If the "peanut-shaped" roundabout alternative is indeed the chosen alternative for this project, the area of potential effects ("APE") proposed in the historic property short report ("HPSR"; Heimlich, 6/6/2019) appears to be of appropriate size to encompass the geographic area in which foreseeable effects of this project on historic properties could occur. However, other alternatives may result in different APEs depending on its size and scope.

Moreover, in regards to buildings and structures, for the purpose of the Section 106 review of this undertaking, we agree that the "peanut-shaped" roundabout alternative contains no above-ground properties listed in or eligible for inclusion in the National Register of Historic Places ("NRHP").

In terms of potential impact on archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the portions of the proposed project area; and we concur with the opinion of the archaeologist,

as expressed in the Indiana archaeological short report (Giedd, 05/23/2019), that no further archaeological investigations appear necessary at the proposed project area.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The structures reviewer on the Indiana SHPO staff for this project is Danielle Kauffmann, and the archaeological reviewer is Wade Tharp. However, if you have a question about our comments or about the review process, we ask that you initially direct your question to a staff member of the INDOT Cultural Resources Office who has been assigned to this project.

In all future correspondence regarding the proposed County Road 18 at County Road 13/County Road 115 intersection improvement project, located in Concord Township, Elkhart County (Des. No. 1401749), please refer to DHPA No. 23797.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:WTT:wtt

emc: Joyce Newland, FHWA
Anuradha Kumar, INDOT
Susan Branigin, INDOT
Shaun Miller, INDOT
Shirley Clark, INDOT
James Heimlich, Orbis Environmental Consulting
Danielle Kauffmann, INDNR-DHPA
Wade Tharp, INDNR-DHPA



FWHA Project Des. No. 1401749, DHPA No. 23797, Proposed CR 18 at CR 13/CR 115 Intersection Improvement Project, in Concord Township, Elkhart County, Indiana.

1 message

James Heimlich <jheimlich@orbisec.com>

Tue, Jul 23, 2019 at 3:15 PM

To: WTharp1@dnr.in.gov, "Kumar, Anuradha" <akumar@indot.in.gov>, "Miller, Shaun (INDOT)" <smiller@indot.in.gov>, "Newland, Joyce (FHWA)" <Joyce.Newland@dot.gov>, "Branigin, Susan" <SBranigin@indot.in.gov>, "Kelly, Clinton" <CKelly1@indot.in.gov>, CJ Cunningham <cjc@troyergroup.com>, J Ryan Duddleson <ryan@orbisec.com>

Dear Mr. Tharp

I was made aware that the subject line for my previous email was incorrect. Please disregard my previous email and accept this corrected version. I apologize for any confusion this may have caused.

This letter is part of the Section 106 review process for this project. A Section 106 early coordination letter was distributed on June 8, 2019. We received your letter dated July 9, 2019. In that letter you state the following:

In the review request submittal from and INDOT's letter, the project description states that

"improvement alternatives being considered include a 'peanut-shaped roundabout...' This wording implies that other alternatives area being considered, yet, no others are discussed. Is this the preferred alternative chosen for the project, or are there indeed other alternatives? Furthermore, INDOT's letter and the review request submittal form also indicate that up to 1 acre of permanent and up to .2 acres of temporary right-of-way may be needed for the proposed project. If there are any other alternatives being considered, will this affect the amount of right-of-way needed?

To answer your inquires, the "peanut-shaped roundabout" is the preferred alternative at this time. This is based upon the abbreviated Engineers report, which indicates that due to the number of vehicular accidents, the limited sight distance, and the unusual road configuration, that this alternative is the only one that will serve the project need. There are no other alternatives being considered for this reason. Since there are no other alternatives being considered at this time, there is no known additional amounts of right-of-way needed other than what was stated for the preferred alternative.

I believe this addresses your comments from your letter.

Sincerely,

James Heimlich

--



James Ingermann Heimlich
Historian-Principal Investigator/Archaeologist

Phone (574) 635-1338

Cell (574) 850-6003

Address P.O. Box 10235, South Bend, Indiana 46680

Email jheimlich@orbisec.com

www.orbisec.com

January 27, 2020

This letter was sent to the listed parties.

RE: Proposed County Road 18 at County Road 13/County Road 115,
Intersection Improvement Project in Elkhart, Concord Township,
Elkhart County, Indiana, Des. No. 1401749

Dear Consulting Party,

Elkhart County, with funding from Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the intersection improvements at the intersection of County Road (CR) 18 at CR 13/CR 115, Des. No. 1401749. Orbis Environmental Consulting and Troyer Group are under contract with Elkhart County to advance the environmental documentation for the referenced project.

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter was distributed on April 16, 2019. In addition, a letter distributed on June 7, 2019 notified consulting parties that a historic property short report and archaeology short report were available for review and comment.

The proposed undertaking is on CR 18 from the intersection of CR 18 and CR 13 and CR 115 in Elkhart County, Indiana. It is within Concord Township, Elkhart, Indiana USGS Topographic Quadrangle, in Section 14, Township 37N, Range 5E (Appendix A). The project proposes intersection improvements in the aforementioned intersection.

Currently there are two separate T-intersections, one at CR 18 with 115, with CR 115 being stop-controlled, and CR 18 being free-flow. The other is at CR 18 with CR 13, both roads being stop-controlled. Improvement alternatives being considered include a “peanut shaped” roundabout that would act as a four-legged, single-lane roundabout, elongated to intersect with both CR 13 and CR 115. Ingress and egress access to all adjacent residential properties will be maintained by relocating and extending driveways where necessary. No pedestrian improvements are included with the project. Much of the improved roadway surface will be curbed and guttered and water will be detained before being outlet into the nearby Elkhart River. It is estimated that 0.47 acres of additional permanent right-of-way will be necessary to accommodate the proposed improvements. Up to 0.65 acres of temporary right-of-way may also be necessary to accommodate incidental construction activities.

The purpose of the proposed project is to increase the operational safety and operational efficiency at the intersection of CR 18, CR 13 and CR 115. The project is needed because the unusual intersection geometry has resulted in frequent accidents. In a span of 29 months between 2014 and 2016, 17 crashes occurred at the intersection, including eight rear-end collisions. The causes of the rear-end collisions were attributed to excessive speed and inadequate advance warning signage. The project is also needed because the southern approach of the existing intersection experiences a Level of Service (LOS) D, which represent a very poor operating condition causing long user delays. Left untreated, this intersection approach is expected to operate at a LOS E by year 2035.

Since the last correspondence, the project area has expanded, requiring a further archaeological and historic structures survey work. Changes to the project area involve an increase of road work. This increased road work entails a realignment of CR 18 and CR 115 in order to have it align with the proposed intersection improvements. Along with the road realignment there is a proposed increase of temporary and permanent right-of-way on the southwest side of the intersection, along CR 115 and along CR 18 on the southeast and northeast sides of the road (Appendix B).

Orbis Environmental Consulting through Troyer Group is under contract with Elkhart County to advance the environmental documentation for the referenced project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE was increased due to the expansion of the project area. The APE is generally one property deep and established through view shed from the project boundary limits. The APE was expanded along the northeast and eastern end of CR 18 to include two additional other above-ground structures and the view shed from the project area. It was also expanded along the southwestern and southern end of CR 115 to include one additional above-ground structure and the view shed from the project area (Appendix C and Appendix D). The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. Results of the additional survey are reported here. The expanded APE resulted in three additional above-ground structures being evaluated as warranting a "contributing" rating. An updated table and photo location of the above-ground structures in the APE can be found in Appendix E. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP.

With regard to the archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards surveyed the expanded project area and identified no sites within the project area. As a result of these efforts, no sites were recommended as eligible for listing in the NRHP and no further work is recommended.

The Historic Property Short Report and the Archaeology Short Report (Tribes only) are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard-copy of this material, please respond to this email with your request within seven (7) days.

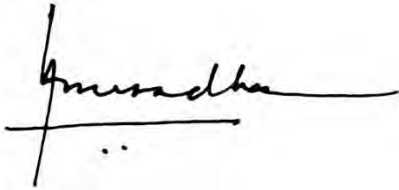
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact James Heimlich of Orbis Environmental Consulting at (574) 850-6003 or jheimlich@orbisec.com. All future responses regarding the proposed project should be forwarded to Orbis Environmental Consulting at the following address:

James L. Ingermann Heimlich
Archaeologist/Historian
Orbis Environmental Consulting
P.O. Box 10235
South Bend, IN 46680

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,

A handwritten signature in black ink, appearing to read "Anuradha", written over a horizontal line. There are two small dots below the line to the right of the signature.

Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:

Appendix A: Project Location Figure
Appendix B: Proposed Intersection Improvements Figure
Appendix C: Project APE

Distribution List:

Indiana State Historic Preservation Officer (IN SHPO)
Forest County Potawatomi Community
Miami Tribe of Oklahoma
Pokagon Band of Potawatomi Indians

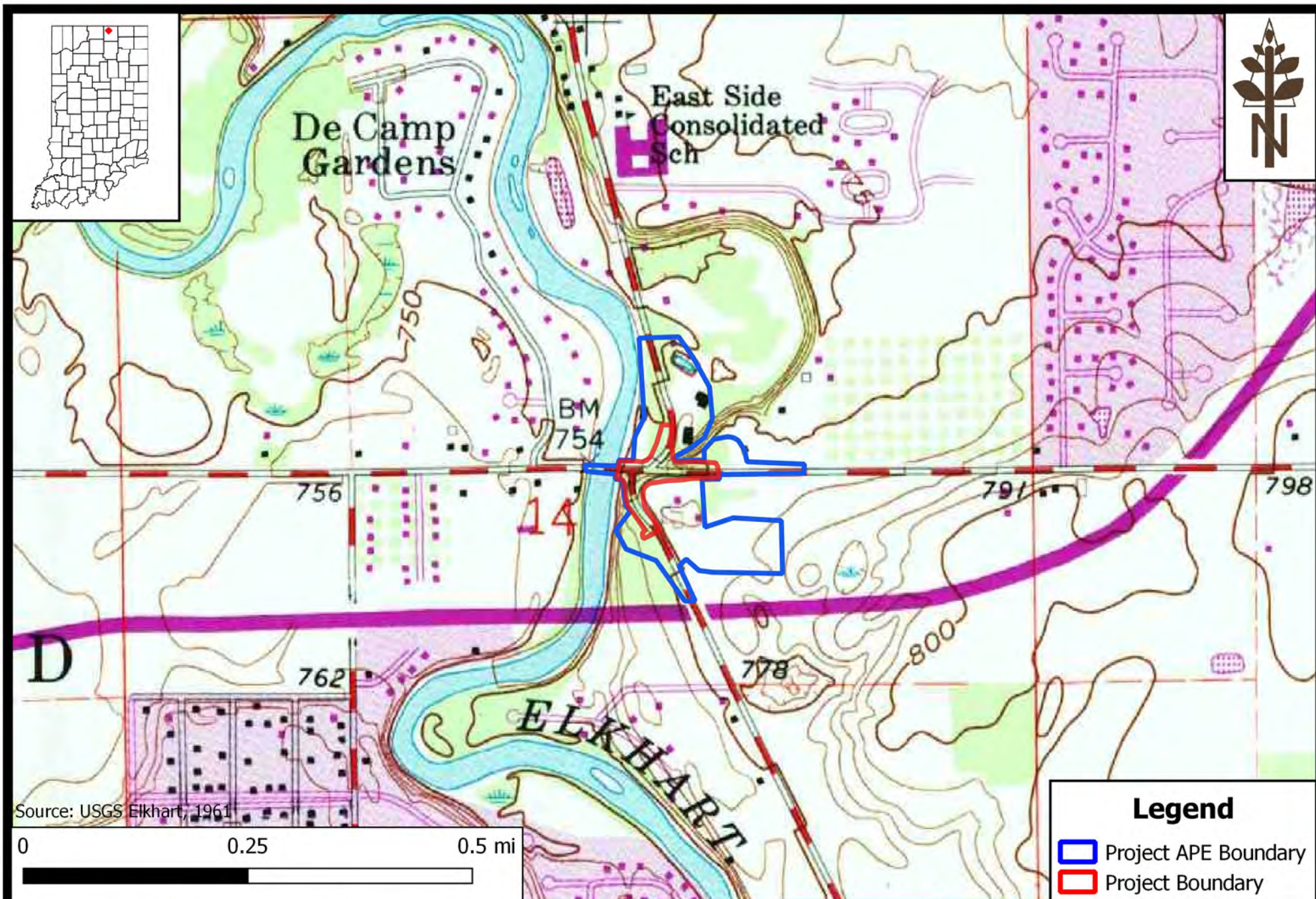
Appendix A

Project Location

Historic Property Short Report Addendum

Proposed CR 18 at CR 13/CR 115,
Intersection Improvement Project
Concord Township, Elkhart County,
Indiana





CR 18 at CR 13/CR 115 Intersection Improvement
 De. No. 1401749
 Troyer Group
 Elkhart, Elkhart County, Indiana

Figure 1
 Project Location
 Project #1809002

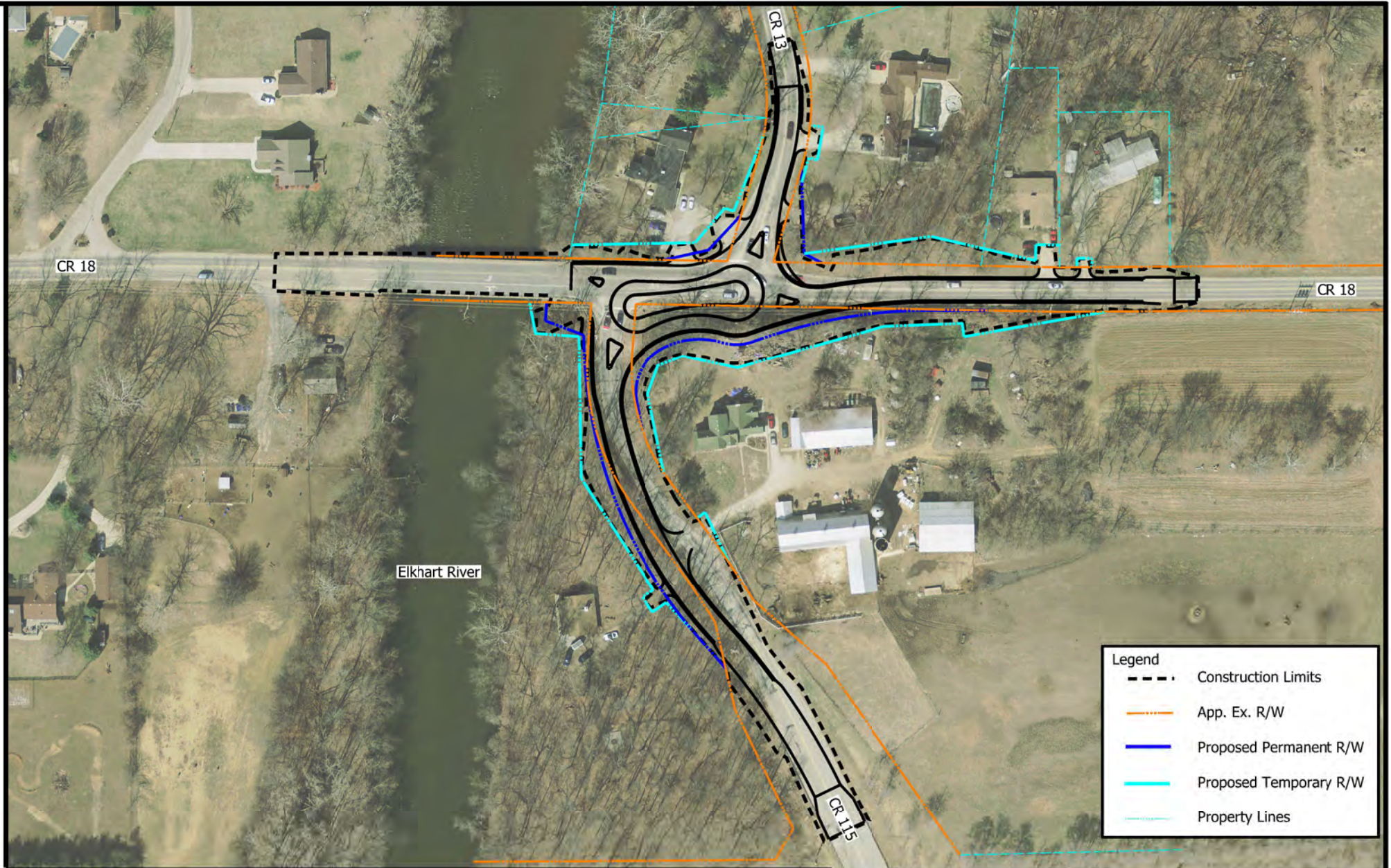
Appendix B

Proposed Intersection Improvements Figure

Section 106 Report Distribution Letter

Proposed CR 18 at CR 13/CR 115,
Intersection Improvement Project
Concord Township, Elkhart County,
Indiana





Legend

- Construction Limits
- App. Ex. R/W
- Proposed Permanent R/W
- Proposed Temporary R/W
- Property Lines

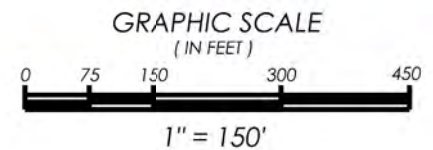


PROJECT
CR 18 at CR 13/CR 115
Intersection Improvement
Elkhart County, IN

DES. NO. 1401749

SHEET
Figure 2

SCALE: 1"=150'



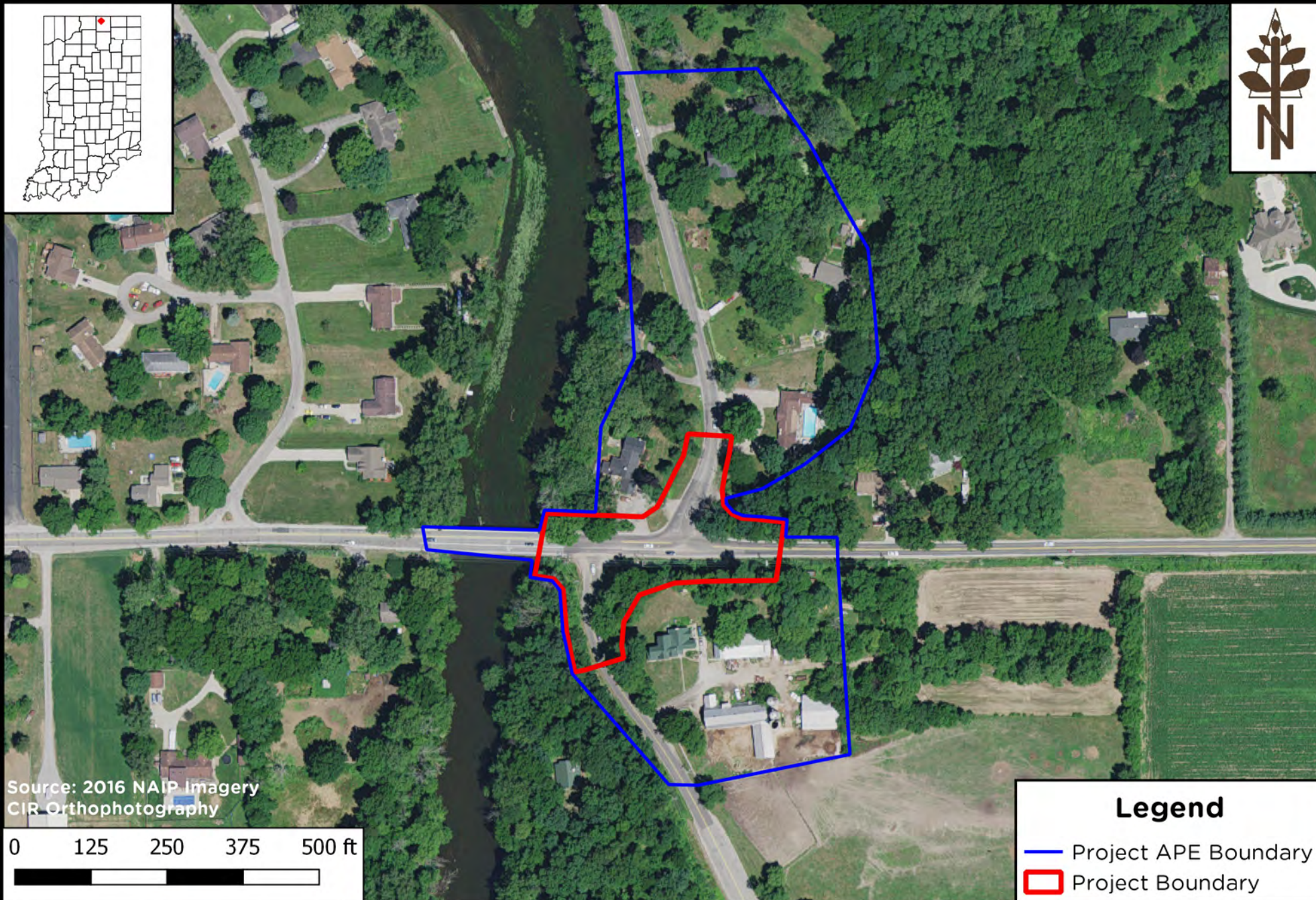
Appendix C

Past Project APE and Revised Project APE

Section 106 Report Distribution Letter

Proposed CR 18 at CR 13/CR 115,
Intersection Improvement Project
Concord Township, Elkhart County,
Indiana





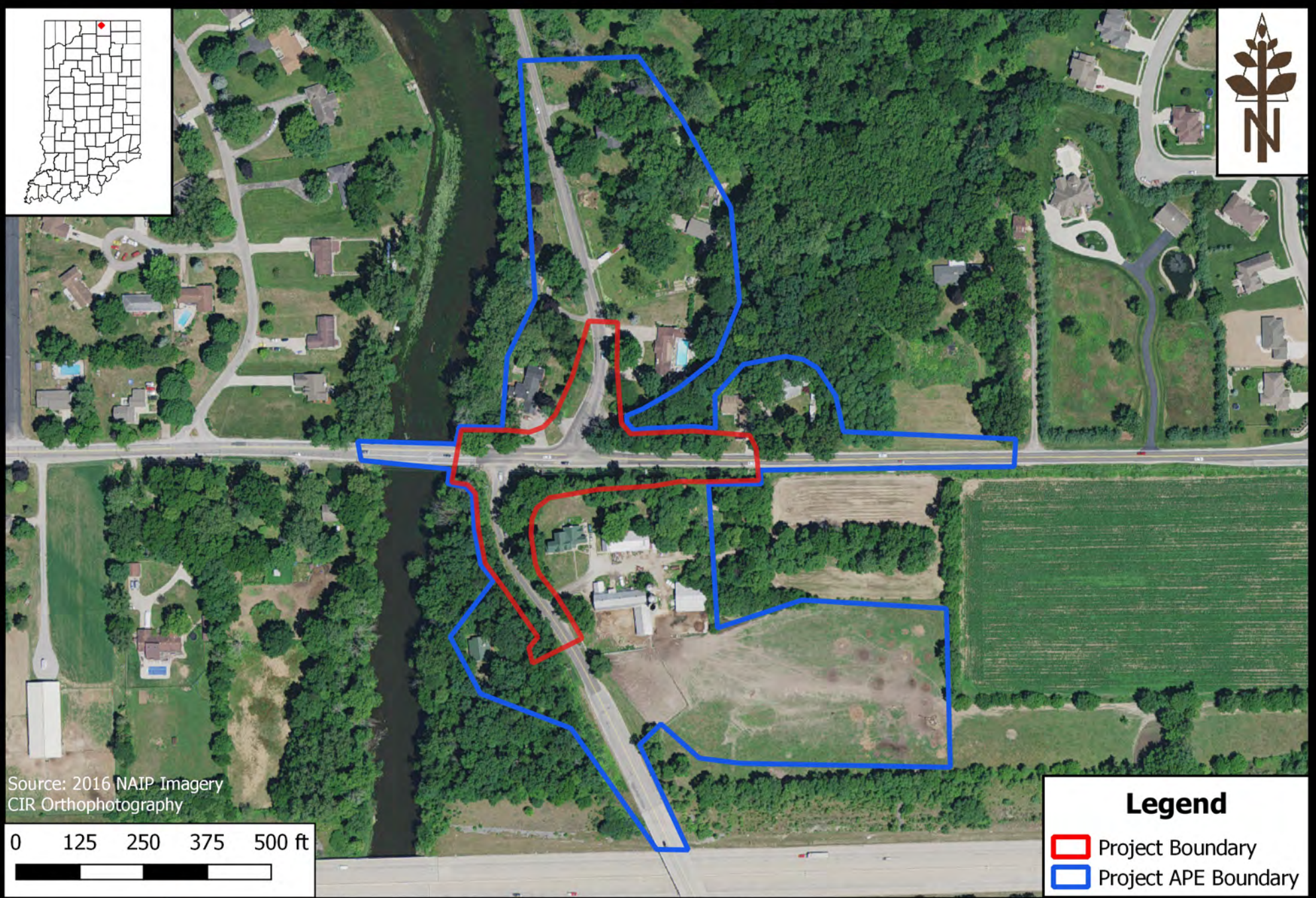
Legend

- Project APE Boundary
- Project Boundary

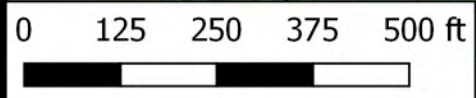


CR 18 at CR 13/CR 115 Intersection Improvement
De. No. 1401749
Troyer Group
Elkhart, Elkhart County, Indiana

Figure 2
Original Proposed APE



Source: 2016 NAIP Imagery
CIR Orthophotography



Legend

- Project Boundary
- Project APE Boundary



CR 18 at CR 13/CR 115 Intersection Improvement
De. No. 1401749
Troyer Group
Elkhart, Elkhart County, Indiana

Figure 2
Revised Proposed APE
Project #1809002



FHWA Project: Des. No. 1401749; Proposed County Road 18 at County Road 13/County Road 115, Intersection Improvement Project in Elkhart County, Indiana

1 message

James Heimlich <jheimlich@orbisec.com>

Mon, Jan 27, 2020 at 3:08 PM

To: "Slider, Chad (DNR)" <cslider@dnr.in.gov>, "Kelly, Clinton" <CKelly1@indot.in.gov>

Des. No.: 1401749

Project Description: CR 18 at CR 13/CR 115, Intersection Improvement

Location: Concord Township, Elkhart County, Indiana

Elkhart County, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with intersection improvements at the intersection of CR 18 at CR 13/CR 115, Des. No. 1401749. The Section 106 Early Coordination Letter for this project was originally distributed on April 16, 2019.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report Addendum, an Archaeology Report, and the Report Distribution Letter have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

James L. Ingermann Heimlich

--



James Ingermann Heimlich
Historian-Principal Investigator/Archaeologist

Phone (574) 635-1338

Cell (574) 850-6003

Address P.O. Box 10235, South Bend, Indiana 46680

Email jheimlich@orbisec.com

www.orbisec.com



FHWA Project: Des. No. 1401749; Proposed County Road 18 at County Road 13/County Road 115, Intersection Improvement Project in Elkhart County, Indiana

1 message

Kelly, Clint <CKelly1@indot.in.gov> Mon, Jan 27, 2020 at 3:21 PM
To: "michael.laronge@fcpotawatomi-nsn.gov" <michael.laronge@fcpotawatomi-nsn.gov>, "dhunter@miamination.com" <dhunter@miamination.com>, "Matthew.Bussler@pokagonband-nsn.gov" <Matthew.Bussler@pokagonband-nsn.gov>
Cc: "Kumar, Anuradha" <akumar@indot.in.gov>, "Branigin, Susan" <SBranigin@indot.in.gov>, "Miller, Shaun (INDOT)" <smiller@indot.in.gov>, "Korzeniewski, Patricia J" <PKorzeniewski@indot.in.gov>, "michelle.allen@dot.gov" <michelle.allen@dot.gov>, "Yarian, Matthew" <MYarian@indot.in.gov>, "Novak, Karen" <KNovak@indot.in.gov>, James Heimlich <jheimlich@orbisec.com>

Des. No.: 1401749

Project Description: CR 18 at CR 13/CR 115, Intersection Improvement

Location: Concord Township, Elkhart County, Indiana

Elkhart County, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with intersection improvements at the intersection of CR 18 at CR 13/CR 115, Des. No. 1401749. The Section 106 Early Coordination Letter for this project was originally distributed on April 16, 2019.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report Addendum, an Archaeology Report, and the Report Distribution Letter have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Clint Kelly
Historian

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Rm. 642

Indianapolis, IN 46204

Office: (317) 232-1349

Email: ckelly1@indot.in.gov





March 2, 2020

James L. Ingermann Heimlich
Archaeologist/Historian
Orbis Environmental Consulting
Post Office Box 10235
South Bend, Indiana 46680

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration ("FHWA")

Re: Addendum Indiana archaeological short report (Giedd and Duddleson, 12/09/2019), and addendum historic property short report (Heimlich, 1/27/2020), for the proposed County Road 18 at County Road 13/County Road 115 Intersection Improvement Project, Concord Township, Elkhart County, Indiana (Des. No. 1401749; DHPA No. 23797)

Dear Mr. Heimlich:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO staff" or "INDNR-DHPA") has reviewed Orbis Environmental Consulting's January 27, 2020, review request submittal form, with the aforementioned enclosures, all of which we received on January 31, 2020.

The area of potential effects ("APE") proposed in the addendum historic property short report ("HPSR"; Heimlich, 1/27/2020) appears to be of appropriate size to encompass the added geographic area in which foreseeable effects of this project on historic properties could occur.

In regard to buildings and structures, for the purpose of the Section 106 review of this undertaking, we agree that there are no above-ground properties listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the expanded area of potential effects.

Additionally, in terms of potential impact on archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the portions of the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the addendum Indiana archaeological short report (Giedd and Duddleson, 12/09/2019), that no further archaeological investigations appear necessary at the proposed project area.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

Unless another consulting party raises concerns about the conclusions of the HPSR and addendum archaeology report, it might now be time to ask INDOT for a finding.

The structures reviewer on the Indiana SHPO staff for this project is Danielle Kauffmann, and the archaeological reviewer is Wade Tharp. However, if you have a question about our comments or about the review process, we ask that you initially direct your question to a staff member of the INDOT Cultural Resources Office who has been assigned to this project.

In all future correspondence regarding the proposed County Road 18 at County Road 13/County Road 115 intersection improvement project, located in Concord Township, Elkhart County (Des. No. 1401749), please refer to DHPA No. 23797.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:WTT:wt

emc: Kari Carmany-George, FHWA
Anuradha Kumar, INDOT
Susan Branigin, INDOT
Shaun Miller, INDOT
Shirley Clark, INDOT
James Heimlich, Orbis Environmental Consulting
Danielle Kauffmann, INDNR-DHPA
Wade T. Tharp, INDNR-DHPA



Pokégnek Bodéwadmik • Pokagon Band of Potawatomi
Department of Language and Culture

59291 Indian Lake Road • Dowagiac, MI 49047 • www.PokagonBand-nsn.gov
(269) 462-4316 • (269) 782-2499 fax

1/28/2020

Shaun Miller
INDOT
Phone: 317-233-6795
Email: SMiller@indot.in.gov

FHWA Project: Des. No. 1401749; Proposed County Road 18 at County Road 13/County Road 115, Intersection Improvement Project in Elkhart County, Indiana

Dear Responsible Party:

Migweth for contacting me regarding this project. As THPO, I am responsible for handling Section 106 Consultations on behalf of the tribe. I am writing to inform you that I have reviewed the details for the project referenced above. The proposed work is occurring within a mile of a known historic site that is recorded in the Pokagon Band Registry of Historic Places. I have made the determination that this undertaking will have **No Adverse Effect** on any historic, religious, or culturally significant resources to the Pokagon Band of Potawatomi Indians.

If any cultural or archaeological resources are uncovered during construction, please stop work and contact me immediately. Should you have any other questions, please don't hesitate to contact me at your earliest convenience.

Sincerely,

Matthew J.N. Bussler
Tribal Historic Preservation Officer
Pokagon Band of Potawatomi Indians
Office: (269) 462-4316
Cell: (269) 519-0838
Matthew.Bussler@Pokagonband-nsn.gov

A proud, compassionate people committed to strengthening our sovereign nation.
A progressive community focused on culture and the most innovative opportunities for all of our citizens.

FHWA Documentation of Section 106 Findings of No Historic Properties Affected

Proposed CR 18 at CR 13/CR 115, Intersection
Improvement Project

Concord Township, Elkhart County, Indiana





Photo 1. Northern end of project area on CR 13, facing north.



Photo 2. Northern end of project area on CR 13, facing south.



Historic Property Short Report for the Proposed CR 18 at CR 13/CR
115, Intersection Improvement Project
Des. No. 1401749
Elkhart, Concord Township, Elkhart County, Indiana

Photographic Documentation
11/14/2018



Photo 3. Eastern end of project area on CR 18, facing east.



Photo 4. Western end of project area on CR 18, facing west.



Historic Property Short Report for the Proposed CR 18 at CR 13/CR
115, Intersection Improvement Project
Des. No. 1401749
Elkhart, Concord Township, Elkhart County, Indiana

Photographic Documentation
11/14/2018
1809002



Photo 5. Western end of project area on CR 18, facing east.



Photo 6. Southern end of project area on CR 115, facing south.



Historic Property Short Report for the Proposed CR 18 at CR 13/CR
115, Intersection Improvement Project
Des. No. 1401749
Elkhart, Concord Township, Elkhart County, Indiana

Photographic Documentation
11/14/2018
1809002



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



May 11, 2020

James L. Ingermann Heimlich
Archaeologist/Historian
Orbis Environmental Consulting
P.O. Box 10235
South Bend, Indiana 46680

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration (“FHWA”)

Re: Indiana Department of Transportation’s finding of “no historic properties affected” on
behalf of the Federal Highway Administration for the proposed County Road 18 at County
Road 13/County Road 115 Intersection Improvement Project, Concord Township, Elkhart
County, Indiana (Des. No. 1401749; DHPA No. 23797)

Dear Mr. Heimlich:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO staff” or “INDNR-DHPA”) has reviewed your April 27, 2020, review request submittal form, with the aforementioned finding and supporting documentation, all of which we received on April 28, 2020.

As previously indicated, regarding buildings and structures, for the purposes of the Section 106 review of this federal undertaking, we agree that there are no above-ground properties listed in or eligible for inclusion in the National Register of Historic Places (“NRHP”) within the project’s area of potential effects.

Additionally, as previously indicated, in terms of potential impact on archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the portions of the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Giedd, 5/23/2019) and the addendum Indiana archaeological short report (Giedd/Duddleson, 12/09/2019), that no further archaeological investigations appear necessary at the proposed project area.

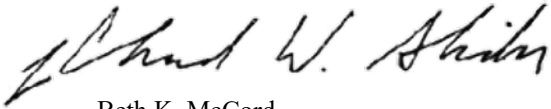
If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

Accordingly, we concur with INDOT’s Section 106 finding, on behalf of FHWA, of “no historic properties affected” for this federal undertaking.

The structures reviewer on the Indiana SHPO staff for this project is Danielle Kauffmann, and the archaeological reviewer is Wade Tharp. However, if you have a question about our comments or about the review process, we ask that you initially direct your question to a staff member of the INDOT Cultural Resources Office who has been assigned to this project.

In any future correspondence regarding the proposed County Road 18 at County Road 13/County Road 115 intersection improvement project, located in Concord Township, Elkhart County (Des. No. 1401749), please refer to DHPA No. 23797.

Very truly yours,

A handwritten signature in black ink, appearing to read "Beth K. McCord". The signature is fluid and cursive, with the first name "Beth" being more prominent.

Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Kari Carmany-George, FHWA
Anuradha Kumar, INDOT
Susan Branigin, INDOT
Shaun Miller, INDOT
Shirley Clark, INDOT
James Heimlich, Orbis Environmental Consulting
Michael LaRonge, Forest County Potawatomi Community
Diane Hunter, Miami Tribe of Oklahoma
Matthew J.N. Bussler, Pokagon Band of Potawatomi Indians
Danielle Kauffmann, INDNR-DHPA
Wade T. Tharp, INDNR-DHPA

AFFIDAVIT OF PUBLICATION

STATE OF INDIANA
County of Elkhart
City of Goshen
Goshen News Fed ID # 82-2664009

ad# 1639928

ISSUED:

The subscriber, being duly sworn, deposes and say
she is the said Mackenzie Deal of THE GOSHEN NEWS
and that the foregoing notice for

Public Notice Des. No. 1401749

was published in said newspaper in one edition
of said newspaper issued on

05/06/20

Cost: \$ 86.40

Mackenzie Deal

Mackenzie Deal

Public Notice Des. No. 1401749

Elkhart County is planning to undertake an intersection improvement, funded in part by the Federal Highway Administration. The project is located on County Road 18 at Country Road 13/County Road 115 in Concord Township, Elkhart County, Indiana. Under the preferred alternative, the proposed project would involve a "peanut shaped" roundabout that would act as a four-legged, single-lane roundabout, elongated to intersect with both CR 13 and CR 115. Ingress and egress access to all adjacent residential properties will be maintained by relocating and extending driveways where necessary. No pedestrian improvements are included with the project. Much of the improved roadway surface will be curbed and guttered and water will be detained before being outlet into the nearby Elkhart River. It is estimated that between 0.47 acres of additional permanent right-of-way will be necessary to accommodate the proposed improvements. Up to 0.65 acres of temporary right-of-way may also be necessary to accommodate incidental construction activities. The proposed action does not impact properties listed in or eligible for the National Register of Historic Places. The Indiana Department of Transportation (INDOT), on behalf of the FHWA, has issued a "No Historic Properties Affected" finding for the project due to the fact that no historic properties are present within the Area of Potential Effects (APE). In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(1), the documentation specified in 36 CFR 800.11(d) can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://ems.indot.in.gov/Section106Documents>. Persons with limited internet access may request project information be mailed, please contact James Heimlich, (574) 850-6003, jheimlich@orbisec.com. This documentation serves as the basis for the "No Historic Properties Affected" finding. The views of the public on this effect finding are being sought. Please reply with any comments to James Heimlich of Orbis Environmental Consulting, P.O. Box 10235, South Bend, IN 46680, (574) 850-6003, jheimlich@orbisec.com, no later than June 6, 2020. In accordance with the "Americans with Disabilities Act", if you have a disability for which Elkhart County needs to provide accessibility to the document(s) such as interpreters or readers, please contact Michelle Goodman, ADA Coordinator, (574) 283-2572.

May 6 hspaxlp

SUBSCRIBED AND SWORN BEFORE ME ON THIS DAY: May 6, 2020

Angela S. Kulczar

Notary Public

Angela S. Kulczar

My Commission Expires February 04, 2027
Commission # NP0718334



APPENDIX E

Hazardous Materials



Date: August 21, 2019

From: C.J. Cunningham
The Troyer Group (on behalf of Elkhart County)
550 Union Street
Mishawaka, IN

Re: RED FLAG INVESTIGATION
DES #1401749, Local Project
Intersection Improvement, Roundabout
C.R. 18 at C.R. 13 & C.R. 115
Elkhart County, Indiana

PROJECT DESCRIPTION

Brief Description of Project:

The project is located along County Road 18, east of the City of Elkhart, at the intersections with County Road 13 and County Road 115, in Elkhart County. The purpose of the project is to increase the operational safety at the intersection by replacing the existing stop-controlled intersection with a safer alternative while still maintaining, or improving, operation capacity for the projected traffic volume demand. The existing intersections are offset by approximately 130 ft. CR 13 is also skewed to the mainline roadway (CR 18). CR 18 is an east-west roadway. Its west approach has a dedicated left-turn lane extending from the Elkhart River bridge to CR 13, and one shared through-right-turn lane. CR 18 is stop controlled with CR 13 and free at CR 115. CR 115 and CR 13 are north-south roadways that form two T-intersections with CR 18. CR 115 and CR 13 are stop controlled at CR 18.

The proposed scope for improvement involves construction of an elongated, "peanut-shaped" roundabout. Improvements to the intersection are needed to reduce the frequency of vehicular accidents that occur at the intersection. This will provide a facility capable of handling the anticipated traffic growth as well as reducing the number of stop-controlled intersections along arterial corridors.

Roadway approach reconstruction and adjacent roadside grading will extend approximately 200 ft. north of the roundabout, 400 ft. east, and 600 ft. south. A modular block retaining wall will run along the east side of CR 115 for approximately 100 ft. and proceed eastward along the south side of the roundabout for approximately another 400 ft. The Elkhart River bridge is located immediately west of the roundabout. The only bridge work to be included with the project is the addition of guardrail on the bridge deck and on CR 18's western bridge approach.

Bridge and/or Culvert Project: Yes ☐ No ☒ Structure # _____

If this is a bridge project, is the bridge Historical? Yes ☐ No ☐ , Select ☐ Non-Select ☐

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

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Proposed right of way: Temporary ☒ # Acres <0.5 Permanent ☒ # Acres >0.5

Type of excavation: Excavation work will be necessary for work related to the installation of the roundabout, grading of the approaches and installation of a storm sewer system. Excavation could extend to as far as 20 feet below existing grade in order to install the roundabout at appropriate grades and achieve proper sight distance.

Maintenance of traffic: Traffic will be maintained via road closure and a detour. The detour will utilize Toledo Rd. to the north, County Rd 17 to the east, and County Rd 45 to the south and west.

Work in waterway: Yes ☒ No ☐ Above ordinary high water mark: Yes ☐ No ☒

State Project: ☐ LPA: ☒

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	N/A	Recreational Facilities	1
Airports ¹	N/A	Pipelines	2
Cemeteries	1	Railroads	N/A
Hospitals	N/A	Trails	N/A
Schools	1	Managed Lands	N/A

¹In order to complete the required airport review, a review of public airports within 3.8 miles (20,000 feet) is required.

Explanation:

Cemeteries- One (1) cemetery is located within the 0.5 mile search radius. The cemetery is located 0.38 mile northwest of the project area. No impact is expected.

Schools- One (1) school is located within the 0.5 mile search radius. Concord East Side Elementary School is located 0.22 mile north of the project area. Traffic will be maintained through the use of a detour. Coordination with Concord Community School corporation will occur.

Recreational Facilities- One (1) recreational facility is located within the 0.5 mile search radius. The recreational facility is located 0.29 mile north of the project area. No impact is expected

Pipelines- Two (2) pipeline segments are located within the 0.5 mile search radius. The nearest segment, Northern Indiana Public Service Co. pipeline, is located 0.06 mile south of the project area. No impact is expected.

WATER RESOURCES TABLE AND SUMMARY

Water Resources Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	N/A

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Karst Springs	N/A	NWI - Wetlands	16
Canal Structures – Historic	N/A	Lakes	13
NPS NRI Listed	N/A	Floodplain - DFIRM	6
NWI-Lines	N/A	Cave Entrance Density	N/A
IDEM 303d Listed Streams and Lakes (Impaired)	N/A	Sinkhole Areas	N/A
Rivers and Streams	3	Sinking-Stream Basins	N/A

Explanation:

Rivers and Streams- Three (3) river are located within the 0.5 mile search radius. The Elkhart River is adjacent to the project area. A Waters of the US Report is recommended and coordination with the appropriate agency, if applicable, will occur.

NWI- Wetlands- Sixteen (16) wetlands are located within the 0.5 mile search radius. The nearest wetland is located adjacent to the project area. A Waters of the US Report is recommended and coordination with the appropriate agency, if applicable, will occur.

Lakes- Thirteen (13) lakes are located within the 0.5 mile search radius. The nearest lake segment is located 0.04 mile north of the project area. No impact is expected.

Floodplains- Six (6) floodplain polygons are located within the 0.5 mile search radius. Part of the project area is located within a floodplain polygon. Coordination with the appropriate agency will occur.

URBANIZED AREA BOUNDARY SUMMARY

Urbanized Area Boundary (UAB): This project lies within the Elkhart County MS4. Post construction Storm Water Quality Best Management Practices (BMPs) may need to be considered. An early coordination letter with topographic and aerial maps showing the project area should be sent to the Elkhart County MS4 Coordinator at 4239 Elkhart Road, Goshen IN 46526.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	N/A	Mineral Resources	N/A
Mines – Surface	N/A	Mines – Underground	N/A

Explanation: No mining or mineral resources are located inside the 0.5 mile search radius.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	N/A	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	N/A	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	1	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	N/A
Construction Demolition Waste	N/A	Institutional Controls	N/A
Solid Waste Landfill	N/A	NPDES Facilities	N/A
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	N/A
Leaking Underground Storage (LUST) Sites	N/A	Notice of Contamination Sites	N/A

Explanation:

Underground Storage Tank (UST) Sites- One (1) underground storage tank (UST) site is located within the 0.5 mile search area. The Dale E Taylor UST site is located 0.37 mile southwest of the project area. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Elkhart County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is attached with ETR species highlighted. A preliminary review of the Indiana Natural Heritage Database by INDOT Environmental Services did not indicated the presence of ETR species. Coordination with USFWS and IDNR will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects"

RECOMMENDATIONS SECTION

INFRASTRUCTURE:

Schools: Concord East Side Elementary School is located 0.22 mile north of the project. Coordination with Concord Community School corporation will occur.

WATER RESOURCES:

The presence of following water resources will require the preparation of a Waters of the US Report and coordination with the appropriate agency, if applicable:

- The Elkhart River is adjacent to the project area.
- One wetland is located within the project area.
- Part of the project area is located within a regulated floodplain. Coordination with the appropriate agency will occur.

URBANIZED AREA BOUNDARY: This project lies within the Elkhart County MS4. Post construction Storm Water Quality Best Management Practices (BMPs) may need to be considered. An early coordination letter with topographic and aerial maps showing the project area should be sent to the Elkhart County MS4 Coordinator at 4239 Elkhart Road, Goshen IN 46526.

MINING/MINERAL EXPLORATION: N/A

HAZMAT CONCERNS: N/A

ECOLOGICAL INFORMATION: Coordination with USF WS and IDNR will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the "Using the USFWS' IPaC System for Listed Bat Consultation for INDOT Projects."

Prepared by:
C.J. Cunningham
Environmental Service-Manager
Troyer Group

Graphics:

SITE LOCATION: YES

INFRASTRUCTURE: YES

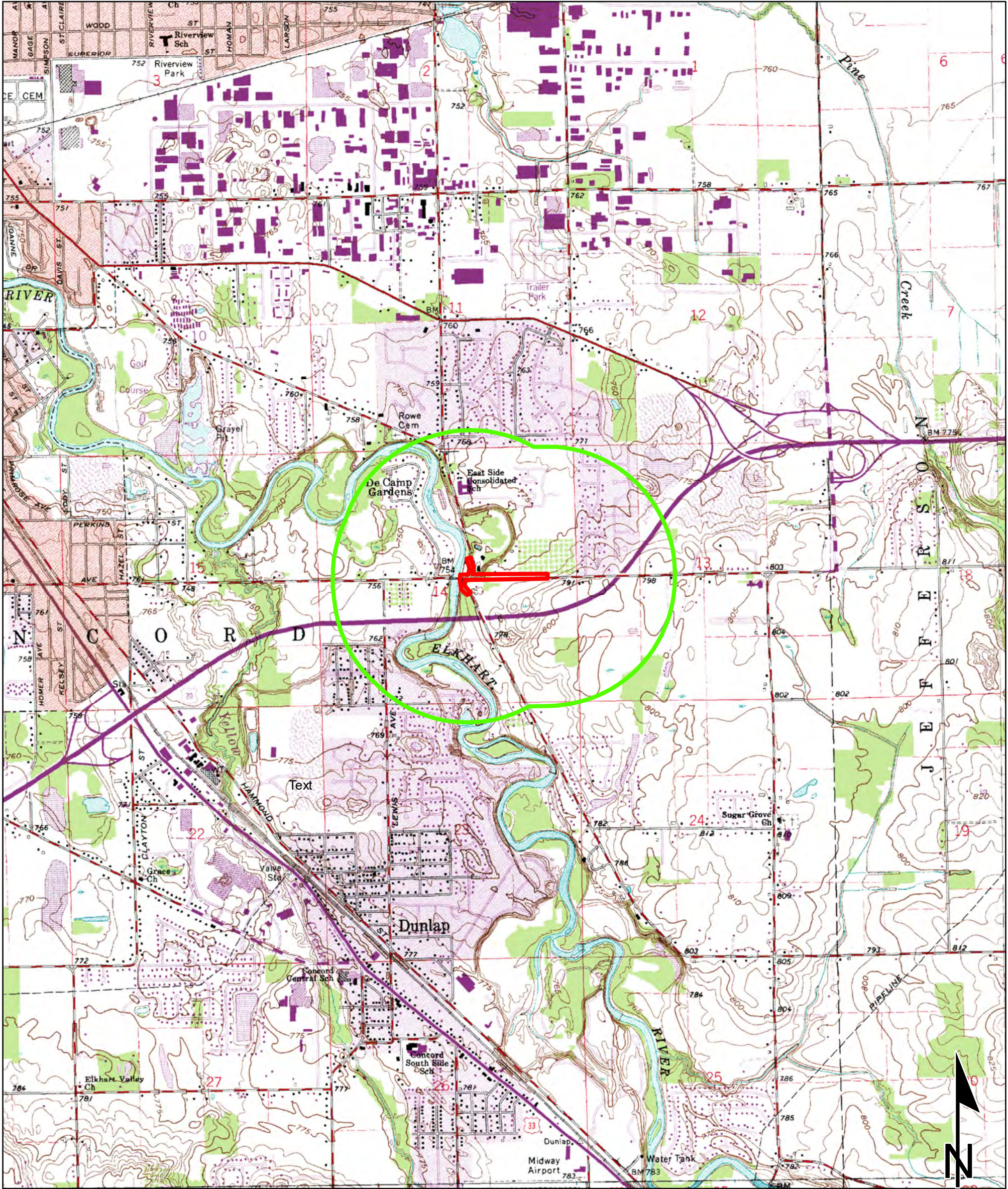
WATER RESOURCES: YES

URBANIZED AREA BOUNDARY: YES

MINING/MINERAL EXPLORATION: N/A

HAZMAT CONCERNS: YES

Red Flag Investigation - Site Location
C.R. 18 at C.R. 13 & C.R. 115
Des. No. 1401749, Intersection Improvement, Roundabout
Elkhart County, Indiana



Sources: 0.5 0.25 0 0.5 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

ELKHART QUADRANGLE
INDIANA
7.5 MINUTE SERIES
(TOPOGRAPHIC)

Red Flag Investigation - Infrastructure
C.R. 18 at C.R. 13 & C.R. 115
Des. No. 1401749, Intersection Improvement, Roundabout
Elkhart County, Indiana



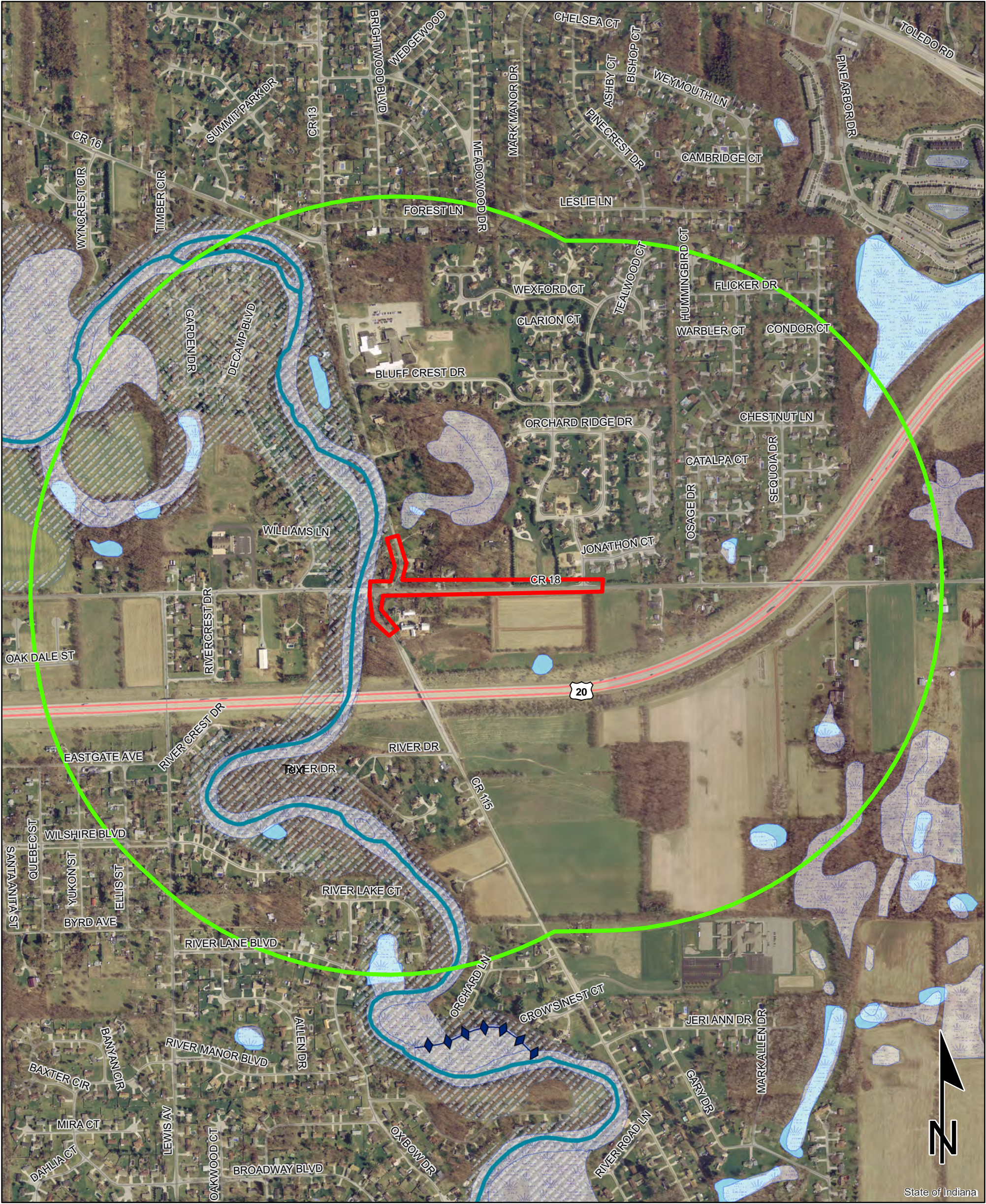
Sources: 0.15 0.075 0 0.15 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
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	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road

Red Flag Investigation - Water Resources

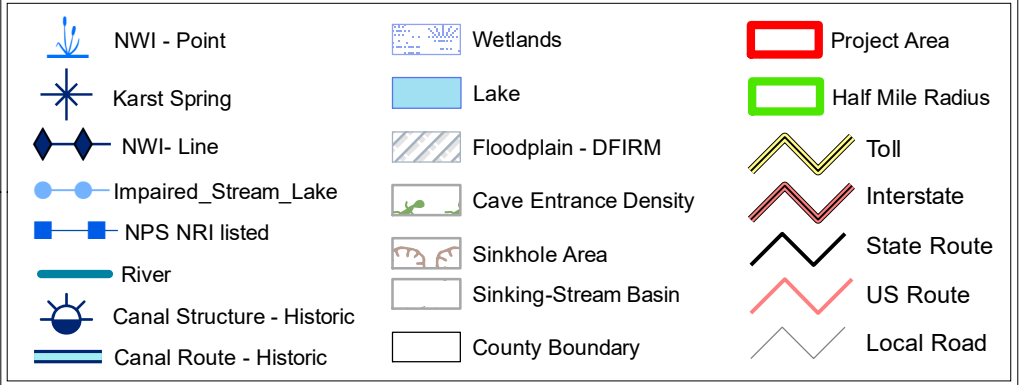
C.R. 18 at C.R. 13 & C.R. 115

Des. No. 1401749, Intersection Improvement, Roundabout
Elkhart County, Indiana

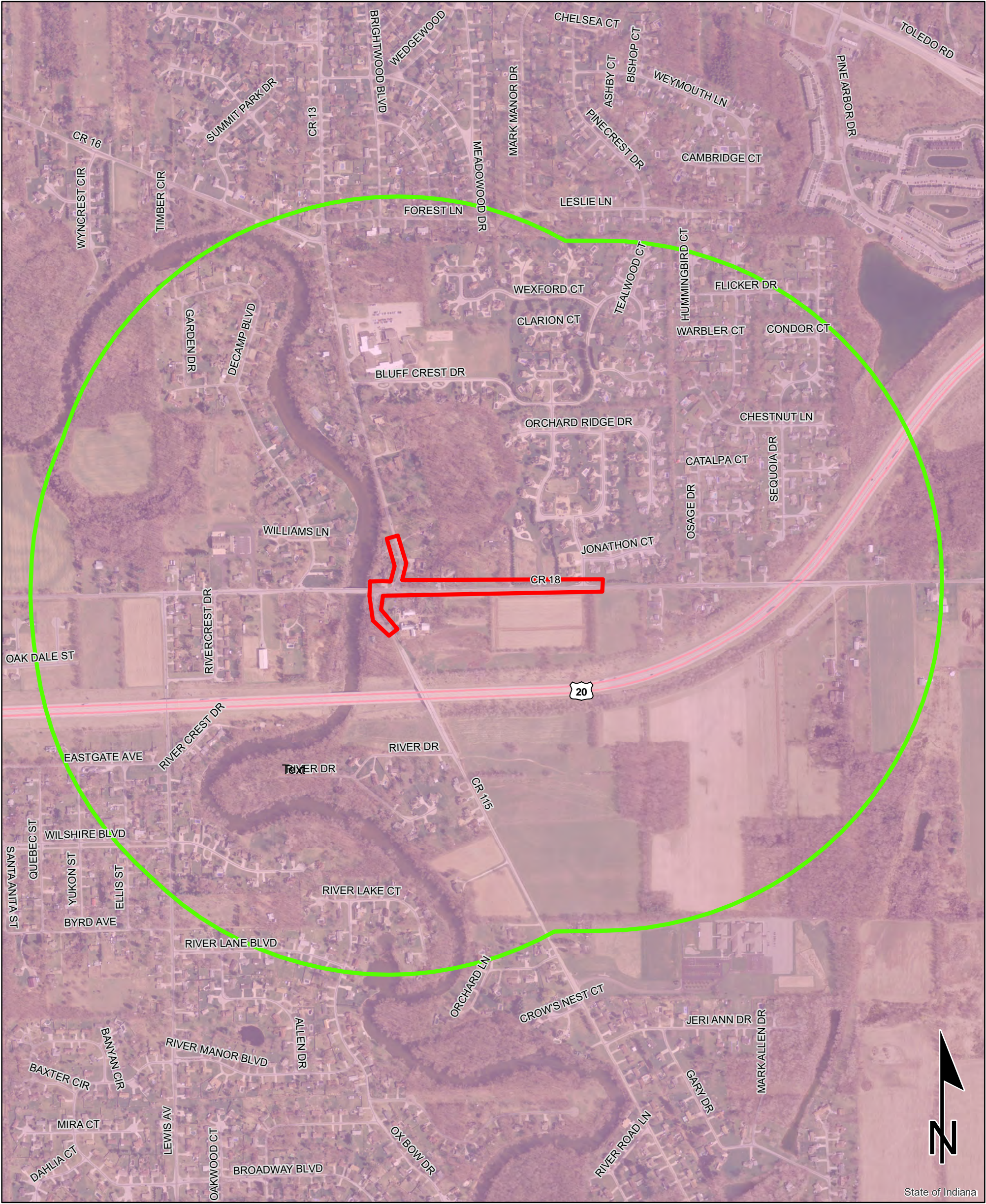


Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

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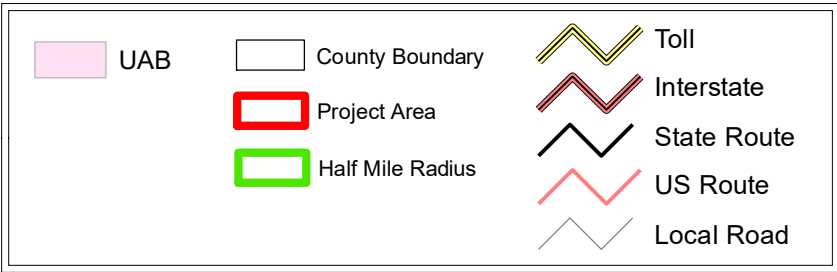


Red Flag Investigation - Urbanized Area Boundary
C.R. 18 at C.R. 13 & C.R. 115
Des. No. 1401749, Intersection Improvement, Roundabout
Elkhart County, Indiana

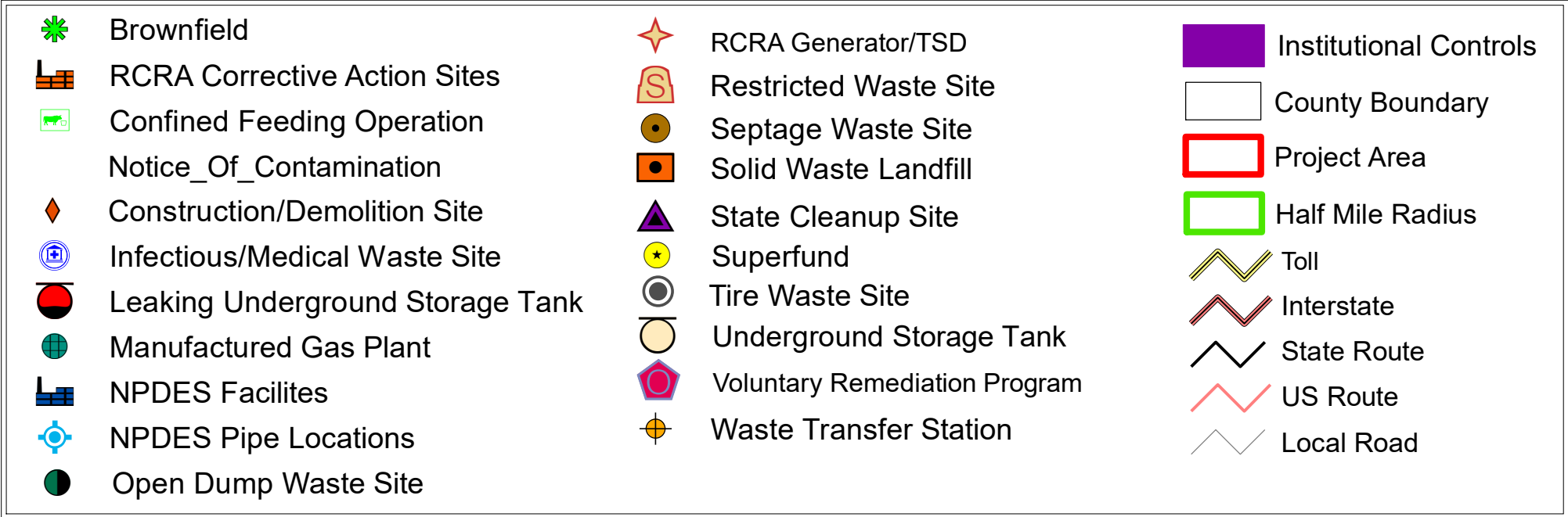
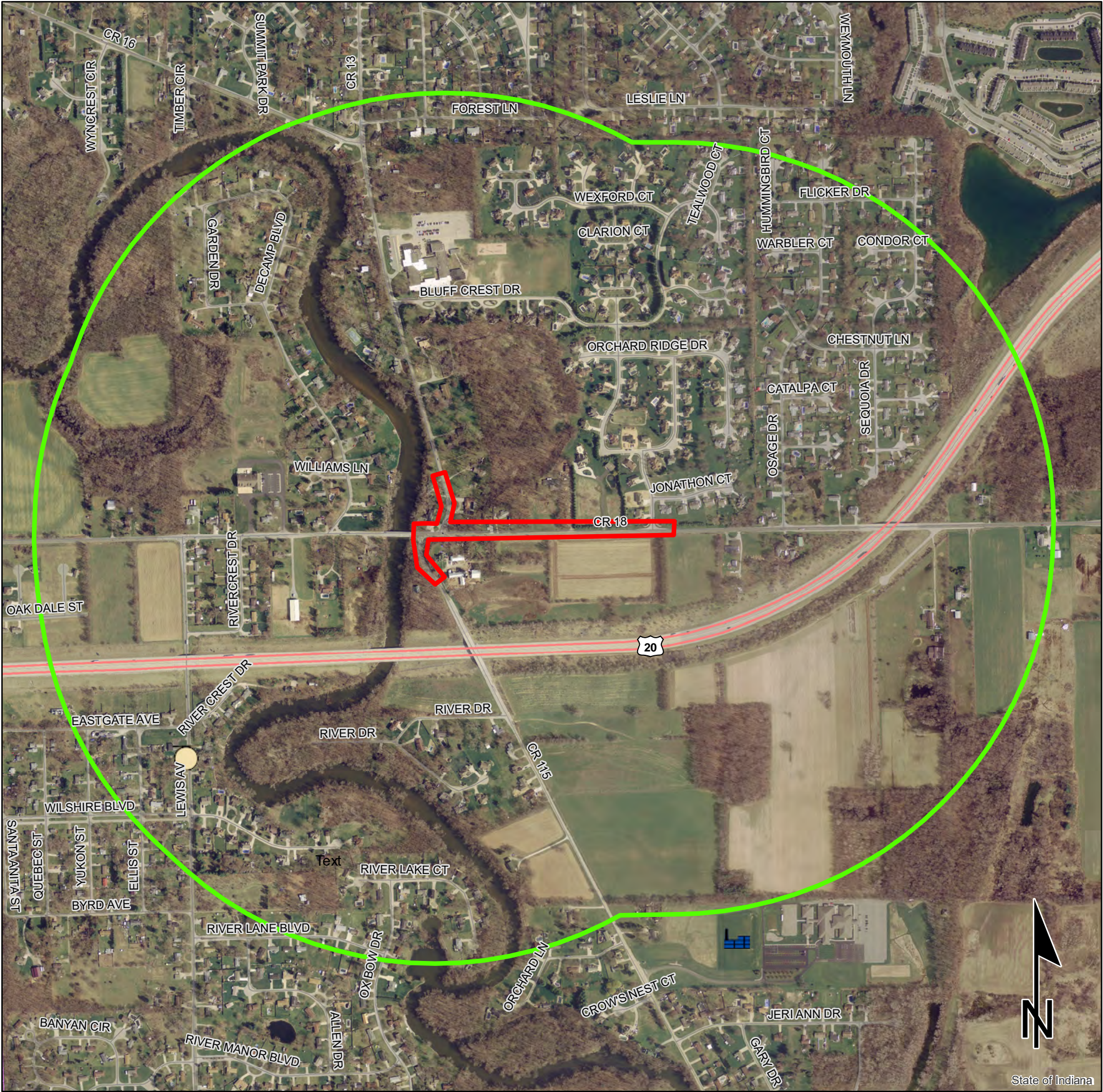


Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

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Red Flag Investigation - Hazardous Material Concerns
C.R. 18 at C.R. 13 & C.R. 115
Des. No. 1401749, Intersection Improvement, Roundabout
Elkhart County, Indiana



Indiana County Endangered, Threatened and Rare Species List

County: Elkhart



Species Name	Common Name	FED	STATE	GRANK	SRANK
Insect: Plecoptera (Stoneflies)					
<i>Acroneuria lycorias</i>	Boreal Stonefly		SE	G5	S1
<i>Perlesta golconda</i>	Two-lined Stone		SE	G2G3	S1
<i>Pteronarcys dorsata</i>	American Salmonfly		SE	G5	S1
Mollusk: Bivalvia (Mussels)					
<i>Venustaconcha ellipsiformis</i>	Ellipse			G4	S2
Mollusk: Gastropoda					
<i>Campeloma decisum</i>	Pointed Campeloma		SSC	G5	S2
Insect: Coleoptera (Beetles)					
<i>Nicrophorus americanus</i>	American Burying Beetle	LE	SX	G3	SX
Insect: Hymenoptera					
<i>Formica ulkei</i>				G5	S1
Insect: Lepidoptera (Butterflies & Moths)					
<i>Apamea lignicolora</i>	The Wood-colored Apamea		ST	G5	S1S2
<i>Apamea nigrrior</i>	Black-dashed Apamea		SR	G5	S2S3
<i>Capis curvata</i>	Curved Halter Moth		ST	G5	S2S3
<i>Catocala praeclara</i>	Praeclara Underwing		SR	G5	S2S3
<i>Crambus girardellus</i>	Orange-striped Sedge Moth		SR	GNR	S2S3
<i>Dasychira cinnamomea</i>	Cinnamon Tussock Moth		SE	G4	S1
<i>Exyra fax</i>	Pitcher Window Moth		SE	G4	S1S2
<i>Iodopepla u-album</i>	White-eyed Borer Moth		SR	G5	S2
<i>Leucania multilinea</i>	Many-lined Wainscot		SR	G5	S1S2
<i>Macrochilo absorptalis</i>	Slant-lined Owlet		SR	G4G5	S2S3
<i>Macrochilo hypocritalis</i>	Twin-dotted Macrochilo		SR	G4	S2
<i>Melanomma auricinctaria</i>	Huckleberry Eye-spot Moth		SR	G4	S2S3
<i>Papaipema appassioanata</i>	The Pitcher Plant Borer Moth		SE	G4	S1
<i>Papaipema speciosissima</i>	The Royal Fern Borer Moth		ST	G4	S2S3
Insect: Odonata (Dragonflies & Damselflies)					
<i>Sympetrum semicinctum</i>	Band-winged Meadowhawk		SR	G5	S2S3
Insect: Tricoptera (Caddisflies)					
<i>Setodes oligius</i>	A Caddisfly		SE	G5	S1
Fish					
<i>Coregonus artedi</i>	Cisco		SSC	G5	S2
<i>Ichthyomyzon fossor</i>	Northern Brook Lamprey		SE	G4	S1
<i>Moxostoma valenciennesi</i>	Greater Redhorse		SE	G4	S2
<i>Rhinichthys cataractae</i>	Longnose Dace		SSC	G5	S2
Amphibian					
<i>Necturus maculosus</i>	Common mudpuppy		SSC	G5	S2

Reptile

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Indiana County Endangered, Threatened and Rare Species List

County: Elkhart



Species Name	Common Name	FED	STATE	GRANK	SRANK
<i>Clemmys guttata</i>	Spotted Turtle	C	SE	G5	S2
<i>Clonophis kirtlandii</i>	Kirtland's Snake		SE	G2	S2
<i>Emydoidea blandingii</i>	Blanding's Turtle	C	SE	G4	S2
<i>Sistrurus catenatus</i>	Eastern Massasauga	LT	SE	G3	S2
<i>Terrapene carolina carolina</i>	Eastern Box Turtle		SSC	G5T5	S3
Bird					
<i>Bartramia longicauda</i>	Upland Sandpiper		SE	G5	S3B
<i>Botaurus lentiginosus</i>	American Bittern		SE	G5	S2B
<i>Certhia americana</i>	Brown Creeper			G5	S2B
<i>Circus hudsonius</i>	Northern Harrier		SE	G5	S2
<i>Cistothorus palustris</i>	Marsh Wren		SE	G5	S3B
<i>Cistothorus platensis</i>	Sedge Wren		SE	G5	S3B
<i>Empidonax alnorum</i>	Alder Flycatcher			G5	S2B
<i>Grus canadensis</i>	Sandhill Crane		SSC	G5	S2B,S1N
<i>Haliaeetus leucocephalus</i>	Bald Eagle		SSC	G5	S2
<i>Ixobrychus exilis</i>	Least Bittern		SE	G4G5	S3B
<i>Lanius ludovicianus</i>	Loggerhead Shrike		SE	G4	S3B
<i>Pandion haliaetus</i>	Osprey		SSC	G5	S1B
<i>Rallus elegans</i>	King Rail		SE	G4	S1B
<i>Rallus limicola</i>	Virginia Rail		SE	G5	S3B
Mammal					
<i>Condylura cristata</i>	Star-nosed Mole		SSC	G5	S2?
<i>Mustela nivalis</i>	Least Weasel		SSC	G5	S2?
<i>Taxidea taxus</i>	American Badger		SSC	G5	S2
Vascular Plant					
<i>Actaea rubra ssp. rubra</i>	red baneberry		ST	G5T5	S1?
<i>Amelanchier humilis</i>	running serviceberry		SE	G5	S1
<i>Andromeda glaucophylla</i>	bog rosemary		ST	G5T5	S2
<i>Besseyia bullii</i>	kitten tails		SE	G3	S1
<i>Boechera stricta</i>	Drummond's rockcress		SE	G5	S1
<i>Borodinia missouriensis</i>	Missouri rockcress		SE	G5	S1
<i>Carex bebbii</i>	Bebb's sedge		ST	G5	S3
<i>Carex debilis var. rudgei</i>	white-edge sedge		WL	G5T5	S3
<i>Carex straminea</i>	straw sedge		ST	G5	S2
<i>Chimaphila umbellata ssp. cisatlantica</i>	pipsissewa		SE	G5T5	S1
<i>Dendrolycopodium hickeyi</i>	Hickey's clubmoss		ST	G5	S3
<i>Dendrolycopodium obscurum</i>	tree clubmoss		ST	G5	S3
<i>Eleocharis equisetoides</i>	horse-tail spikerush		SE	G4	S1
<i>Eleocharis robbinsii</i>	Robbins' spikerush		ST	G4G5	S2
<i>Epigaea repens</i>	trailing arbutus		ST	G5	S3

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Indiana County Endangered, Threatened and Rare Species List

County: Elkhart



Species Name	Common Name	FED	STATE	GRANK	SRANK
<i>Eriocaulon aquaticum</i>	pipewort		SE	G5	S1
<i>Eriophorum gracile</i>	slender cotton-grass		ST	G5	S2
<i>Eriophorum viridicarinatum</i>	green-keeled cotton-grass		ST	G5	S3
<i>Fuirena pumila</i>	dwarf umbrella-sedge		ST	G4	S2
<i>Geranium robertianum</i>	herb-Robert		ST	G5	S3
<i>Juniperus communis</i> var. <i>depressa</i>	ground juniper		ST	G5T5	S3
<i>Linum striatum</i>	ridged yellow flax		WL	G5	S3
<i>Malaxis unifolia</i>	green adder's-mouth orchid		SE	G5	S1
<i>Matteuccia struthiopteris</i>	ostrich fern		ST	G5	S3
<i>Milium effusum</i>	tall millet-grass		ST	G5	S1
<i>Minuartia michauxii</i> var. <i>michauxii</i>	Michaux's stitchwort		ST	G5T5	S2
<i>Pinus strobus</i>	eastern white pine		ST	G5	S3
<i>Piptochaetium avenaceum</i>	blackseed needlegrass		ST	G5	S3
<i>Platanthera leucophaea</i>	prairie white-fringed orchid	LT	SE	G2G3	S1
<i>Platanthera psycodes</i>	small purple-fringe orchid		ST	G5	S3
<i>Poa paludigena</i>	bog bluegrass		ST	G3G4	S3
<i>Potamogeton pulcher</i>	spotted pondweed		ST	G5	S2
<i>Pseudognaphalium macounii</i>	winged cudweed		SX	G5	SX
<i>Pyrola americana</i>	American wintergreen		ST	G5	S2
<i>Quercus prinoides</i>	dwarf chinquapin oak		SE	G5	S1
<i>Rhamnus alnifolia</i>	alderleaf buckthorn		ST	G5	S3
<i>Rhynchospora macrostachya</i>	tall beaked-rush		ST	G4	S3
<i>Rhynchospora scirpoides</i>	long-beaked baldrush		ST	G4	S3
<i>Schoenoplectiella purshiana</i>	weakstalk bulrush		ST	G4G5	S3
<i>Schoenoplectiella smithii</i>	Smith's Bulrush		ST	G5?	S2
<i>Selaginella rupestris</i>	ledge spike-moss		SE	G5	S1
<i>Spiranthes lucida</i>	shining ladies'-tresses		ST	G4	S3
<i>Symphyotrichum boreale</i>	rushlike aster		ST	G5	S2
<i>Triantha glutinosa</i>	false asphodel		ST	G5	S2
<i>Utricularia cornuta</i>	horned bladderwort		SE	G5	S1
<i>Utricularia minor</i>	lesser bladderwort		ST	G5	S1
<i>Utricularia purpurea</i>	purple bladderwort		ST	G5	S3
<i>Vaccinium oxycoccos</i>	small cranberry		ST	G5	S2
<i>Valerianella chenopodiifolia</i>	goose-foot corn-salad		WL	G4	S3
<i>Xyris difformis</i>	Carolina yellow-eyed grass		ST	G5	S2

High Quality Natural Community

Forest - floodplain mesic	Mesic Floodplain Forest	SG	G3?	S1
Forest - floodplain wet-mesic	Wet-mesic Floodplain Forest	SG	G3?	S3
Forest - upland mesic Northern Lakes	Northern Lakes Mesic Upland Forest	SG	GNR	S1

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Indiana County Endangered, Threatened and Rare Species List

County: Elkhart



Species Name	Common Name	FED	STATE	GRANK	SRANK
<i>Lake - lake</i>	Lake		SG	GNR	S2
<i>Prairie - sand dry-mesic</i>	Dry-mesic Sand Prairie		SG	G3	S3
<i>Wetland - beach marl</i>	Marl Beach		SG	G3	S2
<i>Wetland - bog acid</i>	Acid Bog		SG	G3	S2
<i>Wetland - bog circumneutral</i>	Circumneutral Bog		SG	G3	S3
<i>Wetland - fen</i>	Fen		SG	G3	S3
<i>Wetland - flat muck</i>	Muck Flat		SG	G2	S2
<i>Wetland - flat sand</i>	Sand Flat		SG	G2	S1
<i>Wetland - marsh</i>	Marsh		SG	GU	S4
<i>Wetland - swamp shrub</i>	Shrub Swamp		SG	GU	S2

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APPENDIX F

Water Resources

Wetland Delineation Report



CR 18 at CR 13/CR 115 Intersection Improvement Project Elkhart, Elkhart County, Indiana

October 25, 2019

Prepared for:



Project #1809002



Orbis Environmental Consulting
P.O. Box 10235 • South Bend, Indiana 46680
Phone: (574) 635-1338

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EXECUTIVE SUMMARY	1
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METHODS.....	1
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Site Visit Results	3
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APPENDICES

Appendix A – Figures

Appendix B – Site Photographs

Appendix C – USACE Wetland Delineation Data Sheets

WETLAND DELINEATION REPORT

CR 18 AT CR 13/CR 115 INTERSECTION IMPROVEMENT PROJECT

October 25, 2019

Scott Namestnik and Amelia Harris

Orbis Environmental Consulting

P.O. Box 10235

South Bend, Indiana 46680

EXECUTIVE SUMMARY

A wetland delineation was conducted at the approximately 2.5-acre CR 18 at CR 13/CR 115 Intersection Improvement Project site located in Elkhart, Elkhart County, Indiana. Fieldwork was conducted November 7, 2018 and October 10, 2019 to identify all surface water features within the study area. A 0.01 acre potentially jurisdictional wetland and a stretch of the Elkhart River were located on the site. The location, extent, and jurisdictional status of any surface water feature is considered potential until verified by the US Army Corps of Engineers (USACE). It is recommended to submit this report to the USACE – Detroit District for verification prior to any construction activities on the site.



INTRODUCTION

Troyer Group (Troyer) contracted Orbis Environmental Consulting (Orbis) to conduct a wetland delineation at a site located at the intersection of County Road (CR) 18, CR 13, and CR 115 in Elkhart, Elkhart County, Indiana. Fieldwork was conducted November 7, 2018 and October 10, 2019 within the approximately 2.5-acre study area following a desktop review of resource maps and materials. The proposed project is intended to improve the safety and traffic flow within the intersection. Figures depicting the site are presented in Appendix A, photographs of the site are presented in Appendix B, and completed United States Army Corps of Engineers (USACE) wetland delineation data forms are presented in Appendix C.



METHODS

Orbis first received a map of the approximate project site in correspondence from Troyer dated September 11, 2018 and conducted a field investigation on November 7, 2018 based on this figure. Orbis received a second map showing a larger study area on September 23, 2019 and conducted an additional field investigation on October 10, 2019 to inspect the additional area. Orbis was requested to conduct the wetland delineation field survey within the area shown and within an additional 20 feet beyond the expected project boundaries. The figures in Appendix A show the approximate field survey areas including this 20-foot buffer.

Prior to fieldwork, Orbis reviewed available resource maps, including National Wetlands Inventory (NWI) maps, soils maps, topographic maps, and aerial photography, to determine the potential presence of

wetlands and surface water features on the site. This information was also used to determine site topography, soil types, and whether any portions of the site are located within mapped floodways of drainage features. Identification of wetlands required characterization of plant community types, identification of hydric soils, and documentation of wetland hydrology indicators for each community type. The site was assessed using methodology from Environmental Laboratory (1987) and USACE (2012).



RESULTS

Literature Review

As shown in Appendix A Figure 1, the site lies between approximately 690 feet and 750 feet in elevation (National Geodetic Vertical Datum) according to the USGS 7.5' Series Elkhart, Indiana quadrangle map (USGS 1961). No wetlands or marsh symbols are mapped for the site, but the Elkhart River is shown forming the western boundary of the site.

As shown on Appendix A Figure 2, the aerial map depicts the study area as paved roads, residential yards, and forested area. The area surrounding the site is similar and also includes agricultural fields. The Elkhart River is shown forming the western boundary of the site.

The Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map (FIRM) was reviewed for the site (FEMA 2018). As shown on Appendix A Figure 3, the majority of the project site (all but the extreme western portion) is located in an area designated Zone X (unshaded), indicating minimal flood hazard mapped outside the 500-year flood level. The western portion of the site is located in an area designated Zone AE, indicating a 1% probability of flooding every year (100-year floodplain). The extreme western portion of the site is mapped within the regulatory floodway.

The US Fish and Wildlife Service's (USFWS) National Wetlands Inventory Map (NWI) was reviewed for the site (USFWS 2014). As shown on Appendix A Figure 4, no NWI wetlands are present within the site boundaries. A permanently flooded, unconsolidated bottom, lower perennial riverine system (R2UBH) is mapped on the western side of the site.

According to the Web Soil Survey (NRCS-USDA 2018) (Appendix A Figure 5), two soils are mapped for the site; these soils are described in Table 1. According to the National Hydric Soils List for Indiana (USDA-NRCS 2018), Gravelton loam, 0 to 1 percent slopes, frequently flooded, long duration (GodAl) is a hydric soil, whereas Urban land-Bristol complex, 0 to 1 percent slopes is a non-hydric soil that does not typically contain hydric inclusions.

TABLE 1. SOILS MAPPED FOR THE PROJECT SITE.

Mapped Soil Unit	Hydric Status	Type of Inclusions	Location of Hydric Inclusions
Gravelton loam, 0 to 1 percent slopes, frequently flooded, long duration (GodAI)	Hydric	-	-
Urban land-Bristol complex, 0 to 1 percent slopes (UdpA)	Non-hydric	-	-

Site Visit Results

The study area primarily consists of paved roads (CR 18, CR 13, and CR 115), undeveloped road right-of-way, and single-family homes with associated residential yards. Topography on the site generally slopes down to the west, with some potentially artificial steep slopes at the eastern end of the site.

The northwest side of the intersection consists of a home and residential yard dominated by *Glechoma hederacea* (groundivy, FACU), *Poa pratensis* (Kentucky blue grass, FACU), and *Taraxacum officinale* (common dandelion, FACU) with scattered trees including *Acer saccharinum* (silver maple, FACW), *Juglans nigra* (black walnut, FACU), *Morus alba* (white mulberry, FACU), and *Robinia pseudoacacia* (black locust, FACU). The Elkhart River forms the western border of this quadrant. A steep slope is present directly adjacent to the Elkhart River; plant species present on this slope include *Asarum canadense* (Canadian wild ginger, UPL), *Commelina communis* (Asiatic dayflower, FAC), *Elymus virginicus* (Virginia wild rye, FACW), and *Symphotrichum lanceolatum* (white paniced American-aster, FACW); *Platanus occidentalis* (American sycamore, FACW) and *Ulmus americana* (American elm, FACW) are present at the base of the slope along the edge of the river.

The northeast side of the intersection consists of forested land to the south and open residential lawn and road right-of-way to the north. In the forested portion, dominant plant species include *Acer platanoides* (Norway maple, UPL), *Anemone americana* (round-lobed hepatica, UPL), *Celtis occidentalis* (common hackberry, FAC), *Euonymus alatus* (winged burning bush, UPL), *Morus alba*, *Prunus serotina* (black cherry, FACU), *Quercus alba* (northern white oak, FACU), *Quercus velutina* (black oak, UPL), *Robinia pseudoacacia*, *Smilax hispida* (Chinaroot, FAC), and *Solidago ulmifolia* (elm-leaf goldenrod, UPL). In the road right-of-way and residential lawn, *Bromus inermis* (smooth brome, UPL), *Digitaria ischaemum* (smooth crab grass, FACU), *Glechoma hederacea*, *Poa pratensis*, and *Taraxacum officinale* are common.

The southeast side of the intersection is mostly forested on steep slopes, with open road right-of-way along the road. The forested portion is dominated by *Acer negundo* (ash-leaf maple, FAC), *Celtis occidentalis*, *Chelidonium majus* (greater celandine, UPL), *Euonymus hamiltonianus* (Hamilton's strawberry bush, UPL), *Galium aparine* (sticky-willy, FACU), *Geum canadense* (white avens, FAC), *Juglans nigra*, *Lonicera maackii* (amur honeysuckle, UPL), *Morus alba*, *Osmorhiza longistylis* (aniseroot, FACU), and *Ulmus americana*; *Elymus virginicus* is common at the base of the slope. In the more topographically level road right-of-way, in the vicinity of a culvert that moves runoff under CR 115 towards the Elkhart River, dominant plant species include *Arctium minus* (lesser burdock, FACU), *Bromus inermis*, *Conium*

maculatum (poison-hemlock, FACW), *Dactylis glomerata* (orchard grass, FACU), *Digitaria sanguinalis* (hairy crab grass, FACU), *Echinochloa crus-galli* (large barnyard grass, FAC), *Leonurus cardiaca* (motherwort, UPL), *Persicaria longiseta* (bristly lady's-thumb, FAC), *Persicaria maculosa* (spotted lady's-thumb, FAC), *Phalaris arundinacea* (reed canary grass, FACW), *Setaria faberi* (Japanese bristle grass, FACU), and *Setaria pumila* (yellow bristle grass, FAC).

On the southwest side of the intersection, a forested upland area slopes down to the west; the extreme western end of the project area is the Elkhart River and associated forested floodplain. Dominant plant species on the forested slope include *Acer negundo*, *Asarum canadense*, *Celtis occidentalis*, *Fraxinus pennsylvanica* (green ash, FACW), *Prunus serotina*, and *Quercus alba*. Between the base of the slope and the Elkhart River, dominant plant species include *Acer negundo*, *Acer rubrum* (red maple, FAC), *Elymus virginicus*, *Ranunculus hispidus* (bristly buttercup, FAC), *Rudbeckia laciniata* (green-head coneflower, FACW), *Saururus cernuus* (lizard's-tail, OBL), *Symphytotrichum lanceolatum*, *Tilia americana* (American basswood, FACU), and *Ulmus americana*; where trees are absent due to proximity to CR 18, dominant plant species include *Impatiens capensis* (spotted touch-me-not, FACW), *Lythrum salicaria* (purple loosestrife, OBL), *Phalaris arundinacea*, and *Pilea pumila* (Canadian clearweed, FACW).

The Elkhart River (approximately 104 linear feet) and one potentially jurisdictional 0.01-acre wetland were documented on the site (Appendix A Figure 6). Wetland delineation results are summarized in Table 2 and each surface water feature is described in detail below.

TABLE 2. SURFACE WATER FEATURES SUMMARY

Feature Name	Classification	Stream Length (In ft)	Wetland Area (ac)
W-001-PFO	Potentially Jurisdictional	-	0.01
Elkhart River	Potentially Jurisdictional	104.13	-
Total		104.13	0.01

W-001-PFO is a forested wetland located on the western side of the site, south of CR 18, in floodplain along the Elkhart River. The wetland comprises approximately 0.01 acre and continues off-site to the south. The USACE data form recorded at Data Point 1 shows that 80% of the dominant plant species are hydrophytic and thus the dominance test is met. Four hydric soil indicators are present: sandy redox (S5), loamy gleyed matrix (F2), depleted matrix (F3), and redox dark surface (F6). Primary indicators of wetland hydrology present include high water table (A2), saturation (A3), water-stained leaves (B9), and oxidized rhizospheres on living roots (C3); one secondary indicator of wetland hydrology, FAC-neutral test (D5), is also present. W-001-PFO is directly connected to the Elkhart River and would potentially be considered a jurisdictional 'waters of the US'.

Data Point 2 was recorded in upland adjacent to W-001-PFO. At this location, 50% of the dominant plant species are hydrophytic and thus the dominance test is not met. No indicators of hydric soils or wetland hydrology are present. As the area does not exhibit hydrophytic vegetation, hydric soils, or wetland hydrology Data Point 2 is not within wetland.

The **Elkhart River** runs along the west edge of the site, including under the bridge along CR 18, for approximately 104.13 linear feet. The river continues off-site to the north, west, and south. North of CR 18, a steep slope is present immediately adjacent to the river, whereas the slope is much shallower south of CR 18, where W-001-PFO is located. The Elkhart River would potentially be considered a jurisdictional 'waters of the US'.



CONCLUSIONS AND RECOMMENDATIONS

A wetland delineation was conducted for Troyer at the approximately 2.5-acre CR 18 at CR 13/CR 115 intersection improvement project area in Elkhart, Elkhart County, Indiana on November 7, 2018 and October 10, 2019. One forested wetland, comprising approximately 0.01 acre and approximately 104 linear feet of the Elkhart River, were observed on the site. Both of these features will likely be considered jurisdictional, but the location, extent, and jurisdictional status of any surface water feature is considered potential until verified by the USACE. It is recommended to forward this report to the USACE – Detroit District for verification and a jurisdictional determination prior to any construction activities.



LITERATURE CITED

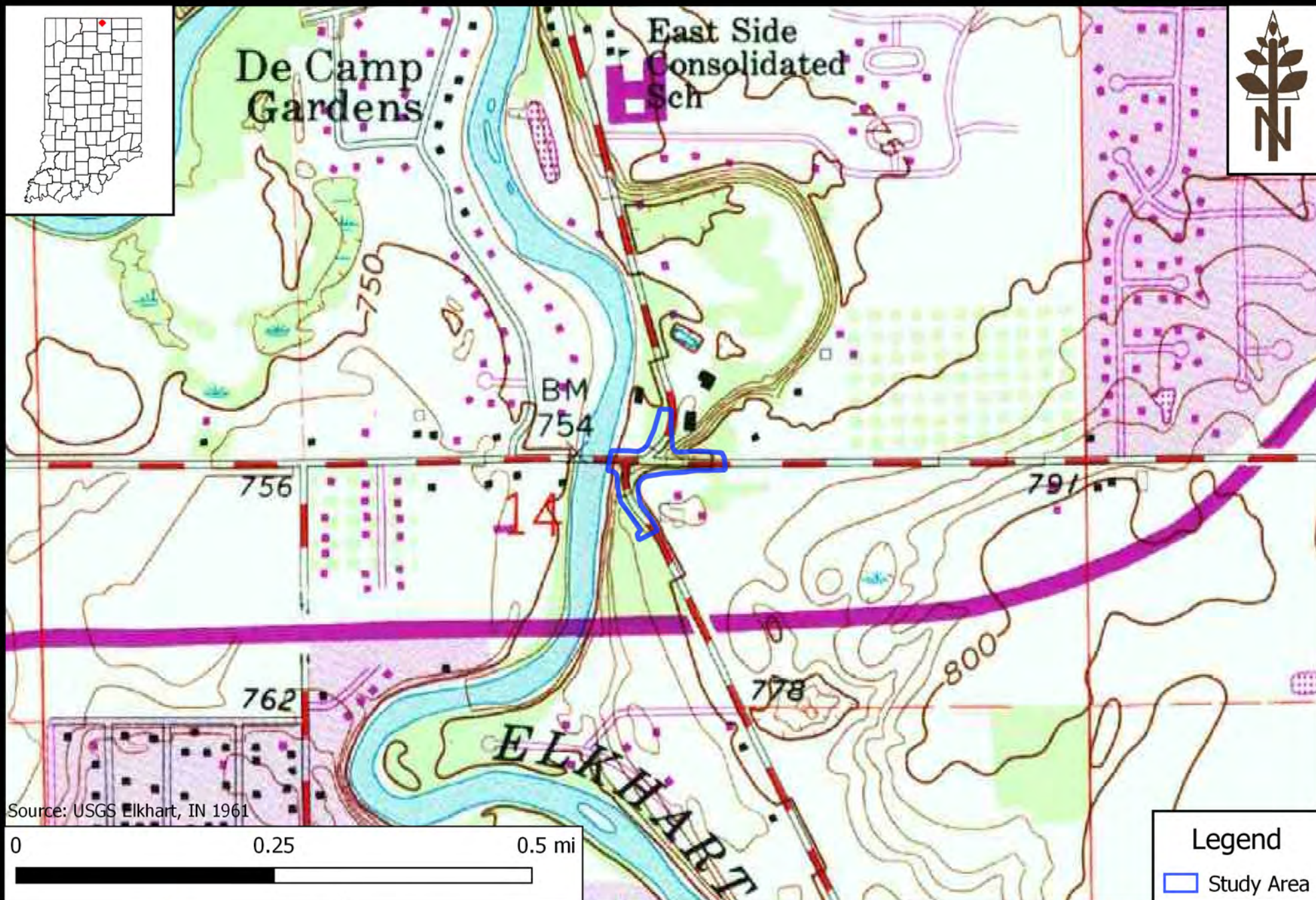
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Wetland Delineation Report

CR 18 at CR 13/CR 115 Intersection Improvement
Project

Elkhart, Elkhart County, Indiana





Wetland Delineation Report
CR 18 at CR 13/CR 115 Intersection Improvement
Elkhart, Elkhart County, Indiana

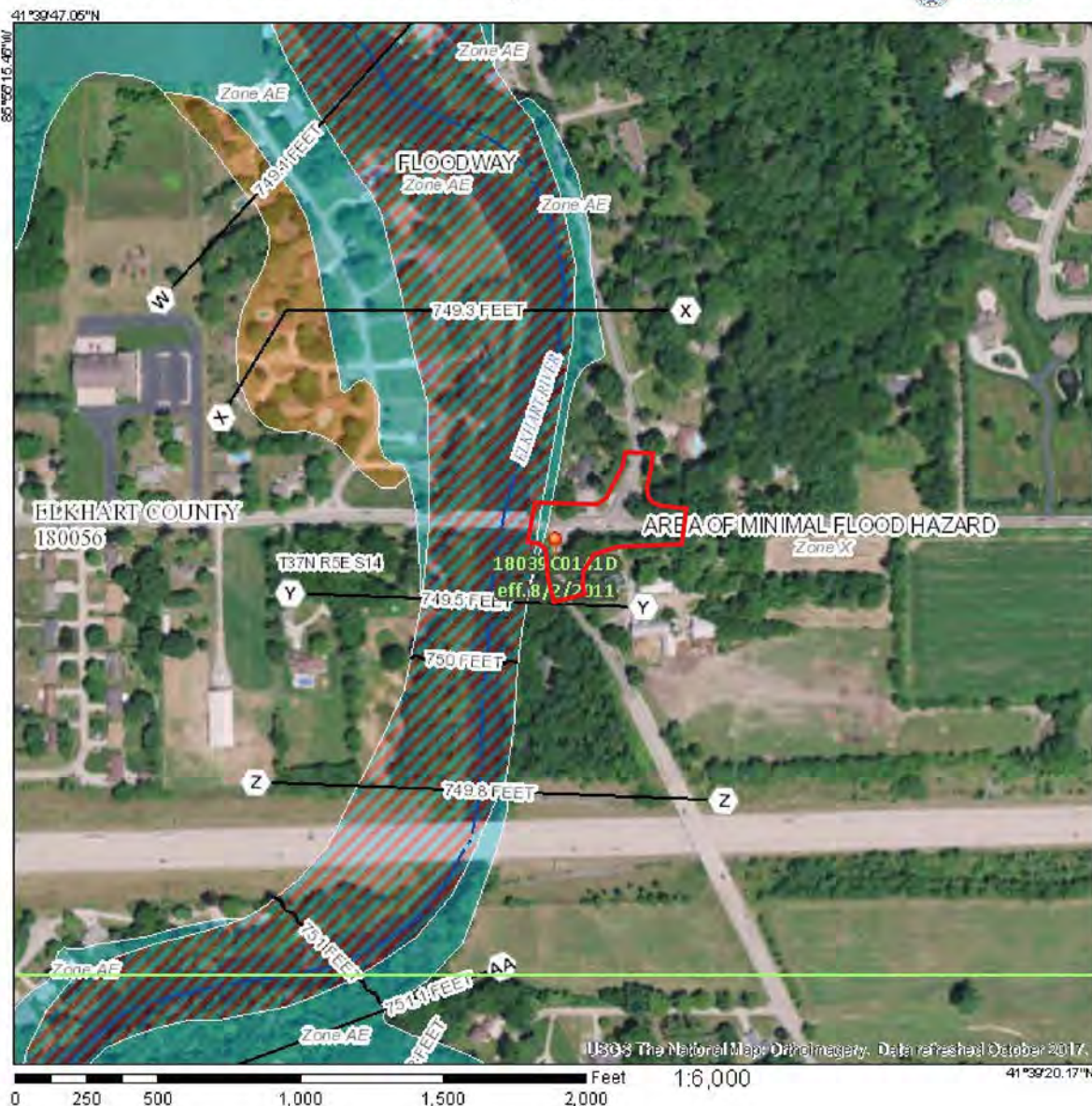
Figure 1
Topographic Map
Project #1809002



Wetland Delineation Report
CR 18 at CR 13/CR 115 Intersection Improvement
Elkhart, Elkhart County, Indiana

Figure 2
Aerial Map
Project #1809002

National Flood Hazard Layer FIRMette



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone X, AGD
		With BFE or Depth Zone AE, AG, AH, VE, AR
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		Area of Minimal Flood Hazard Zone X
		Effective LOMRs
GENERAL STRUCTURES		Area of Undetermined Flood Hazard Zone D
		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall
OTHER FEATURES		Cross Sections with 1% Annual Chance Water Surface Elevation
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
MAP PANELS		Digital Data Available
		No Digital Data Available
		Unmapped

The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards.

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 11/9/2018 at 2:24:01 PM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.



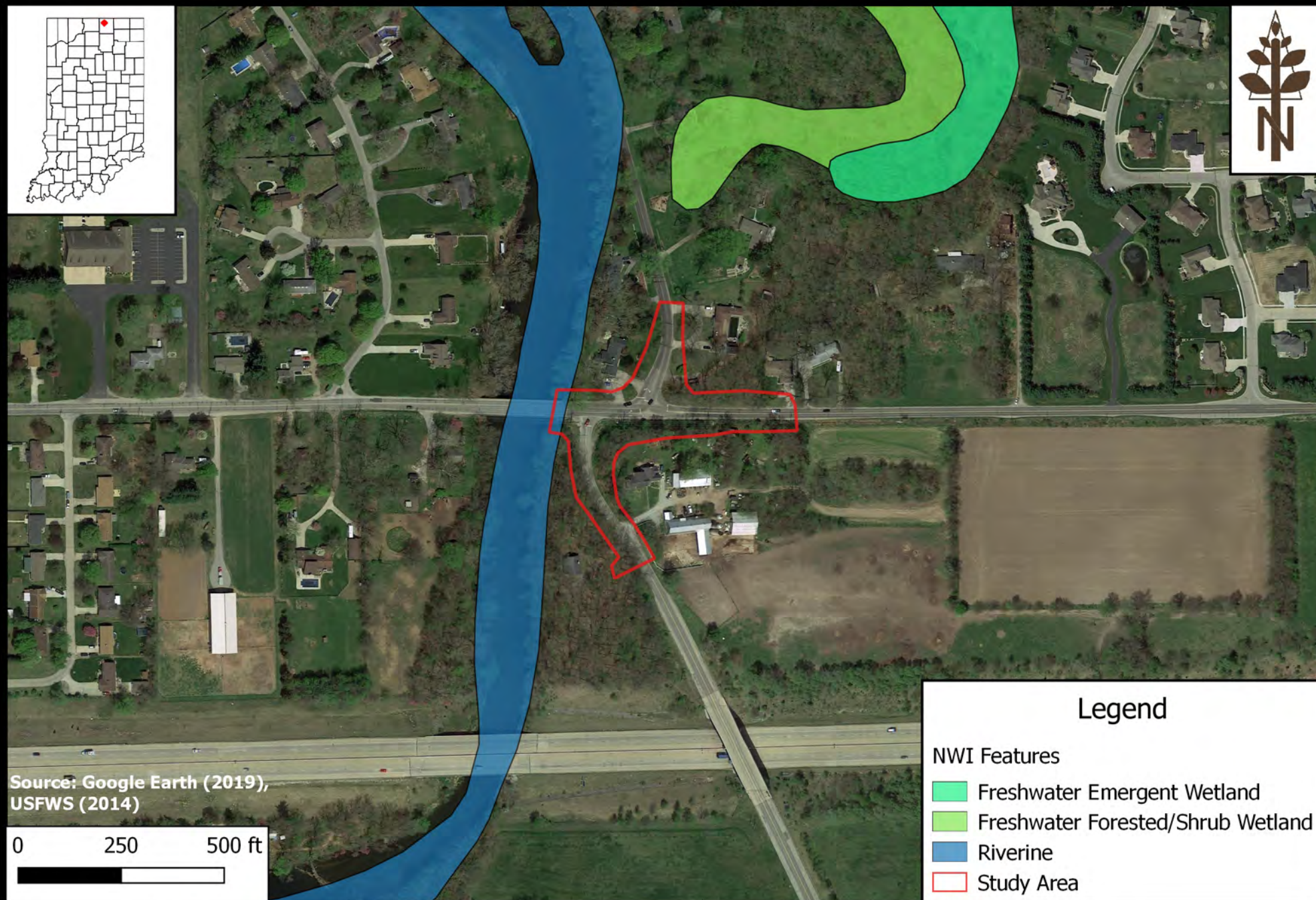
Legend

Site Boundary



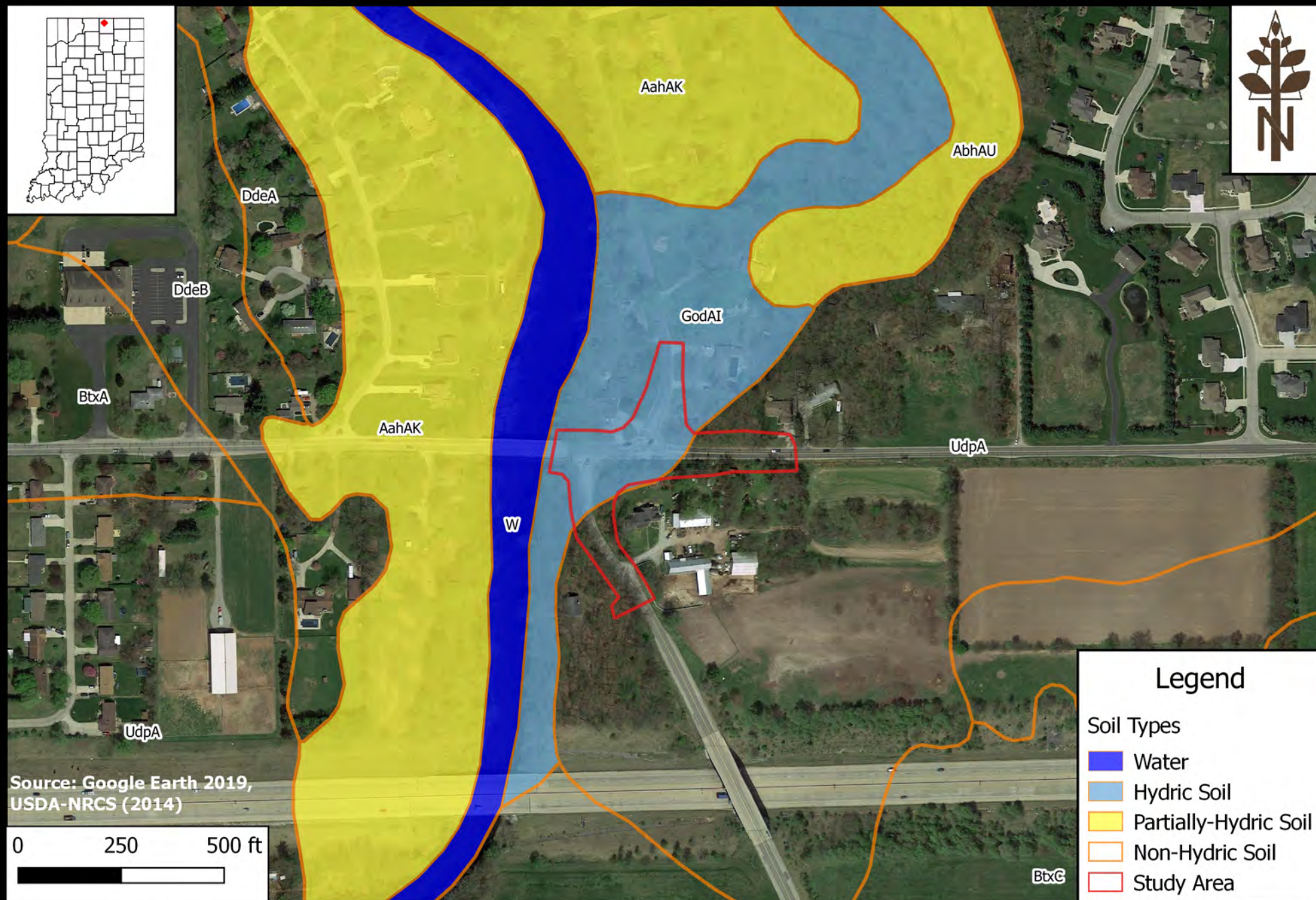
Wetland Delineation Report
CR 18 at CR 13/CR 115 Intersection Improvement
Elkhart, Elkhart County, Indiana

Figure 3
FEMA Map
Project #1809002



Wetland Delineation Report
CR 18 at CR 13/CR 115 Intersection Improvement
Elkhart, Elkhart County, Indiana

Figure 4
NWI Map
Project #1809002



Legend

Soil Types

- Water
- Hydric Soil
- Partially-Hydric Soil
- Non-Hydric Soil
- Study Area



Wetland Delineation Report
CR 18 at CR 13/CR 115 Intersection Improvement
Elkhart, Elkhart County, Indiana

Figure 5
Soils Map
Project #1809002



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CR 18 at CR 13/CR 115 Intersection Improvement
Elkhart, Elkhart County, Indiana

Figure 6
Results Map
Project #1809002

Appendix B

Site Photographs

Wetland Delineation Report

CR 18 at CR 13/CR 115 Intersection Improvement
Project

Elkhart, Elkhart County, Indiana





Northwest side of intersection, looking north.



Northwest side of intersection, looking south.



Northeast side of intersection, looking southeast.



Northeast side of intersection, looking south.



Wetland Delineation Report
CR 18 at CR 13/CR 115 Intersection Improvement
Elkhart, Elkhart County, Indiana

Photographic Documentation
November 7, 2018
#1809002



Southeast side of intersection, looking southeast.



Southeast side of intersection, looking south.



Southeast side of intersection, looking northeast.



Culvert on southeast side of intersection,
looking west.



Wetland Delineation Report
CR 18 at CR 13/CR 115 Intersection Improvement
Elkhart, Elkhart County, Indiana

Photographic Documentation
November 7, 2018
#1809002



DP 1 on southwest side of intersection, looking west.



DP 2 on southwest side of intersection, looking east.



W-001-PFO (right) and Elkhart River (left), looking north.



Wetland Delineation Report
CR 18 at CR 13/CR 115 Intersection Improvement
Elkhart, Elkhart County, Indiana

Photographic Documentation
November 7, 2018
#1809002

Appendix C

USACE Wetland Delineation Data Sheets

Wetland Delineation Report

CR 18 at CR 13/CR 115 Intersection Improvement
Project

Elkhart, Elkhart County, Indiana



WETLAND DETERMINATION DATA FORM – Northcentral and Northeast Region

Project/Site: Elkhart CR 18/13/115 Intersection City/County: Elkhart County Sampling Date: 11/7/2018
 Applicant/Owner: Troyer Group/INDOT State: IN Sampling Point: DP 1
 Investigator(s): S. Namestnik, N. Pilla Section, Township, Range: Sec 14, Twp 37 N, Rng 5 E
 Landform (hillside, terrace, etc.): flood plain Local relief (concave, convex, none): concave Slope %: 1
 Subregion (LRR or MLRA): LRR L, MLRA 98 Lat: 41°39'33.64"N Long: 85°54'57.74"W Datum: WGS 84
 Soil Map Unit Name: Gravelton loam, 0 to 1% slopes, frequently flooded, long duration (GodAI) NWI classification: R2UBH
 Are climatic / hydrologic conditions on the site typical for this time of year? Yes X No (If no, explain in Remarks.)
 Are Vegetation , Soil , or Hydrology significantly disturbed? Are "Normal Circumstances" present? Yes X No
 Are Vegetation , Soil , or Hydrology naturally problematic? (If needed, explain any answers in Remarks.)

SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.

Hydrophytic Vegetation Present? Yes <u>X</u> No <u> </u> Hydric Soil Present? Yes <u>X</u> No <u> </u> Wetland Hydrology Present? Yes <u>X</u> No <u> </u>	Is the Sampled Area within a Wetland? Yes <u>X</u> No <u> </u> If yes, optional Wetland Site ID: <u>W-001-PFO</u>
Remarks: (Explain alternative procedures here or in a separate report.) Photo 1 west	

HYDROLOGY

Wetland Hydrology Indicators: <u>Primary Indicators (minimum of one is required; check all that apply)</u> <u> </u> Surface Water (A1) <u>X</u> Water-Stained Leaves (B9) <u>X</u> High Water Table (A2) <u> </u> Aquatic Fauna (B13) <u>X</u> Saturation (A3) <u> </u> Marl Deposits (B15) <u> </u> Water Marks (B1) <u> </u> Hydrogen Sulfide Odor (C1) <u> </u> Sediment Deposits (B2) <u>X</u> Oxidized Rhizospheres on Living Roots (C3) <u> </u> Drift Deposits (B3) <u> </u> Presence of Reduced Iron (C4) <u> </u> Algal Mat or Crust (B4) <u> </u> Recent Iron Reduction in Tilled Soils (C6) <u> </u> Iron Deposits (B5) <u> </u> Thin Muck Surface (C7) <u> </u> Inundation Visible on Aerial Imagery (B7) <u> </u> Other (Explain in Remarks) <u> </u> Sparsely Vegetated Concave Surface (B8)	<u>Secondary Indicators (minimum of two required)</u> <u> </u> Surface Soil Cracks (B6) <u> </u> Drainage Patterns (B10) <u> </u> Moss Trim Lines (B16) <u> </u> Dry-Season Water Table (C2) <u> </u> Crayfish Burrows (C8) <u> </u> Saturation Visible on Aerial Imagery (C9) <u> </u> Stunted or Stressed Plants (D1) <u> </u> Geomorphic Position (D2) <u> </u> Shallow Aquitard (D3) <u> </u> Microtopographic Relief (D4) <u>X</u> FAC-Neutral Test (D5)
Field Observations: Surface Water Present? Yes <u> </u> No <u>X</u> Depth (inches): <u> </u> Water Table Present? Yes <u>X</u> No <u> </u> Depth (inches): <u>9</u> Saturation Present? Yes <u>X</u> No <u> </u> Depth (inches): <u>0</u> (includes capillary fringe)	Wetland Hydrology Present? Yes <u>X</u> No <u> </u>
Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available:	
Remarks:	

VEGETATION – Use scientific names of plants.

 Sampling Point: DP 1

Tree Stratum (Plot size: <u>30'x100'</u>)	Absolute % Cover	Dominant Species?	Indicator Status																	
1. <u>Acer rubrum</u>	<u>10</u>	<u>Yes</u>	<u>FAC</u>	Dominance Test worksheet: Number of Dominant Species That Are OBL, FACW, or FAC: <u>4</u> (A) Total Number of Dominant Species Across All Strata: <u>5</u> (B) Percent of Dominant Species That Are OBL, FACW, or FAC: <u>80.0%</u> (A/B)																
2. <u>Acer negundo</u>	<u>5</u>	<u>Yes</u>	<u>FAC</u>																	
3. <u>Tilia americana</u>	<u>5</u>	<u>Yes</u>	<u>FACU</u>																	
4. <u>Fraxinus pennsylvanica</u>	<u>2</u>	<u>No</u>	<u>FACW</u>																	
5. <u>Ulmus americana</u>	<u>2</u>	<u>No</u>	<u>FACW</u>																	
6. <u>Acer saccharinum</u>	<u>1</u>	<u>No</u>	<u>FACW</u>																	
7. _____	_____	_____	_____																	
		<u>25</u>	=Total Cover																	
Sapling/Shrub Stratum (Plot size: <u>15'</u> radius)																				
1. <u>Acer negundo</u>	<u>2</u>	<u>No</u>	<u>FAC</u>	Prevalence Index worksheet: <table style="width: 100%;"> <tr> <th style="width: 40%;">Total % Cover of:</th> <th style="width: 60%;">Multiply by:</th> </tr> <tr> <td>OBL species <u>20</u></td> <td>x 1 = <u>20</u></td> </tr> <tr> <td>FACW species <u>32</u></td> <td>x 2 = <u>64</u></td> </tr> <tr> <td>FAC species <u>21</u></td> <td>x 3 = <u>63</u></td> </tr> <tr> <td>FACU species <u>17</u></td> <td>x 4 = <u>68</u></td> </tr> <tr> <td>UPL species <u>2</u></td> <td>x 5 = <u>10</u></td> </tr> <tr> <td>Column Totals: <u>92</u></td> <td>(A) <u>225</u> (B)</td> </tr> <tr> <td colspan="2">Prevalence Index = B/A = <u>2.45</u></td> </tr> </table>	Total % Cover of:	Multiply by:	OBL species <u>20</u>	x 1 = <u>20</u>	FACW species <u>32</u>	x 2 = <u>64</u>	FAC species <u>21</u>	x 3 = <u>63</u>	FACU species <u>17</u>	x 4 = <u>68</u>	UPL species <u>2</u>	x 5 = <u>10</u>	Column Totals: <u>92</u>	(A) <u>225</u> (B)	Prevalence Index = B/A = <u>2.45</u>	
Total % Cover of:	Multiply by:																			
OBL species <u>20</u>	x 1 = <u>20</u>																			
FACW species <u>32</u>	x 2 = <u>64</u>																			
FAC species <u>21</u>	x 3 = <u>63</u>																			
FACU species <u>17</u>	x 4 = <u>68</u>																			
UPL species <u>2</u>	x 5 = <u>10</u>																			
Column Totals: <u>92</u>	(A) <u>225</u> (B)																			
Prevalence Index = B/A = <u>2.45</u>																				
2. _____	_____	_____	_____																	
3. _____	_____	_____	_____																	
4. _____	_____	_____	_____																	
5. _____	_____	_____	_____																	
6. _____	_____	_____	_____																	
7. _____	_____	_____	_____																	
		<u>2</u>	=Total Cover																	
Herb Stratum (Plot size: <u>5'</u> radius)																				
1. <u>Saururus cernuus</u>	<u>20</u>	<u>Yes</u>	<u>OBL</u>	Hydrophytic Vegetation Indicators: <u>1</u> - Rapid Test for Hydrophytic Vegetation <u>X</u> 2 - Dominance Test is >50% <u>X</u> 3 - Prevalence Index is ≤3.0 ¹ <u>4</u> - Morphological Adaptations ¹ (Provide supporting data in Remarks or on a separate sheet) <u> </u> Problematic Hydrophytic Vegetation ¹ (Explain) ¹ Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic.																
2. <u>Symphyotrichum lanceolatum</u>	<u>20</u>	<u>Yes</u>	<u>FACW</u>																	
3. <u>Sanicula canadensis</u>	<u>10</u>	<u>No</u>	<u>FACU</u>																	
4. <u>Elymus virginicus</u>	<u>3</u>	<u>No</u>	<u>FACW</u>																	
5. <u>Impatiens capensis</u>	<u>3</u>	<u>No</u>	<u>FACW</u>																	
6. <u>Ranunculus hispidus</u>	<u>3</u>	<u>No</u>	<u>FAC</u>																	
7. <u>Carex sp. (C. blanda or C. grisea?)</u>	<u>2</u>	<u>No</u>	_____																	
8. <u>Asarum canadense</u>	<u>1</u>	<u>No</u>	<u>UPL</u>																	
9. <u>Cinna arundinacea</u>	<u>1</u>	<u>No</u>	<u>FACW</u>																	
10. <u>Galium aparine</u>	<u>1</u>	<u>No</u>	<u>FACU</u>																	
11. _____	_____	_____	_____																	
12. _____	_____	_____	_____																	
		<u>64</u>	=Total Cover																	
Woody Vine Stratum (Plot size: <u>30'x100'</u>)																				
1. <u>Celastrus orbiculatus</u>	<u>1</u>	<u>No</u>	<u>UPL</u>	Definitions of Vegetation Strata: Tree – Woody plants 3 in. (7.6 cm) or more in diameter at breast height (DBH), regardless of height. Sapling/shrub – Woody plants less than 3 in. DBH and greater than or equal to 3.28 ft (1 m) tall. Herb – All herbaceous (non-woody) plants, regardless of size, and woody plants less than 3.28 ft tall. Woody vines – All woody vines greater than 3.28 ft in height.																
2. <u>Parthenocissus quinquefolia</u>	<u>1</u>	<u>No</u>	<u>FACU</u>																	
3. <u>Vitis riparia</u>	<u>1</u>	<u>No</u>	<u>FAC</u>																	
4. _____	_____	_____	_____																	
		<u>3</u>	=Total Cover																	

Remarks: (Include photo numbers here or on a separate sheet.)
 Platanus occidentalis on slope adjacent to wetland, not in plot.

Hydrophytic Vegetation Present?	Yes <u>X</u>	No _____
----------------------------------------	--------------	----------

SOIL

Sampling Point	DP 1
----------------	------

[illegible]

WETLAND DETERMINATION DATA FORM – Northcentral and Northeast Region

Project/Site: Elkhart CR 18/13/115 Intersection City/County: Elkhart County Sampling Date: 11/7/2018
 Applicant/Owner: Troyer Group/INDOT State: IN Sampling Point: DP 2
 Investigator(s): S. Namestnik, N. Pilla Section, Township, Range: Sec 14, Twp 37 N, Rng 5 E
 Landform (hillside, terrace, etc.): hillslope Local relief (concave, convex, none): convex Slope %: 10
 Subregion (LRR or MLRA): LRR L, MLRA 98 Lat: 41°39'33.51"N Long: 85°54'57.17"W Datum: WGS 84
 Soil Map Unit Name: Gravelton loam, 0 to 1% slopes, frequently flooded, long duration (GodAI) NWI classification: NA
 Are climatic / hydrologic conditions on the site typical for this time of year? Yes X No (If no, explain in Remarks.)
 Are Vegetation , Soil , or Hydrology significantly disturbed? Are "Normal Circumstances" present? Yes X No
 Are Vegetation , Soil , or Hydrology naturally problematic? (If needed, explain any answers in Remarks.)

SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.

Hydrophytic Vegetation Present? Yes <u> </u> No <u>X</u> Hydric Soil Present? Yes <u> </u> No <u>X</u> Wetland Hydrology Present? Yes <u> </u> No <u>X</u>	Is the Sampled Area within a Wetland? Yes <u> </u> No <u>X</u> If yes, optional Wetland Site ID: <u> </u>
Remarks: (Explain alternative procedures here or in a separate report.) Photo 3 east; upland adjacent to W-001-PFO	

HYDROLOGY

Wetland Hydrology Indicators: <u>Primary Indicators (minimum of one is required; check all that apply)</u> ___ Surface Water (A1) ___ Water-Stained Leaves (B9) ___ High Water Table (A2) ___ Aquatic Fauna (B13) ___ Saturation (A3) ___ Marl Deposits (B15) ___ Water Marks (B1) ___ Hydrogen Sulfide Odor (C1) ___ Sediment Deposits (B2) ___ Oxidized Rhizospheres on Living Roots (C3) ___ Drift Deposits (B3) ___ Presence of Reduced Iron (C4) ___ Algal Mat or Crust (B4) ___ Recent Iron Reduction in Tilled Soils (C6) ___ Iron Deposits (B5) ___ Thin Muck Surface (C7) ___ Inundation Visible on Aerial Imagery (B7) ___ Other (Explain in Remarks) ___ Sparsely Vegetated Concave Surface (B8)	<u>Secondary Indicators (minimum of two required)</u> ___ Surface Soil Cracks (B6) ___ Drainage Patterns (B10) ___ Moss Trim Lines (B16) ___ Dry-Season Water Table (C2) ___ Crayfish Burrows (C8) ___ Saturation Visible on Aerial Imagery (C9) ___ Stunted or Stressed Plants (D1) ___ Geomorphic Position (D2) ___ Shallow Aquitard (D3) ___ Microtopographic Relief (D4) ___ FAC-Neutral Test (D5)
Field Observations: Surface Water Present? Yes <u> </u> No <u>X</u> Depth (inches): <u> </u> Water Table Present? Yes <u> </u> No <u>X</u> Depth (inches): <u> </u> Saturation Present? Yes <u> </u> No <u>X</u> Depth (inches): <u> </u> (includes capillary fringe)	Wetland Hydrology Present? Yes <u> </u> No <u>X</u>
Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available: Remarks: No saturation or water table within 16" in pit.	

VEGETATION – Use scientific names of plants.

 Sampling Point: DP 2

Tree Stratum (Plot size: <u>30'x100'</u>)	Absolute % Cover	Dominant Species?	Indicator Status																	
1. <u>Celtis occidentalis</u>	30	Yes	FAC	Dominance Test worksheet: Number of Dominant Species That Are OBL, FACW, or FAC: <u>3</u> (A) Total Number of Dominant Species Across All Strata: <u>6</u> (B) Percent of Dominant Species That Are OBL, FACW, or FAC: <u>50.0%</u> (A/B)																
2. <u>Quercus alba</u>	15	Yes	FACU																	
3. <u>Acer negundo</u>	5	No	FAC																	
4. <u>Carya glabra</u>	3	No	FACU																	
5. <u>Ulmus americana</u>	1	No	FACW																	
6. _____																				
7. _____																				
	54	=Total Cover		Prevalence Index worksheet: <table style="width: 100%;"> <thead> <tr> <th style="width: 40%;">Total % Cover of:</th> <th style="width: 60%;">Multiply by:</th> </tr> </thead> <tbody> <tr> <td>OBL species <u>0</u></td> <td>x 1 = <u>0</u></td> </tr> <tr> <td>FACW species <u>6</u></td> <td>x 2 = <u>12</u></td> </tr> <tr> <td>FAC species <u>62</u></td> <td>x 3 = <u>186</u></td> </tr> <tr> <td>FACU species <u>42</u></td> <td>x 4 = <u>168</u></td> </tr> <tr> <td>UPL species <u>23</u></td> <td>x 5 = <u>115</u></td> </tr> <tr> <td>Column Totals: <u>133</u> (A)</td> <td><u>481</u> (B)</td> </tr> <tr> <td colspan="2" style="text-align: center;">Prevalence Index = B/A = <u>3.62</u></td> </tr> </tbody> </table>	Total % Cover of:	Multiply by:	OBL species <u>0</u>	x 1 = <u>0</u>	FACW species <u>6</u>	x 2 = <u>12</u>	FAC species <u>62</u>	x 3 = <u>186</u>	FACU species <u>42</u>	x 4 = <u>168</u>	UPL species <u>23</u>	x 5 = <u>115</u>	Column Totals: <u>133</u> (A)	<u>481</u> (B)	Prevalence Index = B/A = <u>3.62</u>	
Total % Cover of:	Multiply by:																			
OBL species <u>0</u>	x 1 = <u>0</u>																			
FACW species <u>6</u>	x 2 = <u>12</u>																			
FAC species <u>62</u>	x 3 = <u>186</u>																			
FACU species <u>42</u>	x 4 = <u>168</u>																			
UPL species <u>23</u>	x 5 = <u>115</u>																			
Column Totals: <u>133</u> (A)	<u>481</u> (B)																			
Prevalence Index = B/A = <u>3.62</u>																				
Sapling/Shrub Stratum (Plot size: <u>15' radius</u>)																				
1. <u>Acer negundo</u>	15	Yes	FAC																	
2. <u>Prunus serotina</u>	15	Yes	FACU																	
3. <u>Celtis occidentalis</u>	5	No	FAC																	
4. <u>Juglans nigra</u>	3	No	FACU																	
5. <u>Euonymus alatus</u>	2	No	UPL																	
6. <u>Fraxinus pennsylvanica</u>	2	No	FACW																	
7. <u>Lonicera maackii</u>	1	No	UPL																	
	43	=Total Cover		Hydrophytic Vegetation Indicators: <u>1</u> - Rapid Test for Hydrophytic Vegetation <u>2</u> - Dominance Test is >50% <u>3</u> - Prevalence Index is ≤3.0 ¹ <u>4</u> - Morphological Adaptations ¹ (Provide supporting data in Remarks or on a separate sheet) <u> </u> Problematic Hydrophytic Vegetation ¹ (Explain) ¹ Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic.																
Herb Stratum (Plot size: <u>5' radius</u>)																				
1. <u>Asarum canadense</u>	15	Yes	UPL																	
2. <u>Fraxinus pennsylvanica</u>	3	Yes	FACW																	
3. <u>Euonymus hamiltonianus</u>	2	No	UPL																	
4. <u>Glechoma hederacea</u>	2	No	FACU																	
5. <u>Acer negundo</u>	1	No	FAC																	
6. <u>Celtis occidentalis</u>	1	No	FAC																	
7. <u>Chelidonium majus</u>	1	No	UPL																	
8. <u>Euonymus alatus</u>	1	No	UPL																	
9. <u>Glechoma hederacea</u>	1	No	FACU																	
10. <u>Hackelia virginiana</u>	1	No	FACU																	
11. <u>Lonicera maackii</u>	1	No	UPL																	
12. <u>Osmorhiza longistylis</u>	1	No	FACU																	
	32	=Total Cover		Definitions of Vegetation Strata: Tree – Woody plants 3 in. (7.6 cm) or more in diameter at breast height (DBH), regardless of height. Sapling/shrub – Woody plants less than 3 in. DBH and greater than or equal to 3.28 ft (1 m) tall. Herb – All herbaceous (non-woody) plants, regardless of size, and woody plants less than 3.28 ft tall. Woody vines – All woody vines greater than 3.28 ft in height.																
Woody Vine Stratum (Plot size: <u>30'x100'</u>)																				
1. <u>Parthenocissus quinquefolia</u>	1	No	FACU																	
2. <u>Smilax hispida</u>	1	No	FAC																	
3. <u>Toxicodendron radicans</u>	1	No	FAC																	
4. <u>Vitis riparia</u>	1	No	FAC																	
	4	=Total Cover																		

Remarks: (Include photo numbers here or on a separate sheet.)

Additional species present in herbaceous stratum: Smilax hispida, 1%, FAC; Toxicodendron radicans, 1%, FAC

SOIL

Sampling Point DP 2

Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)								
Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type ¹	Loc ²		
0-6	10YR 2/1	99					Loamy/Clayey	sandy loam; 1% unnamed colors (sand grains)
6-11	10YR 3/2	99					Loamy/Clayey	sandy loam; 1% unnamed colors (sand grains)
11-16	10YR 4/4	100					Sandy	

¹Type: C=Concentration, D=Depletion, RM=Reduced Matrix, MS=Masked Sand Grains.

²Location: PL=Pore Lining, M=Matrix.

Hydric Soil Indicators:			Indicators for Problematic Hydric Soils ³ :		
<input type="checkbox"/> Histosol (A1)	<input type="checkbox"/> Polyvalue Below Surface (S8) (LRR R,	<input type="checkbox"/> 2 cm Muck (A10) (LRR K, L, MLRA 149B)			
<input type="checkbox"/> Histic Epipedon (A2)	<input checked="" type="checkbox"/> MLRA 149B)	<input type="checkbox"/> Coast Prairie Redox (A16) (LRR K, L, R)			
<input type="checkbox"/> Black Histic (A3)	<input type="checkbox"/> Thin Dark Surface (S9) (LRR R, MLRA 149B)	<input type="checkbox"/> 5 cm Mucky Peat or Peat (S3) (LRR K, L, R)			
<input type="checkbox"/> Hydrogen Sulfide (A4)	<input type="checkbox"/> High Chroma Sands (S11) (LRR K, L)	<input type="checkbox"/> Polyvalue Below Surface (S8) (LRR K, L)			
<input type="checkbox"/> Stratified Layers (A5)	<input type="checkbox"/> Loamy Mucky Mineral (F1) (LRR K, L)	<input type="checkbox"/> Thin Dark Surface (S9) (LRR K, L)			
<input type="checkbox"/> Depleted Below Dark Surface (A11)	<input type="checkbox"/> Loamy Gleyed Matrix (F2)	<input type="checkbox"/> Iron-Manganese Masses (F12) (LRR K, L, R)			
<input type="checkbox"/> Thick Dark Surface (A12)	<input type="checkbox"/> Depleted Matrix (F3)	<input type="checkbox"/> Piedmont Floodplain Soils (F19) (MLRA 149B)			
<input type="checkbox"/> Sandy Mucky Mineral (S1)	<input type="checkbox"/> Redox Dark Surface (F6)	<input type="checkbox"/> Mesic Spodic (TA6) (MLRA 144A, 145, 149B)			
<input type="checkbox"/> Sandy Gleyed Matrix (S4)	<input type="checkbox"/> Depleted Dark Surface (F7)	<input type="checkbox"/> Red Parent Material (F21)			
<input type="checkbox"/> Sandy Redox (S5)	<input type="checkbox"/> Redox Depressions (F8)	<input type="checkbox"/> Very Shallow Dark Surface (F22)			
<input type="checkbox"/> Stripped Matrix (S6)	<input type="checkbox"/> Marl (F10) (LRR K, L)	<input type="checkbox"/> Other (Explain in Remarks)			
<input type="checkbox"/> Dark Surface (S7)					

³Indicators of hydrophytic vegetation and wetland hydrology must be present, unless disturbed or problematic.

Restrictive Layer (if observed):		Hydric Soil Present?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
Type: _____				
Depth (inches): _____				

Remarks:
This data form is revised from Northcentral and Northeast Regional Supplement Version 2.0 to include the NRCS Field Indicators of Hydric Soils, Version 7.0, 2015 Errata. (http://www.nrcs.usda.gov/Internet/FSE_DOCUMENTS/nrcs142p2_051293.docx)



U.S. Fish and Wildlife Service

National Wetlands Inventory

Des No. 18111 CR 18 & 13 & 115



U.S. Fish and Wildlife Service, National Standards and Support Team,
wetlands_team@fws.gov

January 28, 2019

Wetlands

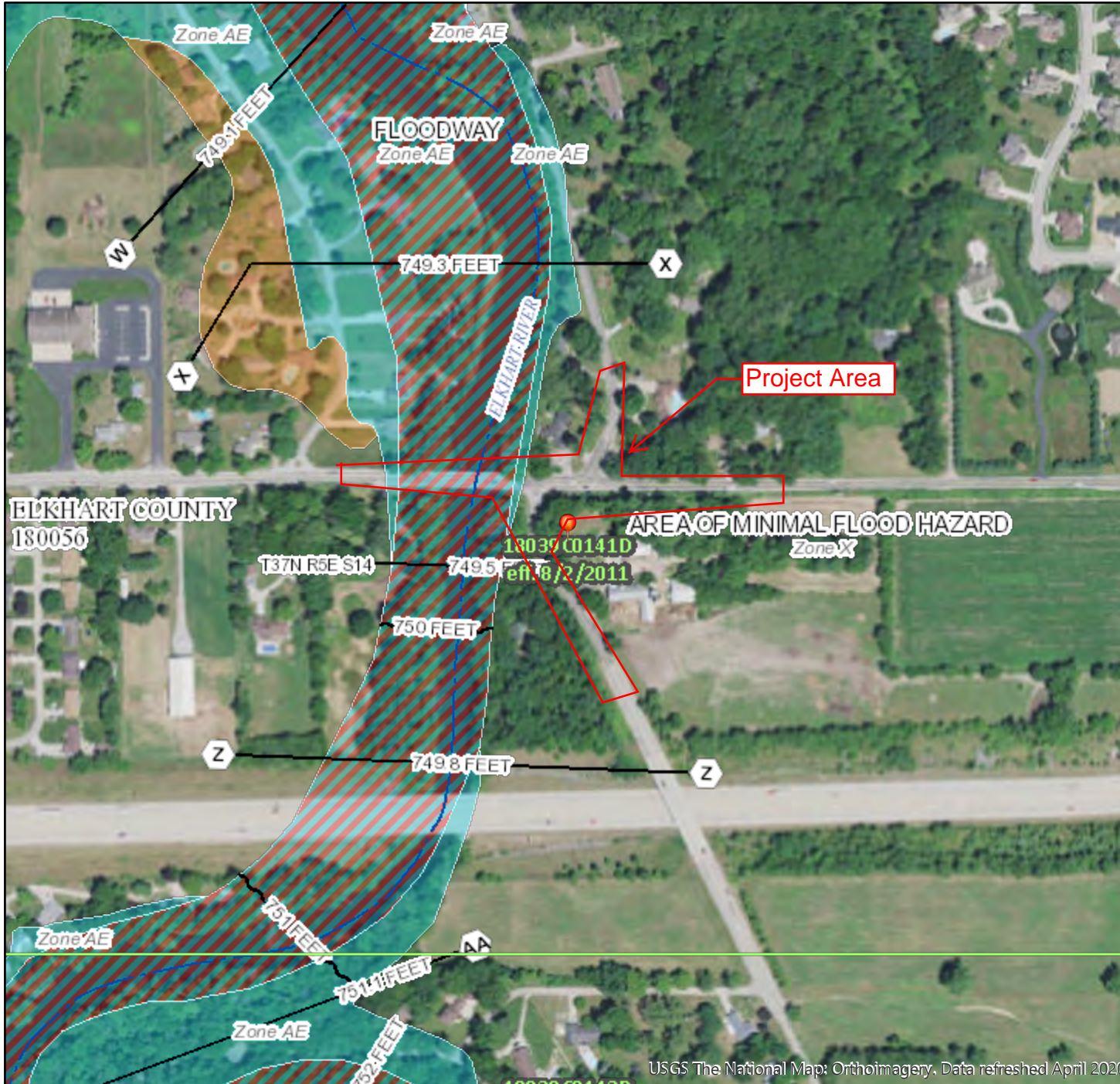
	Estuarine and Marine Deepwater		Freshwater Emergent Wetland		Lake
	Estuarine and Marine Wetland		Freshwater Forested/Shrub Wetland		Other
	Estuarine and Marine Wetland		Freshwater Pond		Riverine

This map is for general reference only. The US Fish and Wildlife Service is not responsible for the accuracy or currentness of the base data shown on this map. All wetlands related data should be used in accordance with the layer metadata found on the Wetlands Mapper web site.

National Flood Hazard Layer FIRMette



85°55'14"W 41°39'46"N



USGS The National Map: Orthoimagery. Data refreshed April 2020

85°54'37"W 41°39'19"N

Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AR
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
GENERAL STRUCTURES		Area of Undetermined Flood Hazard Zone D
		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall
OTHER FEATURES		Cross Sections with 1% Annual Chance Water Surface Elevation
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
		Coastal Transect Baseline
MAP PANELS		Profile Baseline
		Hydrographic Feature
		Digital Data Available
		No Digital Data Available
		Unmapped



The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 6/19/2020 at 3:04 PM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

0 250 500 1,000 1,500 2,000 Feet

1:6,000

Appendix F-28



Indiana Floodplain Information Portal Report

Point of Interest

Approximate Address:

57505 County Road 13
ELKHART, IN 46516

Effective Flood Zone:

X

Preliminary Flood Zone:

N/A

Best Available Flood Zone:

X

Approximate Flood Elevation:

749.4ft NAVD88

Source:

Zone AE Profile Delineation

Nearest Stream:

Elkhart River

Map Legend



Point of Interest



Nearest Point on Stream

Best Available Flood Zone



FEMA Zone AE Floodway



DNR Detailed Floodway



DNR Approximate Floodway



FEMA Zone A



FEMA Zone AE



DNR Detailed Fringe



DNR Approximate Fringe



Additional Floodplain Area



FEMA Protected by Levee



FEMA Floodplain - Ponding (Depth)



FEMA Floodplain - Sheet Flow (Depth)

Site Map with Best Available Flood Zone



Approximate scale 1:2,400

Disclaimer

APPENDIX G

Public Involvement



18111-00

October 25, 2018

Notice of Entry for Survey or Investigation
CR 18 at CR 13/CR 115 Intersection Improvement Project
Des. No. 1401749
Elkhart County, Indiana

Dear Property Owner,

Our information indicates that you own or occupy property near this proposed intersection improvement project. Troyer Group and its subconsultant will be performing surveys and investigations of the project area in the near future. It may be necessary for field crews to come onto your property to complete this work. This is allowed by law by Indiana Code IC 8-23-7-26. They will show you their identification, if you are available, before coming onto your property. If you have sold this property, or it is occupied by someone else, please let us know the name and address of the new owner or current occupant so we can contact them about the surveys and investigations.

At this stage we generally do not know what effect, if any, our project may eventually have on your property. If we determine later that your property is involved, we will contact you with additional information.

Survey work survey work may include archaeological and wetlands investigations (which may include excavation of small shovel test probes), and various other environmental studies. The survey is needed for the proper planning and design of this project. Please be assured of our sincere desire to cause you as little inconvenience as possible during this survey. If any problems do occur, please contact our field crew or contact me at the phone number or address shown herein.

Sincerely,

C.J. Cunningham
Urban Planner
574-259-9976
cjc@troyergroup.com

*It comes down to people and their experiences | **Together, We Will***

550 Union St. | Mishawaka, IN 46544 | p 574.259.9976 | www.troyergroup.com



18111-00

September 23, 2019

Notice of Entry for Survey or Investigation
CR 18 at CR 13/CR 115 Intersection Improvement Project
Des. No. 1401749
Elkhart County, Indiana

Dear Property Owner,

Our information indicates that you own or occupy property near this proposed intersection improvement project. Troyer Group and its subconsultant will be performing surveys and investigations of the project area in the near future. It may be necessary for field crews to come onto your property to complete this work. This is allowed by law by Indiana Code IC 8-23-7-26. They will show you their identification, if you are available, before coming onto your property. If you have sold this property, or it is occupied by someone else, please let us know the name and address of the new owner or current occupant so we can contact them about the surveys and investigations.

At this stage we generally do not know what effect, if any, our project may eventually have on your property. If we determine later that your property is involved, we will contact you with additional information.

Survey work survey work may include archaeological and wetlands investigations (which may include excavation of small shovel test probes), and various other environmental studies. The survey is needed for the proper planning and design of this project. Please be assured of our sincere desire to cause you as little inconvenience as possible during this survey. If any problems do occur, please contact our field crew or contact me at the phone number or address shown herein.

Sincerely,

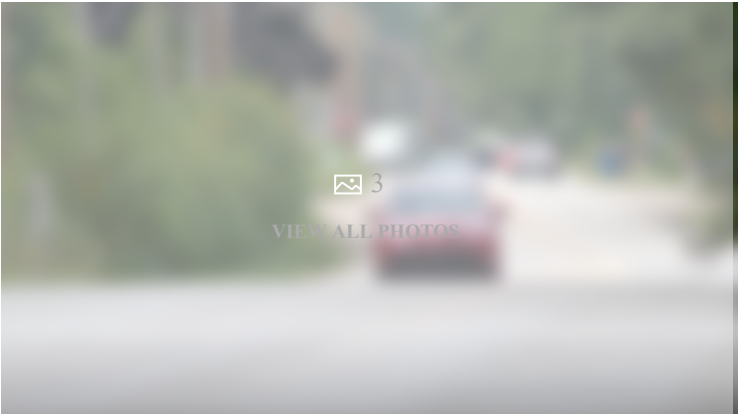
C.J. Cunningham
Manager – Environmental Services
574-259-9976
cjc@troyergroup.com

It comes down to people and their experiences | **Together, We Will**

550 Union St. | Mishawaka, IN 46544 | p 574.259.9976 | www.troyergroup.com

Officials looking to add roundabout at complicated intersection along CR 18 near Elkhart

by Lauren Becker, WSBT 22 Reporter
Tuesday, August 13th 2019



The county highway department is looking to add a single roundabout to fix the out-of-alignment double intersection of County Road 13, 18 and 115. // WSBT 22

ELKHART COUNTY — A \$2 million project is in the works for a complicated intersection near Elkhart.

The county highway department is looking to add a single roundabout to fix the out-of-alignment double intersection of County Road 13, 18 and 115.

This area sees a lot of traffic congestions because there are two stop signs within a couple hundred feet of each other along County Road 18.

Two roads “T” into County Road 18 from both north and south causing for a lot of traffic, but this future project hopes to improve this area.

It’s a constant flow of cars along County Road 18 just outside of Elkhart. But as soon as those cars hit this intersection, it’s a quick halt at the stop sign, only to do it again in a couple hundred feet.

“We have left turn accidents or rear end accidents, things like that at that one intersection,” said Kent Schumacher, Elkhart County engineer.

County Road 115 Ts in from the South and a few yards away County Road 13 Ts in from the north.

The intersection had a lot of accidents, so trying to make improve the safety and the efficiency of the intersection,” said Schumacher.

To do that, the county highway department is looking to construct a peanut roundabout.

“It’s in essence shaped like a peanut or a figure eight and will incorporate both intersections without having to purchase a whole lot of right-of-way,” said Schumacher.

Keeping traffic moving in all four directions while improving safety. Something the assistant superintendent of Concord Community Schools is on board with.

“Especially in transportation getting kids from home to school and then school back. It’s everyday unification in transportation is what we say,” said Scott Kovatch, assistant superintendent of Concord Schools.

Down the road from the intersection is the Concord Township Fire Department. The fire chief says crews don’t have time to sit in traffic while responding to calls.

“It will help us speed up our response times. We aren’t stuck in traffic trying to get around people because lanes are all blocked,” said Chief Phil Sumpter, Concord Township Fire Department.

He is worried construction of the roundabout will slow response time down, but it's a project people in the area say will make a difference.

It's a \$2.1 million project being federally funded.

A public hearing is set for September 12 where people can find out more about the project.

A time and location are not yet scheduled. Construction is expected to begin by February of 2021.

September 4, 2019

[Property Owner]

[Address]

Re: Notice of Planned Improvements for CR18-CR13-CR115 Intersection, Elkhart County, IN

Dear Property Owner:

Our information indicates that you own or occupy property near the proposed roadway intersection improvement projects. This letter is to inform you of an opportunity to provide public comment on the proposed improvements.

Elkhart County Highway Department is developing plans for the improvement to the intersection of County Road 18, 13, and 115, east of Elkhart in Concord Township, T37N, R5E, Section 14 in Elkhart County. Project limits begin at the bridge crossing the Elkhart River on CR 18, extend approximately 500 feet east from the intersection, 275 feet north from the intersection along CR 13, and 700 feet south from the intersection along CR 115.

The need of the project stems from operational safety issues that exist at this intersection. From January 2014 to May 2016, the intersection experienced 17 crashes. The probable cause of these crashes is excessive speed, inadequate advanced warning signs for intersection, or a large total intersection traffic volume.

The purpose of the project is to increase the operation safety of the intersection by replacing the existing unsignalized intersection with a safer alternative while still maintaining, or improving, operational capacity for the projected traffic volume demand. The proposed scope for improvement involves construction of an elongated roundabout. The improvement will increase the safety of the intersection as well as provide a facility capable of handling the anticipated traffic growth.

During construction, the intersection will be closed and through traffic will be detoured utilizing Toledo Rd. to the north, County Road 17 to the east, and County Road 45 to the south and west. Access will be maintained to adjacent properties within the project limits throughout the duration of project.

Right-of-way will need to be acquired as part of the project. Total construction cost of the project is estimated at \$1,709,011.01. At present, it is planned to use both federal and local funds for the construction of the project.

A red flag investigation found 19 NWI – Wetlands, 12 lakes, and 1 River within a half mile of the project. The proposed alternative will not have any impacts on any of these items of concern. If impacts become necessary, then the INDOT Environmental Services Division will be consulted.


INDOT has agreed that this project falls within the guidelines of a Categorical Exclusion Level 4.

The preliminary design plans, along with environmental document and other information, are being made available for review at the following locations:

1. Elkhart County Highway Department, 610 Steury Avenue, Goshen, IN 46528, Engineering Office, Phone: (574)533-0538
2. The Troyer Group, Inc., 550 Union St., Mishawaka, IN 46544, Chris Waidner, P.E., Project Engineer, Phone (574) 229-3629.

Further, a survey crew will complete the road right-of-way staking necessary for the intersection improvements soon. We would like to notify you of this activity in accordance with Indiana Code Section 8-23-7-27, which requires our office to give notification to the property owner of entrance on their land to perform various surveys and/or investigations. The specific dates of staking are not yet known, but owners of property where staking will be necessary will receive a follow up letter to provide advanced notice of entry.

If you prefer, you may contact our office within fourteen (14) days of the date of this letter to schedule a meeting to discuss details of the project.



Christopher L. Waidner, P.E.
Director of TransportationThe Troyer Group, Inc.
550 Union Street
Mishawaka, IN 46544
Phone: 574-229-3629
Fax: 574-254-4048
Email: clw@troyergroup.com

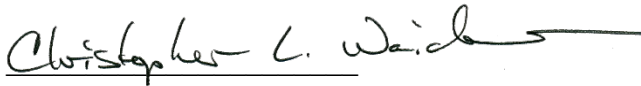
CJC/cc
G:\projects\PS\Elkhart County\18111-00 CR18-CR13-CR115\Correspondence\Public Info Mtg\Property Owner Letters.doc

September 6, 2019

[Property Owner]
[Address]

CR 18 at CR 13/CR 115 Intersection Improvement Project Public Meeting

The focus of the intersection improvement project is to increase the safety and functionality of the intersection. The project is currently scheduled to begin in the Spring of 2021. The Elkhart County Highway Department will be holding a public meeting at Crossroads Community Church, 57415 Alpha Drive, Goshen on Thursday, September 12 from 6:00 - 8:00 PM. The meeting will consist of a short presentation regarding the proposed improvements to the intersection and an opportunity for public and feedback and discussion following the presentation.



Christopher L. Waidner, P.E.

Director of Transportation

The Troyer Group, Inc.

550 Union Street

Mishawaka, IN 46544

Phone: 574-229-3629

Fax: 574-254-4048

Email: clw@troyergroup.com

No.	Owner	Miling Address	Property Address
1	JAHNCKE STEPHEN M	23488 COUNTY RD 18 , ELKHART IN 46516	23488 COUNTY RD 18
2	KIWANIS CLUB OF ELKHART	PO Box 802 , Elkhart IN 465150802	57565 COUNTY RD 115
3	WEEBER KENNETH L TRUSTEE WEEBER FAM TRUST	57564 County Road 115 , Goshen IN 465289079	57564 COUNTY RD 115
4	BOPE TONY E	57505 County Road 13 , Elkhart IN 465166334	57505 COUNTY RD 13
5	WHITMAN ANDREW	57484 COUNTY ROAD 13 , ELKHART IN 465169100	57484 COUNTY RD 13
6	PEDROZA VICTOR & MARIA DELALUZ PEDROZA (H&W)	23339 County Road 18 , Goshen IN 465289080	23339 COUNTY RD 18
7	MACK KENNETH C & LINDA A	23309 County Road 18 , Goshen IN 465289080	23309 COUNTY RD 18
8	GOOD ANNA RUTH (NAFZIGER) AS TRUSTEE OF THE ANNE RUTH (NAFZIGER) GOOD LI TR ½ ETAL	23257 COUNTY ROAD 18 , GOSHEN IN 46528	23257 COUNTY RD 18
9	ADAMS THOMAS R & LISA J	57457 County Road 13 , Elkhart IN 465169164	57457 COUNTY RD 13

AFFP

CR 18 at CR 13/CR 115 MEETING

Affidavit of Publication

STATE OF INDIANA }
COUNTY OF ELKHART }

SS

CR 18 at CR 13/CR 115 Intersection
Improvement Project Public Meeting

Angelique Petersen, being duly sworn, says:

That she is Legal Clerk of the Elkhart Truth, a Daily newspaper of general circulation, printed and published in Elkhart, Elkhart County, Indiana; that the publication, a copy of which is attached hereto, was published in the said newspaper on the following dates:

September 06, 2019

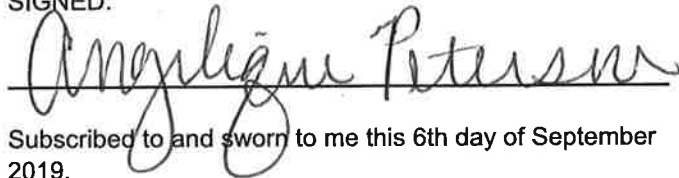
The focus of the intersection improvement project is to increase the safety and functionality of the intersection. The project is currently scheduled to begin in the Spring of 2021.

The Elkhart County Highway Department will be holding a public meeting at Crossroads Community Church, 57415 Alpha Drive, Goshen on Thursday, September 12 from 6:00 - 8:00 PM. The meeting will consist of a short presentation regarding the proposed improvements to the intersection and an opportunity for public feedback and discussion following the presentation.
hspaxlp

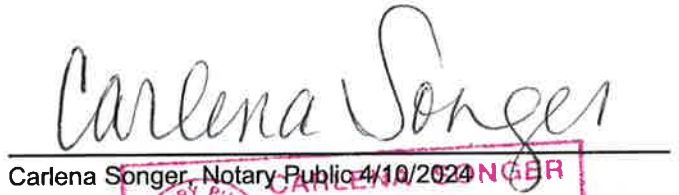
Publisher's Fee: \$ 11.88

That said newspaper was regularly issued and circulated on those dates.

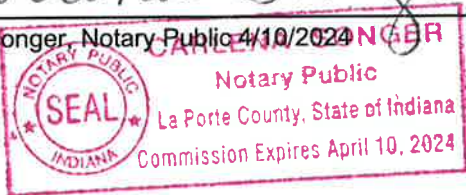
SIGNED:



Subscribed to and sworn to me this 6th day of September 2019.



Carlana Songer, Notary Public 4/10/2024



60170803 61070667

TROYER GROUP (lg)
550 UNION ST.
MISHAWAKA, IN 46544

CR 18 @ CR115/CR13 Intersection Improvement

Public Information Meeting

- Crossroads Community Church
- Thursday, September 12, 2019
- 6:00 p.m.



1

Welcome

- Purpose/explanation of public information meeting
- Informational handouts
- Post meeting follow up
- Project display area



2

C.R. 18 at C.R. 115/C.R. 13

Introductions

Elkhart County
• Project Owner

Troyer Group
• Consultant

- Notice Publishing:
Elkhart Truth, September 6
- A meeting notice was mailed to known property owners within project area
- Sign-in at attendance table to be added to project mailing list
- A copy of the presentation and project documentation will be available on-line via Elkhart County Highway website



3

Project Stakeholders

- Elkhart County Highway Department
- Elected & Local Officials
- Residents and Citizens
- Commuters
- Businesses
- Emergency Services, including Concord Fire Department
- Schools, including Concord East Side Elementary School
- Churches
- Community Organizations



4

Project Resource Locations

- | | |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------|
| <ul style="list-style-type: none"> • Elkhart County Highway Department 610 Steury Ave, Goshen, IN 46528 • http://elkcohw.dreamhosters.com/ • Contact: Kent Schumacher Phone: 574-533-0538 Email: eng@elkcohw.org • Elkhart Public Library – Dunlap Branch 58485 CR 13, Elkhart, IN 46516 • Phone: (574) 875-3100 • Elkhart County Government 117 N. Second St., Goshen, IN 46526 • Phone: (574) 534-3541 (switchboard number) | <ul style="list-style-type: none"> • Troyer Group 550 Union St, Mishawaka, IN 46544 • Phone: (574) 259-9976 |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------|



5

Existing Intersection



6

Need for Project

- Frequency of accidents occurring at/near intersection and level of service illustrate the need for intersection improvement
 - Accident data:
 - 17 total accidents between January 2014 to May 2016
 - Eight (47%) involve rear end collisions
 - Two (12%) involved side-swipe collisions
 - The probable cause of these crashes could be excessive speed, inadequate advanced warning signs for intersection, or a large total intersection traffic volume.



7

Need for Project

- Awareness of the intersection ahead may be a major concern, due to limited intersection sight distance and short storage lane on CR 18 between CR 115 and CR 13
- Delays at the intersection increase response times for nearby Concord Township Fire Department
- Traffic along CR 18 increasing



8

Project Purpose

Increasing the operational safety at the intersection by replacing the existing unsignalized intersection with a safer alternative while still maintaining, or improving, operational capacity for the projected traffic volume.

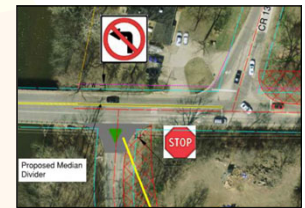


9

Alternatives Considered

#1 - Unsignalized Intersection with CR 115 Right-In/Right-Out (RI/RO)

- Installation of a median on CR 18 to eliminate the left-turn movement onto CR 115
- CR 18 westbound left turn traffic and southbound thru traffic from CR 13 onto CR 115 would be rerouted to use Old CR 17 to the east



Alternative #1

*Images and Alternatives from Final Engineer's Assessment Report, dated February 2018, prepared by DLZ



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Alternatives Considered

#2 - Unsignalized Intersection with Realignment of CR 115

- CR 115 would be realigned parallel to the US 20 Bypass, turning north and intersecting with CR 18 at Apple Ridge Way
- Level of Service would improve by cutting down delays
- Significant right-of-way would need to be acquired



Alternative #2

*Images and Alternatives from Final Engineer's Assessment Report, dated February 2018, prepared by DLZ



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Alternatives Considered

#3 - Roundabout with Two Intersections

- The intersection of CR 13 and CR 18 would be converted to a roundabout
- CR 115 approach would be like the first alternative as RIRO
- Intersection safety would be improved by reducing rear end collisions on CR 18



Alternative #3

*Images and Alternatives from Final Engineer's Assessment Report, dated February 2018, prepared by DLZ



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Alternatives Considered

#4 - Consolidated Roundabout

- Similar to alternative #3, however CR 115 would be realigned to be included in the roundabout.
- Would require significant right-of-way acquisition



Alternative #4

*Images and Alternatives from Final Engineer's Assessment Report, dated February 2018, prepared by DLZ



13

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Alternatives Considered

#5 - Traffic Signal

- CR 115 would be converted to RI/RO
- Existing alignments would be maintained for all approaches, the driveway in the northwest corner would need to be relocated.



Alternative #5

*Images and Alternatives from Final Engineer's Assessment Report, dated February 2018, prepared by DLZ



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Alternatives Considered

#6 - Coordinated Traffic Signals

- All existing approaches and movements maintained
- Signals coordinated by a single controller
- Additional signal heads and raised signal heads would account for sight distance.
- Signal would function similar to Oakland Ave – Indiana Ave.



Alternative #6

*Images and Alternatives from Final Engineer's Assessment Report, dated February 2018, prepared by DLZ



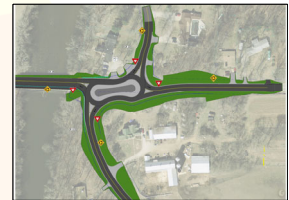
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15

Alternatives Considered

#7 - Peanut Roundabout

- Elongated roundabout to include all four approaches
- Would require significantly less right-of-way acquisition than Alternative #4



Alternative #7

*Images and Alternatives from Final Engineer's Assessment Report, dated February 2018, prepared by DLZ

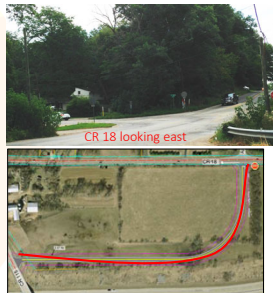


16

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Analysis Considerations

- Sight Distance
 - Alternatives #1, 2, 5, & 6 do not correct the inadequate sight distances near the intersection due to significant grades
- Right-of-Way Acquisition
 - Alternatives #2 and #4 require significant acquisition, which increases project cost
- Traffic Patterns
 - Alternatives #1, 3, & 5 significantly alter existing traffic patterns by limiting CR 115 to RI/RO



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Preferred Alternative – Peanut Roundabout



Peanut Roundabout

- Improves safety and capacity
- Allows for adequate sight distance
- Allows for all traffic movements
- Minimizes right-of-way acquisitions



18

18

Roundabout Elements

- Meets purpose & need of project
- Enhances safety by:
 - Reducing the number of potential vehicle conflict points
 - Reduce the severity of traffic accidents
- Enhances operational efficiency at the intersection
- One-way circular intersection
- Traffic flows counter-clockwise around a center island
- Yield at entrance
- No Parking
- No "activity" in center island

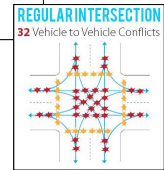
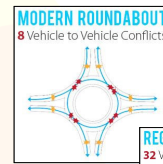


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transportation

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Benefits of Roundabouts

- **Enhances Safety**
 - Roundabouts reduce the number of potential accident points within an intersection
 - 75% fewer conflict points than four-way intersections
 - Significantly reduces the potential for "head-on" and "T-bone" collisions
- **Slower vehicle speeds**
 - Reduces the severity of crashes
- **Efficient traffic flow**
 - Reduces need for turn lanes
 - Improves traffic flow
- **Community benefits**
 - Reduces congestion
 - Aesthetic landscaping



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Roundabouts Enhance Safety

US DOT Federal Highway Administration Statistics

Traditional intersections account for:

- 45% of all crashes - FHWA
- 33% of all traffic fatalities - FHWA

Compared to traditional intersections roundabouts:

- Require vehicles to travel at lower speeds
- Reduce fatalities and injuries by 82% - FHWA
- Reduce total crashes by 44% - FHWA

For more information:

<http://safety.fhwa.dot.gov/intersection/innovative/roundabouts/>



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Approaching the Roundabout (example)



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Approaching the Roundabout (example)



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Approaching the Roundabout (example)



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Project Schedule

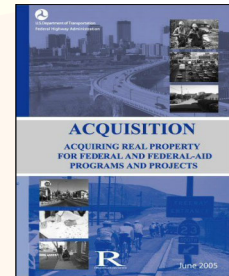
- Public Information Meeting: September 12, 2019
- Public comments requested by COB 10/11/19
- Troyer Group review and consideration of comments – Winter 2019
 - Finalize environmental document
 - Finalize design
- Real estate acquisition phase – Spring 2020
- Construction: 2021



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Real Estate Acquisition Process

- "Uniform Act of 1970"
 - All federal, state and local governments must comply
 - Requires an offer for just compensation
 - Project proposal requires acquisition from 6 parcels



32

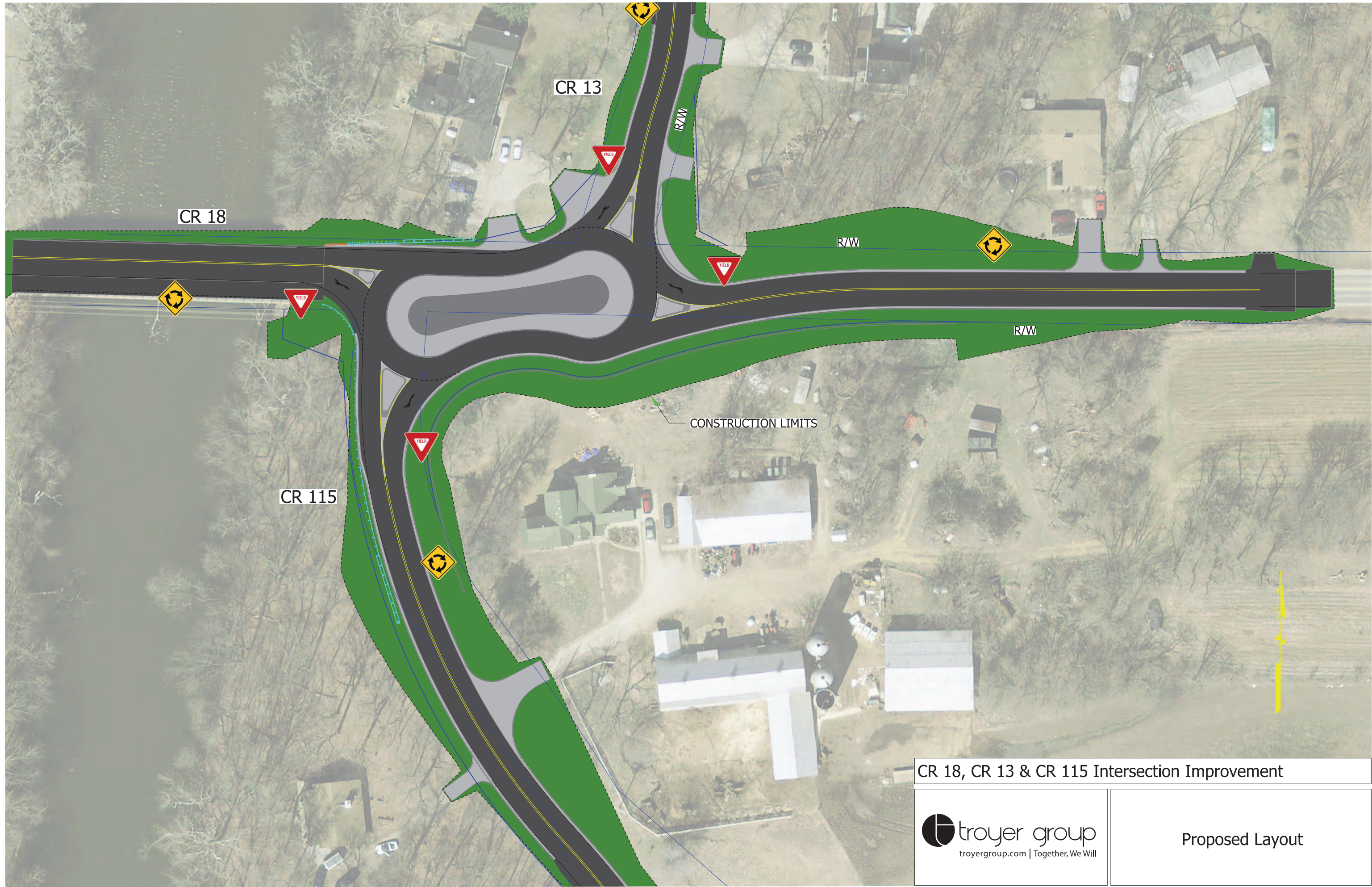
Public Comment Session

- Please visit with County Troyer Group staff following the public comment session
- Project Open House
 - Project maps, displays, real estate acquisition table, project team, and informal Q & A

Thank You For Attending!



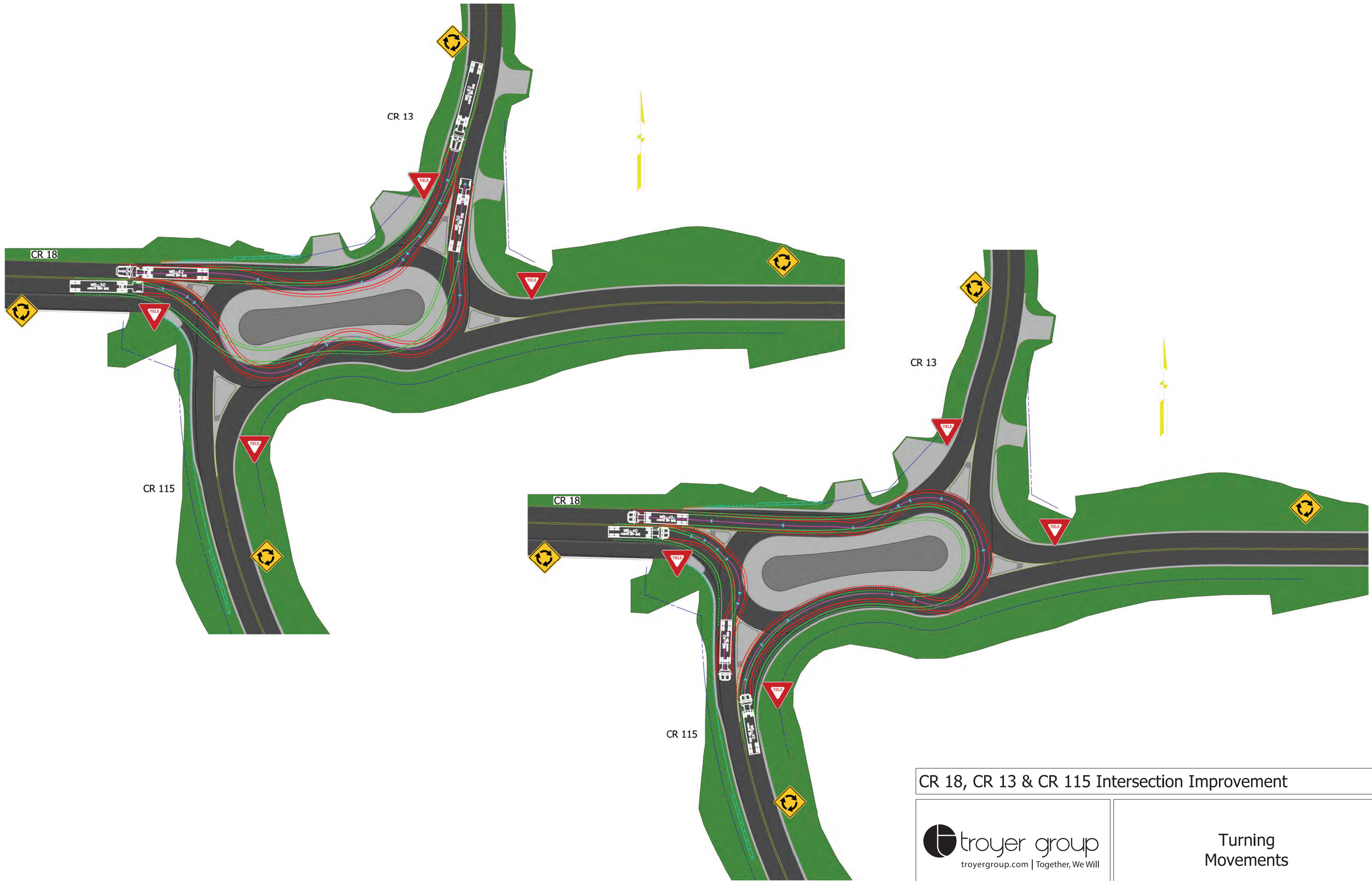
33



CR 18, CR 13 & CR 115 Intersection Improvement

 **troyer group**
troyergroup.com | Together, We Will

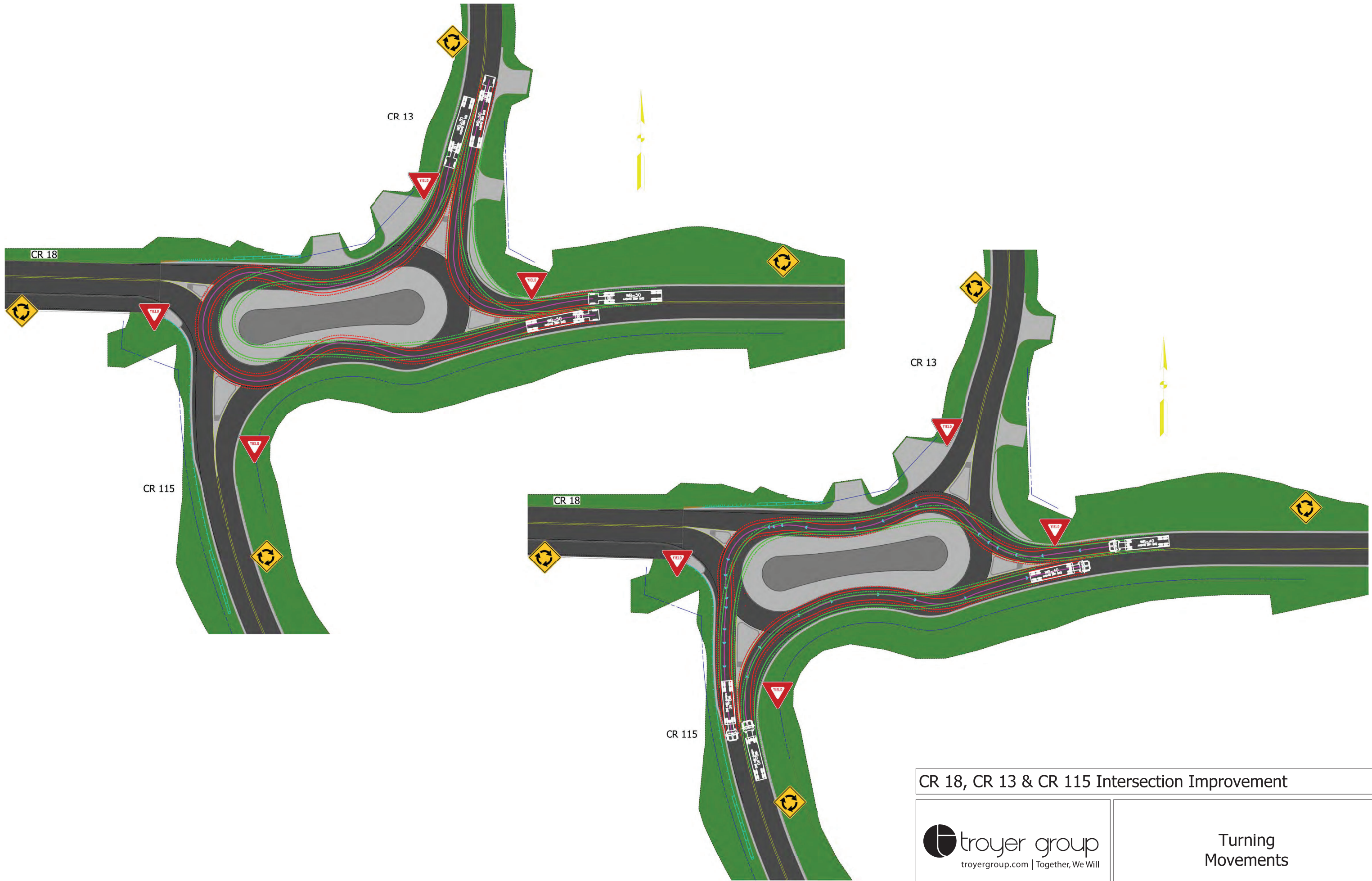
Proposed Layout



CR 18, CR 13 & CR 115 Intersection Improvement



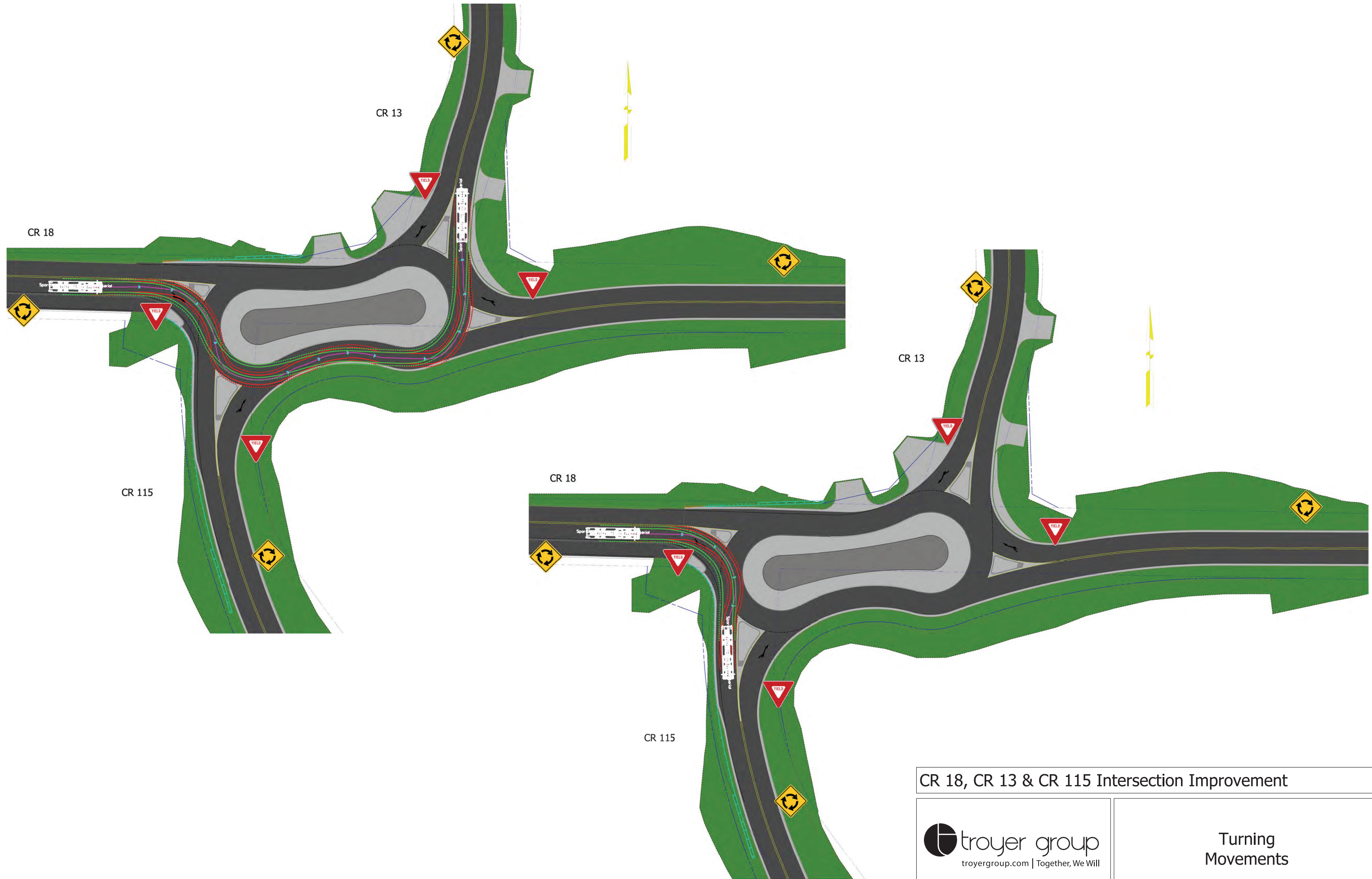
Turning
Movements



CR 18, CR 13 & CR 115 Intersection Improvement



Turning
Movements



CR 18, CR 13 & CR 115 Intersection Improvement



Turning
Movements

Des. No. 1401749
CR 18 at CR 13/CR115, Intersection Improvement
Elkhart County, Indiana
September 12, 2019 - Public Information Meeting Public
Comments Summary

Concord Fire Department

Comment: Detour will result in one-minute response time becoming 20+ minute response

Response: County to work with Fire Department on setting up temporary staging area east of the bridge (Ivy Tech). This was done the last time the bridge was closed for work.

Justification / Crash Rate

Comment: Crashes only shown for the study from 2014-2016. What were the crashes for the initial study, and the years since 2016?

Response: Additional crash data provided by County

Comment: Public does not think that this intersection is dangerous. Majority of accidents due to impaired drivers and that these accidents should not count.

Response: Roundabouts reduce the number of conflicts (opportunities to be hit by impaired drivers) by 75%.

Comment: Sight lines don't matter that much in safety of the intersection.

Response: Stopping sight distance at a horizontal or vertical curve is judged to be one of the most critical indicators of a roads safety and overall serviceability by the Federal Highway Administration (FHWA).

Comment: Reduce posted speed limits as means to improve safety.

Response: Speed limit on CR 18 is 35 mph
Speed limit on CR 13 is 30 mph
Speed limit on CR 115 is 40 mph

Comment: Speeding problem in area will lead to accidents at the intersection.

Response: Geometry of roundabout's curbs designed to ensure motorists have to slow down to safely traverse.

Comment: Motorcycles sling shooting through roundabout at highspeed for fun

Response: Thank you for the comment. Some racing around roundabout circulating roadway has been witnessed, but this is the exception. Not the norm.

Justification / Traffic

Comment: Public does not think that this intersection is congested.

Response: Present projected traffic data

Comment: How will traffic patterns change?

Response: Traffic through intersection will be free flowing. County add additional context stated regarding other improvements along CR 18.

Justification / Cost

Comment: Other less-expensive solutions like signalization or No Build

Response: Preliminary signal design did not address steep grade of CR 18 east of intersection, or horizontal sight distance issues along CR 115. Improving these elements would bring the cost of a signal in-line with the roundabout and possibly exceed it.

Response: Introduction of a signal brings the potential for red-light running and the fatal head on / T-Bone crashes associate with it.

Comment: Other intersections more in need of the money that would instead be spent here

Response: From County

Comment: Why this intersection and not another one

Response: From County

Utilities

Comment: Too many utilities to make major construction practical. Are relocations even possible?

Response: TG has been in coordination with all utilities. None have expressed any issues with the project.

Comment: Who will pay for relocations? Will this come out of local utility bills?

Response: Utilities. Costs will not impact local utility bills

Comment: Public does not think that hill can be cut down due to gas main

Response: TG has been in coordination with NIPSCO gas. Utility has not expressed any concerns with the project at this time.

Detour

Comment: Motorists will use unofficial detours through nearby neighborhoods. Leading to increased traffic and increased speeds on neighborhood roads.

Response: County will work with sheriff to add extra patrols in the area to help keep neighborhood speeds down.

Comment: Changes to school bus route?

Response: County in communication with Schools so that they have ample time to adjust their routes to accommodate closure.

General comments

Comment: Public does not think that a roundabout would be better than the existing intersection.

Response: Single roundabout is more efficient at handling traffic than current separated stop-controlled intersections. Existing intersection legs operate with a Level of Service between A and D with most at D (CR 115 northbound is a D during PM peak hour). Roundabout would operate at a Level of Service A for all legs.

Comment: General concern about traffic backups (queuing)

Response: Intersection is currently experiencing back ups due to the inefficiency of the stop control and their offset. This is reflected in the LOS of D for CR 115. Free-flowing

continuous traffic pattern associated with roundabouts will minimize the number and length of backups. This is reflected in its LOS of A for all roads.

Comment: General concern about local's ability (esp. property owners I believe) to get out onto road?

Response: See answer above

Comment: General concern about the bus stop (both during detour and how roundabout would affect it)

Response: Vehicles stop for buses at a roundabout the same that they do at any other section of roadway. Slower speeds associated with roundabouts provides more time to stop. Roundabouts have been installed at entrances to funeral homes (Portage Ave, South Bend) and their funeral processions with no issues.

Comment: Concern that farming equipment wouldn't be able to make the turn – esp. combines. Don't think that it is a good idea to have large equipment/trucks mount the curb.

Response: Intersection designed to accommodate large equipment/trucks. See attached turning movements. Curbing is shorter and more rounded than normal curb, similar to subdivision roll curb at the driveway.

Comment: Existing concerns about CR 18 hill east of intersection not being resolved by new plans.

Response: Plan addresses the severity of CR 18 hills grade at the intersection to make sure it is conducive to the braking and accelerating associated with an intersection. Roundabout maintains a traffic calming measure to help keep speeds down and in context with the topography (hills) of the corridor.

LEGAL NOTICE OF PLANNED IMPROVEMENT

DES. # 1401749

Elkhart County is developing plans to improve the safety of the intersection of County Road (CR) 18 CR 13 / CR 115, southeast of the City of Elkhart, Concord Township.

The purpose of this proposed project is to increase the operational safety and efficiency at the intersection. The project is needed because the existing offset “T” intersections are experiencing safety concerns due to limited driver sight distance throughout the intersection and short storage lanes along CR 18 between CR 13 and CR 115.

The project proposes to construct an elongated, “peanut-shaped” roundabout. Grading will be necessary outside of the roadway shoulder to ensure proper slopes are achieved prior to tying into existing grades. A retaining wall will be constructed on the southeast corner of the intersection. CR 115 will be realigned for a maximum of 600 ft to help improve sight distance. Lighting improvements located along the roundabout approaches will also be included. The storm sewer drainage system will be replaced, and drainage will be diverted to the same location as before with a new 24” outlet.

The proposed construction of this project will require 0.87 acre of new permanent right-of-way and 0.65 acre of temporary right-of-way.

The maintenance of traffic (MOT) plan proposes a full closure of the intersection during construction with a signed detour route. Access through the construction site for emergency vehicles from the nearby Concord Township Fire Department is being coordinated. All properties will remain accessible during all phases of construction but no other through traffic will be permitted. School corporations and emergency services will be notified of closures prior to construction. The proposed start of construction is spring 2022.

The cost associated with this project is approximately \$3,675,000, which includes preliminary engineering, right-of-way, and construction. Both federal and local funds are anticipated to be used.

The Federal Highway Administration (FHWA) and INDOT have agreed this project falls within the guidelines of a Categorical Exclusion (CE) Level 2 environmental document. Preliminary design plans along with the CE are available for review at the following locations:

1. Elkhart Public Library, Dunlap Branch, 58485 C.R. 13, Elkhart, IN 46516. Documents will be available during all library operating hours. Face coverings and physical distancing will be required of all visitors.
2. Elkhart County Highway Department, Elkhart County Highway Department 610 Steury Avenue, Goshen, IN 46528. Documents will be available Monday through Friday, 7:00 AM to 4:00 PM. Face coverings and physical distancing will be required of all visitors.

3. Troyer Group Office, 3930 Edison Lakes Pkwy, Mishawaka, IN 46545. Documents will be available Monday through Thursday, 8:00 AM to 5:00 PM; Friday, 8:00 AM to 12:00 PM. Face coverings and physical distancing will be required of all visitors.
4. Documents are also available for online viewing at the following web address:
<https://tinyurl.com/y22o63kf>

All interested persons may request a public hearing be held and/or express their concerns by submitting comments to: Troyer Group, ATTN: C.J. Cunningham, 3930 Edison Lakes Pkwy, Mishawaka, IN 46545 or cjc@troyergroup.com, (574) 259-9976. We respectfully request comments be submitted by 5:00 pm November 20, 2020.

In accordance with the Americans with Disabilities Act (ADA), persons and/or groups requiring project information be made available in alternative formats are encouraged to contact Elkhart County for the arrangement and coordination of services. Please contact Katie Niblock, Elkhart County Highway, (574) 534-9394, kniblock@elkcohw.org. In accordance with Title VI of the Civil Rights Act of 1964, persons and/or groups requiring project information be made available in another language are encouraged to contact Katie Niblock, Elkhart County Highway, (574) 534-9394, kniblock@elkcohw.org.

This notice is published in compliance with: 1) Code of Federal Regulations, Title 23, Section 771 (CFR 771.111(h)(1) stating, “Each State must have procedures approved by the FHWA to carry out a public involvement/public hearing program.”; 2) 23 CFR 450.210(a)(1)(ix) stating, “Provide for the periodic review of the effectiveness of the public involvement process to ensure that the process provides full and open access to all interested parties and revise the process, as appropriate.”; and 3) The INDOT *Public Involvement Policies and Procedures* approved by the Federal Highway Administration on August 16, 2012.

AFFP
DES. #1401749

Affidavit of Publication

STATE OF INDIANA }
COUNTY OF ELKHART } SS

Courtney Kruger, being duly sworn, says:

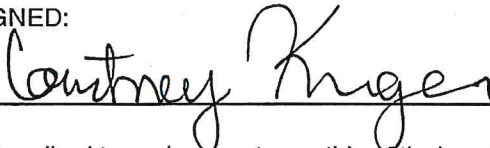
That she is Legal Clerk of the Elkhart Truth, a Daily newspaper of general circulation, printed and published in Elkhart, Elkhart County, Indiana; that the publication, a copy of which is attached hereto, was published in the said newspaper on the following dates:

November 04, 2020
November 12, 2020

Publisher's Fee: \$ 105.52

That said newspaper was regularly issued and circulated on those dates.

SIGNED:



Subscribed to and sworn to me this 12th day of November 2020.


Julie Gaines, Notary Public 05/24/2024

60170803 61157668



TROYER GROUP (lg)
550 UNION ST.
MISHAWAKA, IN 46544

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DES. # 1401749

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The proposed construction of this project will require 0.87 acre of new permanent right-of-way and 0.65 acre of temporary right-of-way.

The maintenance of traffic (MOT) plan proposes a full closure of the intersection during construction with a signed detour route. Access through the construction site for emergency vehicles from the nearby Concord Township Fire Department is being coordinated. All properties will remain accessible during all phases of construction but no other through traffic will be permitted. School corporations and emergency services will be notified of closures prior to construction. The proposed start of construction is spring 2022.

The cost associated with this project is approximately \$3,675,000, which includes preliminary engineering, right-of-way, and construction. Both federal and local funds are anticipated to be used.

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hspaxlp

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The proposed construction of this project will require 0.87 acre of new permanent right-of-way and 0.65 acre of temporary right-of-way.

The maintenance of traffic (MOT) plan proposes a full closure of the intersection during construction with a signed detour route. Access through the construction site for emergency vehicles from the nearby Concord Township Fire Department is being coordinated. All properties will remain accessible during all phases of construction but no other through traffic will be permitted. School corporations and emergency services will be notified of closures prior to construction. The proposed start of construction is spring 2022.

The cost associated with this project is approximately \$3,675,000, which includes preliminary engineering, right-of-way, and construction. Both federal and local funds are anticipated to be used.

The Federal Highway Administration (FHWA) and INDOT have agreed this project falls within the guidelines of a Categorical Exclusion (CE) Level 2 environmental document. Preliminary design plans along with the CE are available for review at the following locations:

1. Elkhart Public Library, Dunlap Branch, 58485 C.R. 13, Elkhart, IN 46516. Documents will be available during all library operating hours. Face coverings and physical distancing will be required of all visitors.
2. Elkhart County Highway Department, Elkhart County Highway Department 610 Steury Avenue, Goshen, IN 46528. Documents will be available Monday through Friday, 7:00 AM to 4:00 PM. Face coverings and physical distancing will be required of all visitors.
3. Troyer Group Office, 3930 Edison Lakes Pkwy, Mishawaka, IN 46545. Documents will be available Monday through Thursday, 8:00 AM to 5:00 PM; Friday, 8:00 AM to 12:00 PM. Face coverings and physical distancing will be required of all visitors.

It comes down to people and their experiences | Together, We Will

550 Union St. | Mishawaka, IN 46544 | p 574.259.9976 | www.troyergroup.com

4. Documents are also available for online viewing at the following web address:
<https://tinyurl.com/y22o63kf>

All interested persons may request a public hearing be held and/or express their concerns by submitting comments to: Troyer Group, ATTN: C.J. Cunningham, 3930 Edison Lakes Pkwy, Mishawaka, IN 46545 or cjc@troyergroup.com, (574) 259-9976. We respectfully request comments be submitted by 5:00 pm November 20, 2020.

In accordance with the Americans with Disabilities Act (ADA), persons and/or groups requiring project information be made available in alternative formats are encouraged to contact Elkhart County for the arrangement and coordination of services. Please contact Katie Niblock, Elkhart County Highway, (574) 534-9394, kniblock@elkcohw.org. In accordance with Title VI of the Civil Rights Act of 1964, persons and/or groups requiring project information be made available in another language are encouraged to contact Katie Niblock, Elkhart County Highway, (574) 534-9394, kniblock@elkcohw.org.

This notice is published in compliance with: 1) Code of Federal Regulations, Title 23, Section 771 (CFR 771.111(h)(1) stating, “Each State must have procedures approved by the FHWA to carry out a public involvement/public hearing program.”; 2) 23 CFR 450.210(a)(1)(ix) stating, “Provide for the periodic review of the effectiveness of the public involvement process to ensure that the process provides full and open access to all interested parties and revise the process, as appropriate.”; and 3) The INDOT *Public Involvement Policies and Procedures* approved by the Federal Highway Administration on August 16, 2012.



Recipient List – Local Elected Officials
Legal Notice of Planned Improvement
Des. 1401749 - CR 18 at CR 13/CR 115 - Intersection Improvement

First name	Last name	Title
Frank	Lucchese	Vice President, County Commissioners
Mike	Yoder	President, County Commissioners
Suzanne	Weirick	County Commissioner
John K.	Letherman	President, County Council
David L.	Hess	Vice President, County Council
Doug S.	Graham	County Council
Darryl J.	Riegsecker	County Council
Randall	Yohn	County Council
Thomas	Stump	County Council
Tina M.	Wenger	County Council
James E.	Weeber	Concord Township Trustee
Cathy	Searcy	Elkhart County Assessor
Christopher	Anderson	Elkhart County Clerk
Patricia A.	Pickens	Elkhart County Auditor
Charlie	McKenzie	Director of Highway Department
Chris	Godlewski	Director of Planning and Development
Tina M.	Bontrager	Treasurer
Phl	Sumpter	Concord Township Fire Chief

CJ Cunningham

From: Katie Niblock <kniblock@elkcohw.org>
Sent: Friday, November 20, 2020 4:06 PM
To: CJ Cunningham
Subject: Fwd: Planned improvement at CR 18/CR 13 and CR 115

Follow Up Flag: Follow up
Flag Status: Flagged

----- Forwarded message -----

From: Katie Niblock <kniblock@elkcohw.org>
Date: Tue, Nov 10, 2020 at 11:43 AM
Subject: Re: Planned improvement at CR 18/CR 13 and CR 115
To: Steven Brown ·

Hi Steven,

Yes, you are receiving the letter because of the right of way to be acquired.

The project proposes to acquire 0.17 acre of new r/w (fee simple) and 0.127 acre of temporary r/w for drive construction and grading.

The building is outside of the r/w to be acquired by the project.

Please let me know if you have any other questions.

Thanks,
Katie

On Mon, Nov 9, 2020 at 3:48 PM Steven Brown ·

Katie,

I am with the Kiwanis Club of Elkhart and we own the Scout Cabin that is southwest of the planned improvement of this intersection. I went to the website listed on this letter. I am assuming that we are getting this letter because some of the acreage you will be needing is on the north end of our parcel of land that houses our Boy Scout Cabin. We own the land on the west side of 115 between the bypass and CR 18.

Do you know yet, how much of our parcel of land you will need and if it will effect the structure that is on the property?

Thanks.

STEVEN BROWN | COMMERCIAL REL MGR III, VP
t: 574-389-2014 | c: 574-596-1498 | f: 574-389-2001
Steven.Brown@oldnational.com
Old National Bank
320 N. Main St | Elkhart, IN 46516



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--
Katie Niblock, PE
Elkhart County Highway
610 Steury Avenue
Goshen, IN 46528
Ph: 574.534.9394
Fax: 574.533.7103

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Katie Niblock, PE
Elkhart County Highway
610 Steury Avenue
Goshen, IN 46528
Ph: 574.534.9394
Fax: 574.533.7103



Concord Township Fire Department

23625 COUNTY ROAD 18
ELKHART, IN 46516
PHONE & FAX (574) 875-9644
EMERGENCY 9-1-1

11/9/2020

Troyer Group

NOV 16 2020

Received

Mr. Cunningham,

As per your email and after speaking with our Township Trustee, I am writing to request a public hearing regarding the proposed roundabout project planned for the intersection of CR 18, CR 115, & CR 13 located in Concord Township, Elkhart, IN. We have several concerns and questions we would like to present. If you have any further questions, please do not hesitate to contact me.

Respectfully,

Phil Sumpter- Fire Chief
Concord Township Fire Department

CONCORD TOWNSHIP TRUSTEE

JIM WEEBER, TRUSTEE

2804 Toledo Road
Elkhart, IN 46516
November 13, 2020

Troyer Group

NOV 16 2020

Received

Troyer Group ATTN: C.J. Cunningham
3930 Edison Lakes Pkwy
Mishawaka, Indiana 46545

Gentlemen:

Concord Township requests a public hearing regarding the project at the intersection of County Road (CR) 18, CR 13 and CR 115.

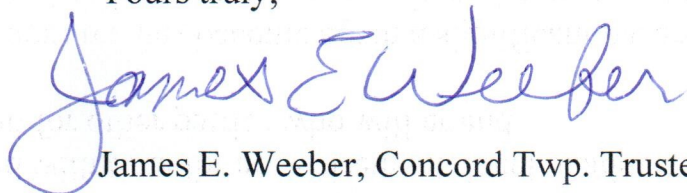
We request the hearing be scheduled with time for advertisement, local resident scheduling and to provide consideration for other parties who will attend.

Currently the Covid 19 situation in our area has become again a significant hazard with 88 Covid patients in the local hospital and ambulance patient receiving being diverted. Additional complications from this situation and national holiday interruptions are present and on the horizon. An in person public hearing will require more than normal accommodations.

A virtual public hearing is unacceptable. Virtual interaction is not an option for numerous taxpayers. Virtual meetings have and continue to not be a good fit for numerous community matters.

Thank you for your attention to this important issue.

Yours truly,



James E. Weeber, Concord Twp. Trustee

TELEPHONE: 574-293-6889 | FACSIMILE: 574-293-6889 | EMAIL: Concordtrustee@gmail.com

ADDRESS: 2804 Toledo Road | Elkhart, IN 46516

CJ Cunningham

From: CJ Cunningham
Sent: Tuesday, January 5, 2021 12:15 PM
To: 'mlg29@frontier.com'
Cc: Christopher Waidner; 'Charles McKenzie'; 'Katie Niblock'
Subject: RE: DES#1401749 - CR18 CR13 CR115 improvement

Good afternoon Mary,

I apologize for the belated response. Your original message was sent to an incorrect email address for me, therefore this did not arrive in my inbox when it was originally sent. However, we wanted to send a brief note to acknowledge receipt of your e-mail. Additionally, your e-mail has been entered into documentation as Elkhart County officials will review and evaluate alternatives to improve this critical transportation infrastructure.

Thank you for attending the September, 2019 public information meeting and for your interest in this project. The County Highway Department took note of the concerns raised during that meeting, but still feels the proposed improvements are a justifiable use of the estimated projects costs. Elkhart County is funding 20% of the project while the remaining 80% is being funded by Federal Highway Administration funds being channeled through the Indiana Department of Transportation.

Roundabouts are becoming a more common traffic control device throughout northern Indiana and their local acceptance tends to increase as more are installed. The City of Elkhart constructed two along Hively Road, directly west of this intersection. Meanwhile, roundabouts have recently been constructed at SR 19/SR 119 in southwest Elkhart County and US 6/SR 13/CR 33 in southeastern Elkhart County. At this intersection, the proposed roundabout seeks to solve the traffic issues associated to the existing stop-controlled intersection without massive geometric changes that would be necessary to either re-align the intersection approaches, or to design a signalized intersection that meets current design standards.

We are aware of the potential for issues with large vehicles such as school buses and fire engines. However, the roundabout design will accommodate navigation by both vehicle types.

Again thank you for submitting comments regarding the intersection proposal for Elkhart County's review and consideration.

C.J. Cunningham | Manager – Environmental Services

cjc@troyergroup.com | w [574.259.9976](tel:574.259.9976) ext.5006
3930 Edison Lakes Pkwy, Mishawaka, IN 46545 | troyergroup.com



----- Forwarded message -----

From: mlg29@frontier.com <mlg29@frontier.com>
Date: Thu, Nov 19, 2020 at 2:41 PM
Subject: DES#1401749 - CR18 CR13 CR115 improvement
To: cjc@troyer-group.com <cjc@troyer-group.com>, kniblock@elkcohw.org <kniblock@elkcohw.org>

I attended a meeting about this at the church. No one there was in favor of putting a round-about at this intersection but it looks like we weren't heard. Spending a ridiculous amount for a round-about there is a waste of the taxpayers money. I don't know anyone that likes them and you already know it will be an issue for school buses and emergency vehicles.

I know this was a waste of my time to write this, but I am sick and tired of my taxes going up every year to pay for the county to waste the money on unnecessary things. I just had to get this off my chest even though I know this waste will continue anyway. At least the county could hire someone to address this project that is from the county and is familiar with and will be affected by this project.

Mary G.



MEETING MINUTES

Project: DES-1401749, CR 18 at CR 13/CR 115, Intersection Improvement, Roundabout
Mtg. Date: December 10, 2020 (1:30 pm)
Issue Date: December 21, 2020 (Draft); January 4, 2021 (Final)
Location: Concord Township Fire Station No. 1
Present: (See attached sign-in sheet)
Copies To: Present, Matt Yarian (INDOT Project Manager), Charlie Mackenzie (Elkhart County Highway), Elkhart County Commissioners

This meeting was arranged by Troyer Group in coordination with the Elkhart County Highway Department. Troyer Group is the design consultant responsible for preparing the construction plans and ensuring environmental compliance for Elkhart County's preferred intersection improvement alternative.

Concord Township Fire Chief Phil Sumpter, and Concord Township Trustee Jim Weeber had requested a public hearing upon receiving the project's "legal notice of planned improvements." Troyer Group invited Mr. Sumpter and Mr. Weeber to a meeting along with the Elkhart County Highway Department to discuss their concerns about the proposed improvement. Mr. Weeber invited other local/elected officials and members of the community to meeting.

Follow up commentary to the meeting's discussion items is in red.

Discussion Items:

- Chris Waidner (Troyer Group Project Manager) provided introductory remarks and explained the primary purpose for the meeting was to learn more about Mr. Weeber's and Mr. Sumpter's concerns as they related to their requests for a public hearing.
- Introductions were made around the room (*refer to attached sign-in sheet*). Mr. Weeber mentioned that he had invited the Elkhart County Commissioners, but they were unable to attend.
- C.J. Cunningham (Troyer Group Environmental Service Manager) summarized the project's public involvement process.
 - The draft environmental document was recently released for public involvement (10/21/2020).
 - This permitted Elkhart County to begin the formal public involvement process, as mandated by INDOT and FHWA due the federal funds involved with the project.
 - Prior to the formal public involvement process, Elkhart County chose to hold a public information meeting, although not required by INDOT or FHWA. The meeting was held much in the same fashion as a formal public hearing, and the preferred roundabout alternative was introduced. Public comments were solicited during the meeting. This meeting occurred in September, 2019.

- Per INDOT and FHWA requirements, a legal notice of planned improvements was published twice in the Elkhart Truth (November 4th and November 12th). The notice offered the public the opportunity to submit comment or to request a public hearing.
- The text of the legal notice was mailed to all affected property owners, and to a list of 18 local and elected officials throughout Elkhart County.
- Four comments were received resulting from the notice, two of which included requests for a public hearing. The requests for a public hearing were submitted by Mr. Sumpter and Mr. Weeber.
- Lynn Loucks (Concord Community Schools bus driver) asked how long the project would take to construct.
 - Chris Waidner answered that the intersection would take essentially one construction season to complete (spring through fall), and the intersection would be closed to general traffic during that time.
 - It was later clarified that this would be the case for any of the improvement alternatives considered, except for the no-build alternative.
- It was noted that 50-60 buses cross the bridge each day.
 - This does not include merit bus trips or special needs bus trips.
 - Chris Waidner said that Maintenance of Traffic (MOT) accommodations will be implemented to ensure regular bus routes could be accommodated by the one-way access through the construction site.
 - The current plan is to have a construction gate with lock and to be opened by contractor. This is done to prevent the general public from using the on-way access route. *(This was discussed in more detail later in the meeting. See below for more detail.)*
- Has the load rating for the CR 18 bridge over the Elkhart River been downgraded?
 - No, but the nearby Yellow Creek Bridge's rating has been downgraded. Mr. Weeber stated that available funds should be focused there instead.
- What's wrong with the existing intersection?
 - C.J. Cunningham cited the project's purpose and need, which is primarily related to traffic efficiency, but also related to driver safety.
 - *(Injury-causing accidents were mentioned as safety concern, but it was later corrected during the meeting that no injury causing accidents have occurred at the intersection during the period analyzed. However, 17 crashes occurred during a two-year period evaluated during project scoping)*
 - PM Peak-hour flow is congested. Meanwhile the county anticipates growth along this corridor which will further diminish the intersection's operational efficiency.
 - Mr. Weeber said he lives next to the intersection and does not witness accidents.
 - Others blamed the accidents that have occurred on drunk drivers.
 - Mr. Weeber invited a local reporter out to the intersection, who agreed that there currently are no issues with the flow of traffic at the intersection.
 - Mr. Weeber stated that the peak hour traffic back-ups clear as a result of driver politeness.
- Sight distance on the vertical crest curve (hill) along CR 18 east of CR 115 was discussed. Kim Martin (Concord Community Schools bus driver) said the only issues she has experienced on the hill were related to ice.

- Chris Waidner noted that the grade of the roundabouts east end is raised to promote a shallower grade from the hill into the intersection to help vehicles stop during bad weather (icy) conditions.
- Recommendations were provided for other improvement alternatives:
 - Re-align CR 13 to create a traditional 4-way stop. The commenter said this would concentrate the right-of-way acquisition to a single parcel in the NW quadrant.
 - Mr. Weeber noted that the roads were once aligned like this, but land was sold to a developer who built two river-front homes west of a newly realigned CR 13.
 - A signalized intersection was proposed.
 - A signalized intersection would require massive grade revisions. If the stop control is removed, CR 18 would need to be designed at full design speed and its vertical curve and sight distance deficiencies corrected.
 - The proposed pavement footprint would also be enlarged relative to the existing intersection and the proposed roundabout because of the necessary inclusion of turning lanes.
- Mr. Weeber provided the following comments:
 - He prefers the no-build alternative, even though he understands the project is primarily federally funded.
 - He feels as though he is being harassed by the County, as this is the third time they have attempted to acquire his property.
 - He cited that the church was 1/3 full (*referring to the September, 2019 public involvement meeting*), and that everybody there opposed the project.
 - He spoke with County Commissioner Suzie Weirick who stated that only two other comments were received from during the public comment period. (*referring to those beyond Mr. Sumpter and Mr. Weeber's request for a hearing*)
 - He cited the truck drivers who appeared at the last meeting that opposed the roundabout, who argue that roundabout make drivers go faster.
 - Believes federal dollars are being spent secretly.
 - Chris Waidner later responded to this comment by explaining how projects are awarded federal funds. Municipalities respond to a call for projects with a report defining their proposed project, why it is needed (safety, capacity, maintenance), and how much it will cost. The project is then scored alongside other projects. The highest scoring projects are awarded funds.
 - Project cost does not justify the resulting tax increase.
 - Prefers the no-build alternative.
 - Favors the private property rights of taxpayers.
 - MACOG is a contributing issue as they are a group of unelected bureaucrats who are making funding decisions concerning Elkhart County's road.
 - County Councilman Stump clarified that MACOG is overseen by a board comprised of local elected officials.
- Concord School Corporation bus drivers expressed their concerns:
 - Concerned about running over curbs and the discomfort that would cause their student passengers.
 - Chris Waidner said the roundabout is designed with mountable curbs intended to be used by only the largest of vehicles. Vehicle simulations/analysis have shown that

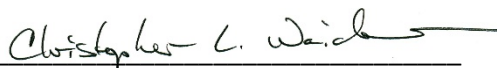
school buses will be able to maneuver the roundabout without mounting the curbs
(see attached analysis).

- Maneuvering the roundabout will require too many changes of direction and will be difficult to navigate with a bus.
 - Chris Waidner noted that the shape of the roundabout is such that it mimics the existing path of the heaviest movements (eastbound CR 18 turning left onto northbound CR 13 and southbound CR 13 turning right onto westbound CR 18). Vehicles on side roads (CR 13 and CR 115) will have to travel farther and make more steering wheel corrections than currently, but there are few vehicles making these maneuvers and in the case of CR 115, their access onto CR 18 will be improved.
- The current maximum wait to get through the intersection during peak traffic is about 90 seconds.
- Chris Waidner introduced the proposed strategy that involves requiring the contractor to maintain an access route through the construction site for the duration of construction.
 - The route would be paved with aggregate or recycled asphalt.
 - Access to/through the route would be limited, most likely by means of a locked gate.
- Concerns were raised regarding the unlocking of the construction gate during emergency runs.
 - It was cited that the Concord Township Fire Department makes over 1600 runs per year.
 - The emergency response units cannot be responsible for unlocking the gate themselves during an emergency run.
 - Chris Waidner said the responsibility will be placed upon the contractor to provide access control at night or at time when construction activity is not actively ongoing. This will become part of their bid and specifications will be enforced or else the contractor will incur significant monetary damages.
 - Added signage and barricades west of the fire station may limit traffic between the fire station and the intersection.
 - The temporary construction access must accommodate the movement of a ladder truck.
 - Fire Chief Phil Sumpter stressed the importance of the fire department being involved in the regular construction progress meetings. They need to be made aware of all changes in access.
 - Coordination with emergency and bus services is noted on the plans and unique special provisions. They will be invited to the project's pre-construction meeting and progress meetings. Contractor will also be required to notify emergency and bus services of any changes to their access through the site and work out means to accommodate the change.
 - Maintenance of traffic specifications need to ensure continual emergency access is maintained in every direction through the intersection.
 - A county councilman mentioned the possibility of added sheriff patrols during construction as a means to reduce the general public from using the temporary access route.
- Mr. Weeber invited comments from others in attendance
 - Fire Chief Sumpter inquired why this intersection? He observes troubled intersections elsewhere throughout the township and county.
 - Chris explained that Elkhart County identified a need at this intersection and applied for funds to correct that need. He explained that funding FHWA funds through INDOT are sought for specific projects rather than general transportation funds being given to Elkhart County.

- *Elkhart County Highway identified needs for improved operational efficiency and improved safety at this intersection.*
 - How do the ongoing maintenance costs compare to that of the current intersection?
 - *Additional street lights (LED) = Approx. \$1,350 total energy cost per year*
 - *Additional landscape will be added with low maintenance plants and landscape stone. Maintenance will be performed by County Highway Department.*
 - What's the longest straight truck that can fit through the roundabout?
 - A WB-62 semitruck can traverse the intersection, as well as a 41' tri-axle ladder fire engine.
 - Senator Doriot's comments were solicited:
 - He's been aware of the project for many years and understood its purpose is related to increasing peak hour efficiency.
 - He's concerned about the turning radii, as that has been a concern among other roundabout designs.
 - He requested the turning movement simulations.
 - His main question is whether the anticipated costs are justified by the anticipated improvements.
 - Mr. Doriot also requested a terrain model in CAD.
 - *Turning movement simulations and .dtm files were provided to Senator Doriot on 12/15/2020.*
 - Katie Niblock (Elkhart County Highway) inquired if Concord Township Fire would prefer temporary emergency vehicle storage on the east side of the Elkhart River instead of the temporary access route with locked gate. This was a strategy used in the 70's or 80's when the CR 18 bridge was being reconstructed.
 - The current emergency vehicle technology requires more infrastructure to maintain their response readiness. For this reason, the access route seems much more feasible.
 - Mr. Weeber asked if the lowest cost option was still a viable alternative.
 - The no-build alternative has been dismissed since it does not satisfy the project's purpose and need.

We believe these minutes reflect the intent of the events, discussions and decisions made during the meeting. Please notify this office in writing within seven (7) days if participants find any errors, omissions, or discrepancies.

Respectfully submitted,



Christopher Waidner, P.E.
Project Manager

Author: CJC

Creator: CJC/CLW

550 Union Street, Mishawaka, IN 46544
Phone: 574-259-9976 Fax: 574-259-9982

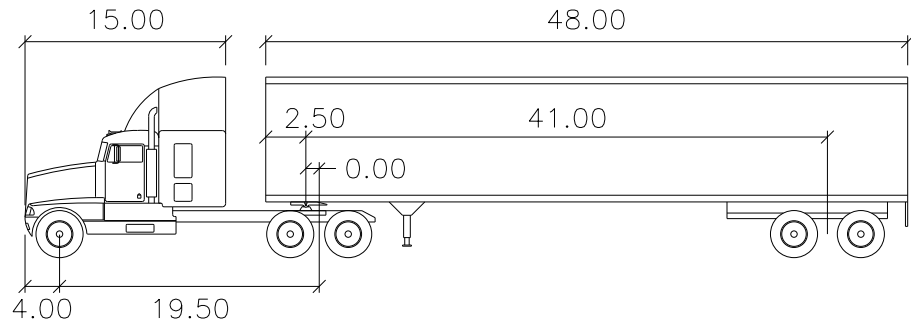
Sign In Sheet

Project: CR 18 AT CR 13/CR 115 Intersection Improvement, DES 1401749
Mtg. December 10, 2020 @ 1 pm (EST)
Date:
Location: CR 18 AT CR 13/CR 115

Sign in Sheet:

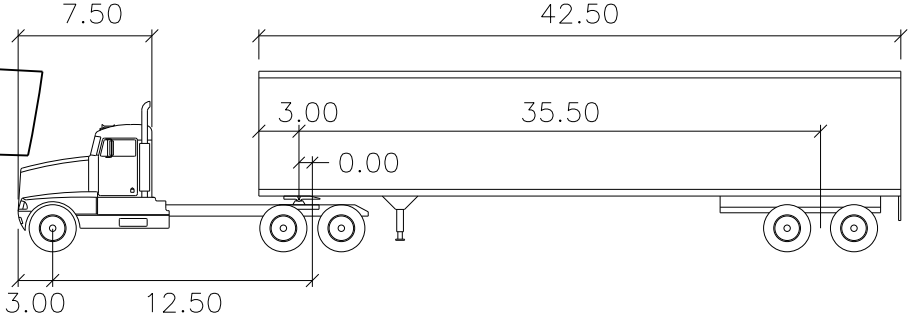
Name	Organization	E-Mail
TOM STUMP	COUNTY COUNCIL	CRIBES 4920@FRONTIER.COM
C. J. CUNNINGHAM	TROYER GROUP	cjc@troyergroup.com
CHRIS WHIDNER	"	clw@troyergroup.com
DAVID TEGGELAAR	"	dteggelaar@troyergroup.com
Tim Jackson	Elkhart County Hwy	tjackson@elkhartcounty.org
Lynn Loucks	Concord Bus Driver	toppissy@aol.com
Kimberly Martin	Concord Bus Driver	Kimmie.martin43@gmail.com
Jim Weber	Concord Trustee	concordtrustee@gmail.com
Phil Sumpter	Concord Fire Chief	psumpter@concordtownshipfire.com
Ken Day	Deputy Chief Concord	KDAVIS@CONCORDTOWNSHIPFIRE.COM
Jerry Adkins	Concord TWP Advisory Board	jadkins452@gmail.com
Joe Frey	Deputy Chief Concord	justmejoe182@gmail.com
James Kuhlonschmidt	Taxpayer	jakuhl@comcast.net
Blake Doriot	Indiana Scout	doriot_survey@gmail.com
Adam Bojalski	County Council	ABojalski@ElkhartCounty.com
Katie Niblock	Elkhart County Hwy	kniblock@elkhartcounty.org

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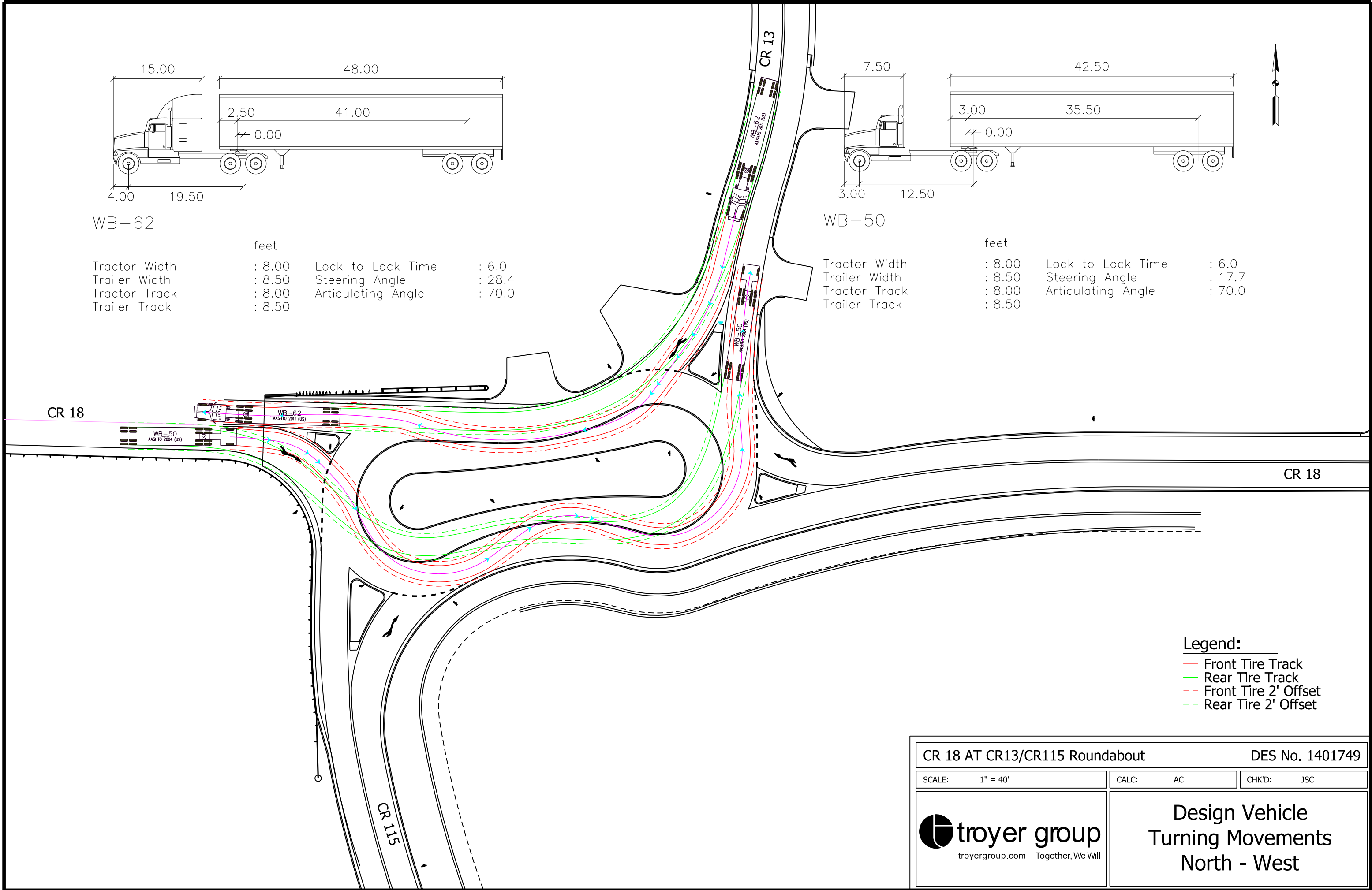
WB-62

	feet		
Tractor Width	: 8.00	Lock to Lock Time	: 6.0
Trailer Width	: 8.50	Steering Angle	: 28.4
Tractor Track	: 8.00	Articulating Angle	: 70.0
Trailer Track	: 8.50		



WB-50

	feet		
Tractor Width	: 8.00	Lock to Lock Time	: 6.0
Trailer Width	: 8.50	Steering Angle	: 17.7
Tractor Track	: 8.00	Articulating Angle	: 70.0
Trailer Track	: 8.50		



- Legend:
- Front Tire Track
 - Rear Tire Track
 - Front Tire 2' Offset
 - Rear Tire 2' Offset

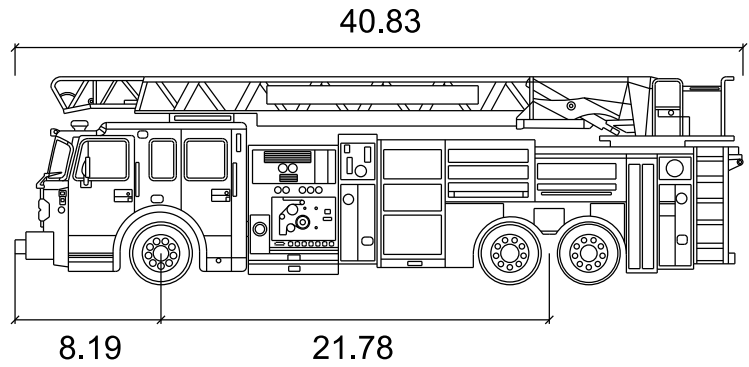
CR 18 AT CR13/CR115 Roundabout DES No. 1401749

SCALE: 1" = 40' CALC: AC CHK'D: JSC



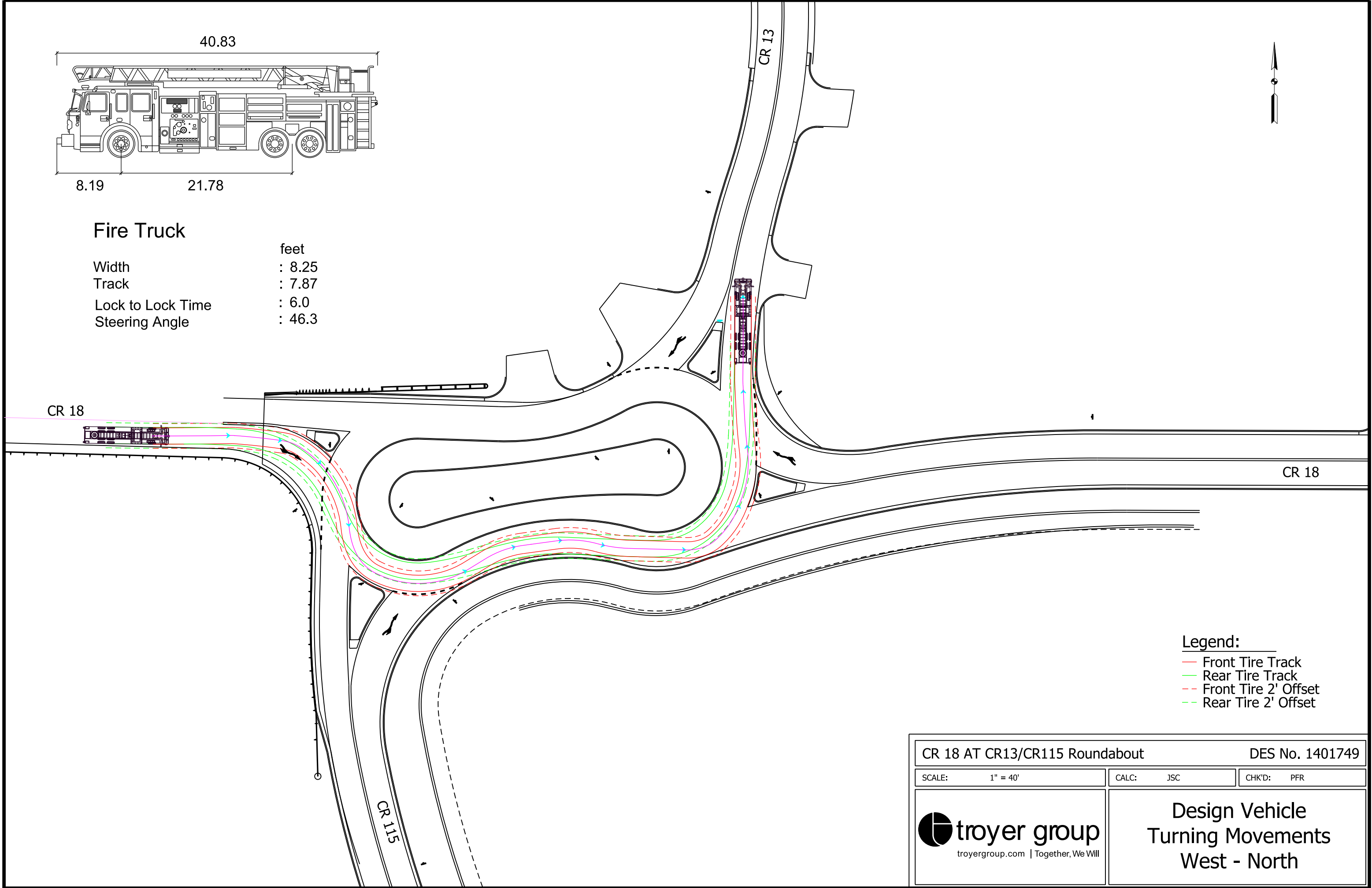
Design Vehicle
Turning Movements
North - West

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Fire Truck

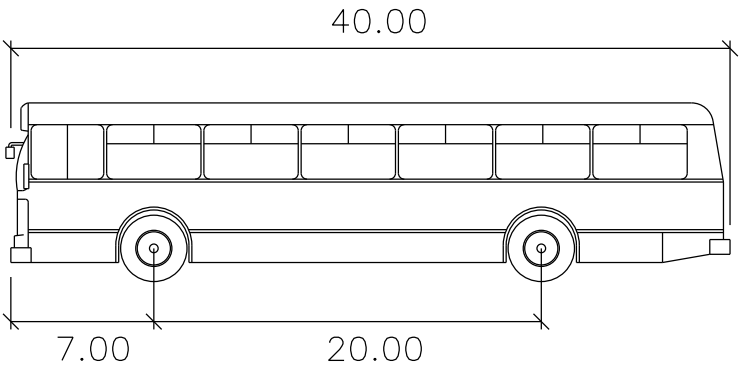
	feet
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Lock to Lock Time	: 6.0
Steering Angle	: 46.3



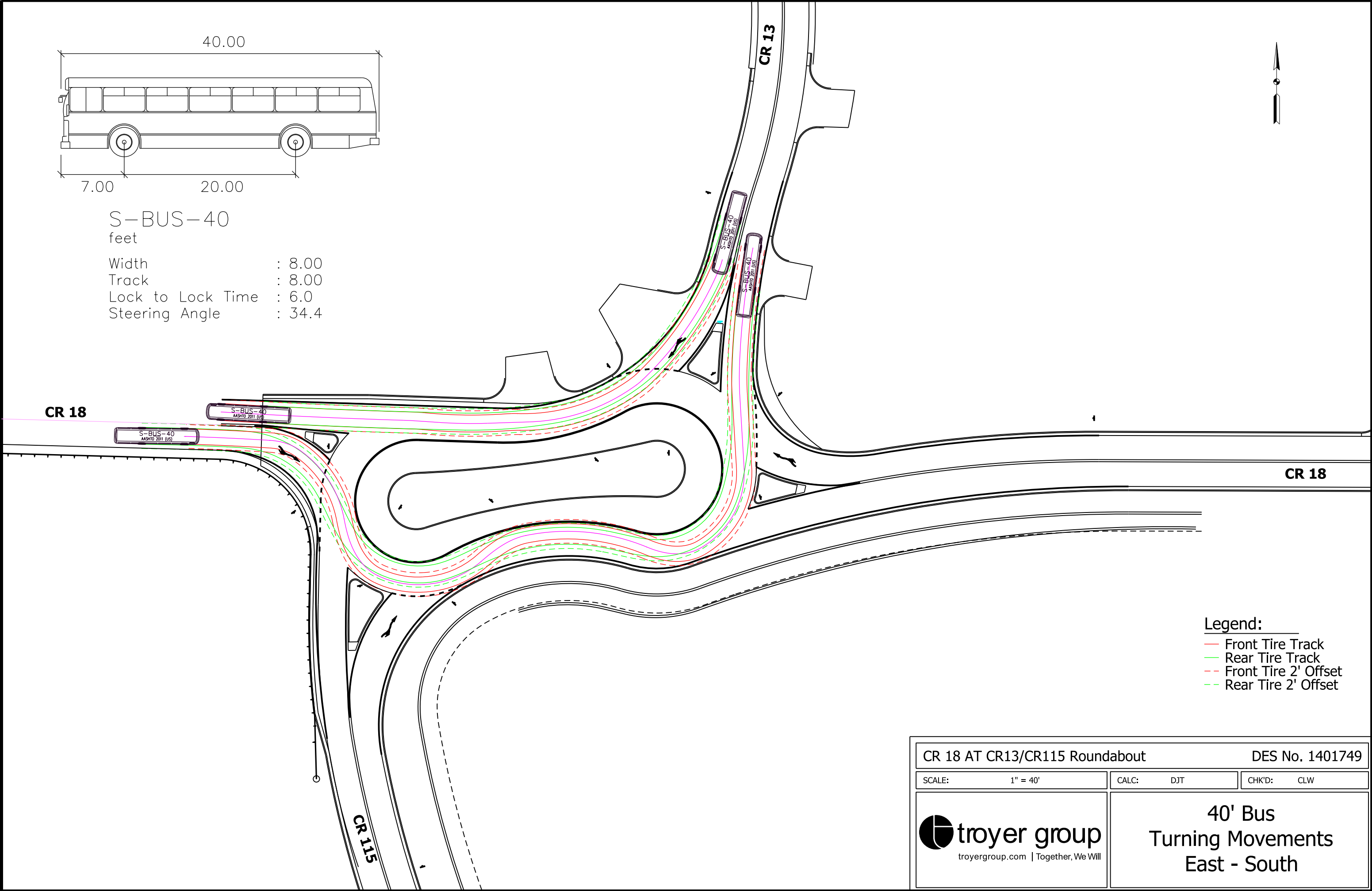
- Legend:
- Front Tire Track
 - Rear Tire Track
 - Front Tire 2' Offset
 - Rear Tire 2' Offset

CR 18 AT CR13/CR115 Roundabout		DES No. 1401749	
SCALE:	1" = 40'	CALC:	JSC
		CHK'D:	PFR
 troyergroup.com Together, We Will		Design Vehicle Turning Movements West - North	


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S-BUS-40
feet
Width : 8.00
Track : 8.00
Lock to Lock Time : 6.0
Steering Angle : 34.4



- Legend:
- Front Tire Track
 - Rear Tire Track
 - Front Tire 2' Offset
 - Rear Tire 2' Offset

CR 18 AT CR13/CR115 Roundabout		DES No. 1401749	
SCALE:	1" = 40'	CALC:	DJT
		CHK'D:	CLW
 troyergroup.com Together, We Will		40' Bus Turning Movements East - South	

CJ Cunningham

From: CJ Cunningham
Sent: Monday, January 4, 2021 8:04 AM
To: Katie Niblock; Christopher Waidner; Timothy Jackson; concordtrustee@gmail.com; Phil Sumpter; David Teggelaar; Cripes4920@frontier.com; toppissy@aol.com; kimmremartin43@gmail.com; kdavis@concordtownshipfire.com; jenkins452@gmail.com; justmejoey82@gmail.com; jakuhl@comcast.net; doriotsurvey@gmail.com; abujalski@elkhartcounty.com
Cc: Charles McKenzie; Yarian, Matthew; ccommissioners@elkhartcounty.com
Subject: RE: CR 18 at CR 13/CR 115 Intersection Improvement Project (Des. No. 1401749), Elkhart County, Indiana
Attachments: 2020-12-10 Mtg Minutes FINAL - Des. 1401749 - Concord Township.pdf

Good Morning,

Thank you to those who participated in December 10th meeting to discuss the proposed improvements to the intersection of CR 18 at CR 13/115 in Elkhart County. The final meeting minutes are attached, and these will be added to the project record.

Troyer Group is continuing coordination with the Elkhart County Highway Department (project sponsor) and INDOT (funding administrator) regarding the project's next steps.

Regards,

C.J. Cunningham | Manager – Environmental Services
cjc@troyergroup.com | c [574.485.6270](tel:574.485.6270) | w [574.259.9976](tel:574.259.9976) ext.5006
3930 Edison Lakes Pkwy, Mishawaka, IN 46545 | troyergroup.com



From: CJ Cunningham
Sent: Monday, December 21, 2020 3:03 PM
To: Katie Niblock <kniblock@elkcohw.org>; Christopher Waidner <clw@troyergroup.com>; Timothy Jackson <tjackson@elkcohw.org>; concordtrustee@gmail.com; Phil Sumpter <psumpster@concordtownshipfire.com>; David Teggelaar <dteggelaar@troyergroup.com>; Cripes4920@frontier.com; toppissy@aol.com; kimmremartin43@gmail.com; kdavis@concordtownshipfire.com; jenkins452@gmail.com; justmejoey82@gmail.com; jakuhl@comcast.net; doriotsurvey@gmail.com; abujalski@elkhartcounty.com
Cc: Charles McKenzie <cmckenzie@elkcohw.org>; Yarian, Matthew <MYarian@indot.IN.gov>; ccommissioners@elkhartcounty.com
Subject: CR 18 at CR 13/CR 115 Intersection Improvement Project (Des. No. 1401749), Elkhart County, Indiana

Good afternoon,

Thank you to those who participated in December 10th meeting to discuss the proposed improvements to the intersection of CR 18 at CR 13/115 in Elkhart County. We have prepared the attached meeting minutes and are circulating them for review among the meeting participants. Please notify Troyer Group in writing within seven (7) days if participants find any errors, omissions, or discrepancies among these meeting minutes and the discussion that occurred during the meeting. Once finalized, these minutes will be issued to each of the participants and entered into the official project record.

Troyer Group is coordinating the project's next steps with Elkhart County Highway Department (project sponsor) and INDOT (funding administrator).

Regards,

C.J. Cunningham | Manager – Environmental Services

cjc@troyergroup.com | w [574.259.9976](tel:574.259.9976) ext.5006

3930 Edison Lakes Pkwy, Mishawaka, IN 46545 | troyergroup.com



APPENDIX H

Air Quality & TIP / STIP Incorporation

Indiana Department of Transportation (INDOT)

State Preservation and Local Initiated Projects FY 2016 - 2019

SPONSOR	CONTRACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2016	2017	2018	2019
	38157 / 1401747	M 24	ST 1026	Road Reconstruction (3R/4R Standards)	Kercher Road Reconstruction from Dierdorff to US 33.	Fort Wayne	0	STP		100% Local Funds	PE	\$0.00	\$0.00	\$78,400.00	(\$78,400.00)		
Comments: MACOG Resolution 21-16. Moving FY17 PE of \$392K to FY 16.																	
	38157 / 1401747	A 32	ST 1026	Road Reconstruction (3R/4R Standards)	Kercher Road Reconstruction from Dierdorff to US 33.	Fort Wayne	0	STP		100% Local Funds	RW	\$0.00	\$0.00		\$50,000.00		(\$50,000.00)
										Elkhart-Goshen MPO	RW	\$0.00	\$0.00		\$200,000.00		(\$200,000.00)
Comments: MACOG Resolution M1111-16. Moving RW from FY 2019 to FY 2017 into 2016-2019 STIP.																	
	38158 / 1401749	M 08	IR 1030	Intersect. Improv. W/ New Signals	CR 18 at CR 13 and CR 115 Intersection	Fort Wayne	2	STP		100% Local Funds	PE	\$0.00	\$50,000.00	\$50,000.00			
										Elkhart-Goshen MPO	PE	\$200,000.00	\$0.00	\$200,000.00			
	38197 / 1382015	Init.	US 33	Relinquishments/Road Transfer	Transfer of US 33 (Monroe to Pike) & SR 4 to ECL then reroute SR 15 to S. 3rd	Fort Wayne	1.153	NHPP		Statewide Construction	CN	\$320,000.00	\$80,000.00		\$400,000.00		
	38326 / 1500347	A 09	SR 19	HMA Overlay, Preventive Maintenance	From 0.17 Miles North of SR 119 to 0.32 Miles North of US 20.	Fort Wayne	8.787	STP		Road Construction	CN	\$2,533,200.00	\$633,300.00	\$3,166,500.00			
Comments: IN MACOG TIP via AMENDMENT / RESOLUTION 41-15. Add CN phase to FY 2016 STIP in 2016-2019 STIP																	
	38558 / 1383607	A 07	SR 13	Small Structure Pipe Lining	Branch of Rock Run Creek, 2.11 Miles South of SR 4	Fort Wayne	0	STP	\$52,500.00	Bridge Consulting	PE	\$8,400.00	\$2,100.00		\$10,500.00		
Comments: Add new project to STIP: MACOG Resolution 36-15, adding PE for 2017.																	
	38559 / 1383522	A 07	SR 19	Small Structure Pipe Lining	Over Branch Bauge Creek, 0.22 Miles North of US 20 Bypass	Fort Wayne	0	STP	\$152,000.00	Bridge Consulting	PE	\$18,400.00	\$4,600.00		\$23,000.00		
Comments: MACOG resolution 36-15: Add new project to TIP for PE in 2017.																	
	38606 / 1383237	A 04	US 33	Other Intersection Improvement	9.42 Miles North of SR 13 at CR 36/College Ave.	Fort Wayne	.242	NHPP	\$1,289,400.00	Safety ROW	RW	\$200,000.00	\$50,000.00			\$250,000.00	
										Safety Consulting	PE	\$192,000.00	\$48,000.00	\$240,000.00			
										Safety Construction	CN	\$400,000.00	\$100,000.00				\$500,000.00
Comments: Add new Project to STIP per MACOG Resolution 31-15: PE in 2016, RW in 2018 CN in 2019.																	
	38607 / 1383237	M 26	US 33	Other Intersection Improvement	9.42 miles N of SR 13 at CR 36/College Ave	Fort Wayne	.242	NHPP		Safety Consulting	PE	\$16,000.00	\$4,000.00	(\$217,348.00)	\$237,348.00		
Comments: MACOG modification M0106-16. Moving Partial FY16 PE to FY17 and increasing FY17 PE by \$20K in 2016-2019 STIP																	
	38607 / 1383238	A 04	US 6	Intersection Improvement, Roundabout	US 6 at SR 13 W. Jct/CR 33	Fort Wayne	.379	STP	\$2,813,500.00	Safety ROW	RW	\$240,000.00	\$60,000.00			\$300,000.00	
										Safety Consulting	PE	\$400,000.00	\$100,000.00	\$500,000.00			

Indiana Department of Transportation (INDOT)

State Preservation and Local Initiated Projects FY 2018 - 2021

SPONSOR	CONTRACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2018	2019	2020	2021
Comments:MACOG Resolution 28-17. Adding PE to FY 2018 and CN to FY 2019 into FY 2018 - 2021 STIP.																	
Indiana Department of Transportation	37707 / 1701297	A 04	SR 119	Bridge Deck Patching	Bridge over Elkhart River, 0.36 miles S SR 15	Fort Wayne	0	STP	\$80,000.00	Bridge Construction	CN	\$43,651.20	\$10,912.80		\$54,564.00		
										Bridge Consulting	PE	\$20,000.00	\$5,000.00	\$25,000.00			
Comments:MACOG Resolution 28-17. Adding PE to FY 2018 and CN to FY 2019 into FY 2018 - 2021 STIP.																	
Elkhart	37803 / 1400712	Init.	ST 1001	Bike/Pedestrian Facilities	Cassopolis Sidewalk from Bristol St to Windsor Ave	Fort Wayne	1.9	STP		100% Local Funds	CN	\$0.00	\$400,000.00	\$400,000.00			
										Elkhart-Goshen MPO - PYB	CN	\$1,600,000.00	\$0.00	\$1,600,000.00			
Elkhart	37803 / 1400712	M 05	ST 1001	Bike/Pedestrian Facilities	Cassopolis Sidewalk from Bristol St to Windsor Ave	Fort Wayne	1.9	CMAQ	\$2,000,000.00	Elkhart-Goshen MPO	CN	\$0.00	\$0.00	(\$1,600,000.00)	\$1,600,000.00		
										100% Local Funds	CN	\$0.00	\$0.00	(\$400,000.00)	\$400,000.00		
Comments:Move CN from 2018 to FY 2019 per MACOG 2018-2021 TIP.																	
Elkhart County	37816 / 1400841	Init.	IR 1038	Bridge Rehabilitation Or Repair	Brg No 102 on CR 35 over the Little Elkhart River	Fort Wayne	.076	STP		100% Local Funds	CN	\$0.00	\$153,349.00	\$153,349.00			
										Local Bridge Program	CN	\$613,396.00	\$0.00	\$613,396.00			
Goshen	38157 / 1401747	A 14	ST 1026	Road Reconstruction (3R/4R Standards)	Kercher Road Reconstruction from Dierdorff to US 33	Fort Wayne	0	STP	\$4,403,125.00	100% Local Funds	CN	\$0.00	\$1,080,625.00		\$1,080,625.00		
										Elkhart-Goshen MPO - PYB	CN	\$2,798,945.70	\$0.00		\$2,798,945.70		
										Elkhart-Goshen MPO	CN	\$1,523,554.30	\$0.00		\$1,523,554.30		
Comments:Per MACOG 2018-2021 TIP																	
Elkhart County	38158 / 1401749	M 16	IR 1030	Intersect. Improv. W/ New Signals	CR 18 at CR 13 and CR 115 Intersection	Fort Wayne	.2	STPBG	\$3,426,000.00	Elkhart-Goshen MPO	RW	\$0.00	\$0.00	(\$400,000.00)	\$400,000.00		
										100% Local Funds	RW	\$0.00	\$0.00	(\$100,000.00)	\$100,000.00		
Comments:Per MACOG Resolution 01-18. Move ROW from '18 to '19.																	
Elkhart County	38158 / 1401749	Init.	IR 1030	Intersect. Improv. W/ New Signals	CR 18 at CR 13 and CR 115 Intersection	Fort Wayne	.2	STP		100% Local Funds	CN	\$0.00	\$2,925,000.00			\$2,925,000.00	
										100% Local Funds	RW	\$0.00	\$100,000.00	\$100,000.00			
										Elkhart-Goshen MPO	RW	\$400,000.00	\$0.00	\$400,000.00			
MPO - South Bend / Elkhart	38515 / 1500575	Init.	MS TRST	Transit Operating	Operating Assistance - Elkhart/ Goshen	Fort Wayne	0	Transit		Transit	PE	\$1,344,526.00	\$0.00		\$1,344,526.00		

Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2020 - 2024

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2020	2021	2022	2023	2024
MPO - South Bend / Elkhart	1700674	Init.	MS TRST	Transit Purchase Vehicles	Vehicle Replacement Buses (1)	Fort Wayne	0	Transit		Transit	PE	\$395,000.00	\$0.00		\$395,000.00			
MPO - South Bend / Elkhart	1700675	Init.	MS TRST	Transit Purchase Vehicles	Vehicle Replacement Paratransit (3)	Fort Wayne	0	Transit		Local Funds	PE	\$0.00	\$37,500.00		\$37,500.00			
										Transit	PE	\$150,000.00	\$0.00		\$150,000.00			
Indiana Department of Transportation	1800090	Init.	US 20	Added Travel Lanes	From SR 15 to 4.14 miles East of SR 15 (CR 35).	Fort Wayne	4.113	STPBG		Mobility Construction	CN	\$13,788,557.60	\$3,447,139.40				\$17,235,697.00	
Goshen	1801265	Init.	ST 1001	Railroad Protection	Beaver Lane at NS RR DOT # 510019A in Goshen	Fort Wayne	0	STPBG		Local Funds	PE	\$0.00	\$2,000.00	\$2,000.00				
										Local Funds	CN	\$0.00	\$36,000.00	\$36,000.00				
										Local Safety Program - 130	PE	\$18,000.00	\$0.00	\$18,000.00				
										Local Safety Program - 130	CN	\$324,000.00	\$0.00	\$324,000.00				
Indiana Department of Transportation	1802804	Init.	SR 15	Other Intersection Improvement	SR 15 and SR 120 intersection, East JCT	Fort Wayne	.13	NHPP		District Other Construction	CN	\$40,000.00	\$10,000.00	\$50,000.00				
Goshen	1900391	Init.	ST 1049	Railroad Protection	Madison St. at NS RR DOT 510039L in Goshen	Fort Wayne	0	STPBG		Local Funds	PE	\$0.00	\$2,000.00	\$2,000.00				
										Local Funds	CN	\$0.00	\$38,500.00	\$38,500.00				
										Local Safety Program - 130	PE	\$18,000.00	\$0.00	\$18,000.00				
										Local Safety Program - 130	CN	\$346,500.00	\$0.00	\$346,500.00				
Goshen	37647 / 1400715	Init.	ST 1039	Road Reconstruction (3R/4R Standards)	Wilden Ave: Rock Run Creek to 6th St	Fort Wayne	.7	STPBG		Local Funds	CN	\$0.00	\$1,085,980.00		\$1,085,980.00			
										Elkhart-Goshen MPO	CN	\$4,343,920.00	\$0.00		\$4,343,920.00			
Elkhart County	38158 / 1401749	Init.	IR 1030	Other Intersection Improvement	CR 18 at CR 13 and CR 115 Intersection	Fort Wayne	2	STPBG		Local Funds	CN	\$0.00	\$585,000.00	\$585,000.00				
										Elkhart-Goshen MPO	CN	\$2,340,000.00	\$0.00	\$2,340,000.00				
Indiana Department of Transportation	38607 / 1383238	Init.	US 6	Intersection Improvement, Roundabout	US 6 at SR 13 W Jct./CR 33	Fort Wayne	.379	STPBG		Safety Construction	CN	\$2,111,034.40	\$527,758.60	\$2,638,793.00				
										Statewide Construction	CN	\$651,379.20	\$162,844.80	\$814,224.00				

*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

TRANSPORTATION IMPROVEMENT PROGRAM

Locally Sponsored Projects

DES	Location	Work Type	Fund Type	Phase	Federal	Match	Total	Estimated to Complete	Fiscal Year
City of Elkhart									
1400712	Cassopolis Multi-use Trail Ph I & II from Windsor Ave to Bristol St	Bike/ Pedestrian Facilities	Grp II STP	CN	\$1,600,000	\$400,000	\$2,000,000		2019
Elkhart County									
1401749	CR 18 at CR 13 and CR 115 Intersection	Intersection Improvement	CMAQ	RW	\$400,000	\$100,000	\$500,000		2018
1401749	CR 18 at CR 13 and CR 115 Intersection	Intersection Improvement	CMAQ	CN	\$2,340,000	\$585,000	\$2,925,000		2020
1400841	Bridge 102 on CR 35 over the Little Elkhart River 0.1 miles N of CR8	Bridge Rehabilitation or Repair	Bridge	CN	\$660,356	\$165,089	\$825,445		2018
1592887	County-wide Bridge Inspection and Inventory Program for Cycle Years 2018-2021	Bridge Inspections	Bridge	PE	\$311,405	\$77,851	\$389,256		2019 2020 2021
1700309	Bridge 152 on Mishawaka Road over Yellow Creek	Bridge Replacement, Concrete	Grp II STP	PE	\$192,000	\$48,000	\$240,000		2018
1700309	Bridge 152 on Mishawaka Road over Yellow Creek	Bridge Replacement, Concrete	Grp II STP	RW	\$80,000	\$20,000	\$100,000	\$2,360,000	2019

TRANSPORTATION IMPROVEMENT PROGRAM

Locally Sponsored Projects

DES	Location	Work Type	Fund Type	Phase	Federal	Match	Total	Estimated to Complete	Fiscal Year
City of Elkhart									
1801611	Bristol St: from Jeanwood Dr. to CR 15	Added Travel Lanes	STBG	RW	\$480,000	\$120,000	\$600,000	\$7,400,000	2021
1801611	Bristol St: from Jeanwood Dr. to CR 15	Added Travel Lanes	STBG	CN	\$4,800,000	\$1,200,000	\$6,000,000	\$7,400,000	2024
1801933	Hively Avenue, east of Main St, crossing the NS Railroad	New Bridge	ST STBG	PE	\$1,252,992	\$313,248	\$1,566,240	\$13,830,240	2022
1801933	Hively Avenue, east of Main St, crossing the NS Railroad	New Bridge	ST STBG	RW	\$984,000	\$246,000	\$1,230,000	\$13,830,170	2021
1801933	Hively Avenue, east of Main St, crossing the NS Railroad	New Bridge	ST STBG	CN	\$8,827,200	\$2,206,800	\$11,034,000	\$13,830,170	2022
Elkhart County									
1401749	CR 18 at CR 13 and CR 115 Intersection	Intersection Improvement	CMAQ	RW	\$400,000	\$100,000	\$500,000	\$3,750,164	2020
1401749	CR 18 at CR 13 and CR 115 Intersection	Intersection Improvement	CMAQ	CN	\$2,340,000	\$585,000	\$2,925,000	\$3,750,164	2021
1592887	Countywide Bridge Inspections for 2018-2021	Bridge Inspections	Bridge	PE	\$175,922	\$43,980	\$219,902	\$219,902	2020 2021 2022
1700309	Bridge 152 on Mishawaka Rd over Yellow Creek	Bridge Repl, Comp. Cont. Conc. Construction	STBG	RW	\$80,000	\$20,000	\$100,000	\$3,300,135	2020
1700309	Bridge 152 on Mishawaka Rd over Yellow Creek	Bridge Repl, Comp. Cont. Conc. Construction	STBG	CN	\$1,888,000	\$472,000	\$2,360,000	\$3,300,135	2023
1700310	CR 17 Multi-Use Path: From US 33 to CR 45	Bike/Pedestrian Facilities	CMAQ	RW	\$72,000	\$18,000	\$90,000	\$3,771,555	2020

APPENDIX I

Other

Bridge Inspection Report

20-00156
CR 18
over
ELKHART RIVER



Inspection Date: 09/12/2018

Inspected By: Stuart May

Inspection Type(s): Routine

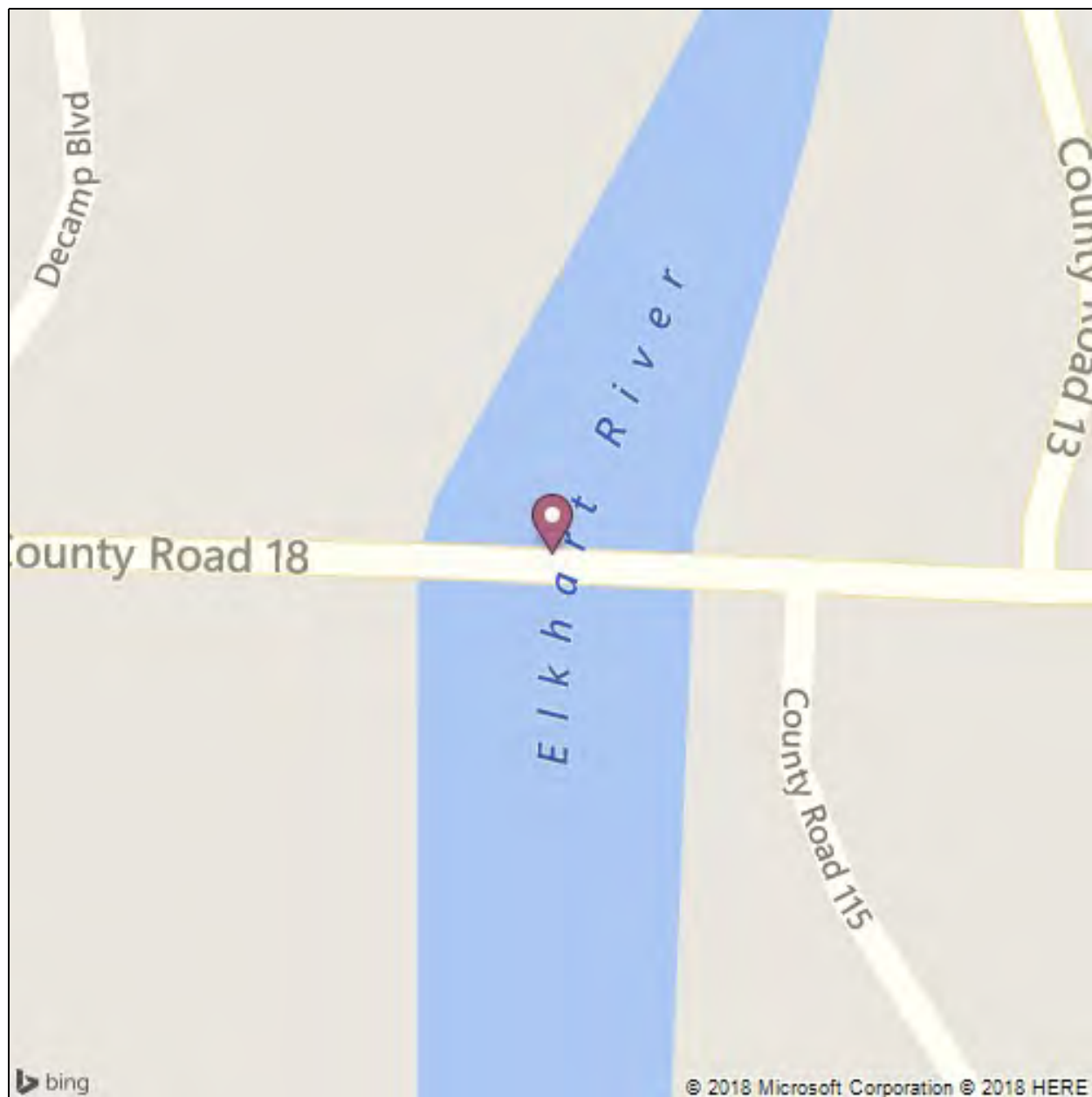
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Inspector: Stuart May
Inspection Date: 09/12/2018

Asset Name: 20-00156
Facility Carried: CR 18

Bridge Inspection Report



Latitude: 41.65954

Longitude: -85.916336

Inspector: Stuart May

Asset Name: 20-00156

Inspection Date: 09/12/2018

Facility Carried: CR 18

Bridge Inspection Report

The bridge was built in 1970 and reconstructed in 1983.

Wearing Surface - SMALL HOLES IN EAST LANE, CRACKS.

Deck - COPINGS REPAIRED.

Superstructure - DIAPHRAGM SPALLS, UTILITIES ALONG NORTH AND SOUTH SIDES OF STRUCTURE.

Substructure - SPALLING AND CRACKING AT ABUTMENTS.

Channel - BOTH SLOPEWALLS UNDERMINED / CRACKED.

The bridge is not scour critical.

Overall the bridge is in good condition.

Inspector: Stuart May
 Inspection Date: 09/12/2018

Asset Name: 20-00156
 Facility Carried: CR 18

Bridge Inspection Report

IDENTIFICATION

(1) STATE CODE:	185 - Indiana	(12) BASE HIGHWAY NETWORK:	0
(8) STRUCTURE:	2000033	(13A) INVENTORY ROUTE:	
(5 A-B-C-D-E) INV. ROUTE:	1 - 5 - 1 - 00066 - 0	(13B) SUBROUTE NUMBER:	
(2) HIGHWAY AGENCY DISTRICT:	02 - Fort Wayne	(16) LATITUDE:	41.65954
(3) COUNTY CODE:	020 - ELKHART	(17) LONGITUDE:	-85.916336
(4) PLACE CODE:	20728 - ELKHART	(98) BORDER	
(6) FEATURES INTERSECTED:	ELKHART RIVER	A) STATE NAME:	
(7) FACILITY CARRIED:	CR 18	B) PERCENT	%
(9) LOCATION:	00.00 W OF CR 13	(99) BORDER BRIDGE STRUCT. NO:	
(11) MILEPOINT:	0000.000		

STRUCTURE TYPE AND MATERIAL

(43) STRUCTURE TYPE, MAIN:		(45) NUMBER OF SPANS IN MAIN	003
A) KIND OF MATERIAL/DESIGN:	6 - Prestressed concrete continuous	UNIT:	
B) TYPE OF DESIGN/CONSTR:	02 - Stringer/Multi-beam or Girder	(46) NUMBER OF APPROACH SPANS:	0000
(44) STRUCTURE TYPE, APPROACH SPANS:		(107) DECK STRUCTURE TYPE:	1 - Concrete Cast-in-Place
A) KIND OF MATERIAL/DESIGN:	0 - Other	(108) WEARING SURFACE/PROT SYS:	
B) TYPE OF DESIGN/CONSTR:	00 - Other	A) WEARING SURFACE:	3 - Latex Concrete or similar additive
		B) DECK MEMBRANE:	0 - None
		C) DECK PROTECTION:	0 - None

AGE OF SERVICE

(27) YEAR BUILT:	1970	(28) LANES:	
(106) YEAR RECONSTRUCTED:	1983	A) ON BRIDGE:	03
(42) TYPE OF SERVICE:		B) UNDER BRIDGE:	00
A) ON BRIDGE:	1 - Highway	(29) AVERAGE DAILY TRAFFIC:	015080
B) UNDER BRIDGE:	5 - Waterway	(30) YEAR OF AVERAGE DAILY TRAFFIC:	2011
		(109) AVERAGE DAILY TRUCK TRAFFIC:	06 %
		(19) BYPASS DETOUR LENGTH:	006 MI

Inspector: Stuart May
Inspection Date: 09/12/2018

Asset Name: 20-00156
Facility Carried: CR 18

Bridge Inspection Report

GEOMETRIC DATA

(48) LENGTH OF MAX SPAN:	0072.8	FT	(35) STRUCTURE FLARED:	0 - No flare
(49) STRUCTURE LENGTH:	00220.2	FT	(10) INV RTE, MIN VERT CLEARANCE:	99.99 FT
(50) CURB/SIDEWALK WIDTHS:			(47) TOT HORIZ CLEARANCE:	039.6 FT
A) LEFT	00.0	FT	(53) VERT CLEAR OVER BR RDWY:	99.99 FT
B) RIGHT:	00.0	FT	(54) MIN VERTICAL UNDERCLEARANCE:	
(51) BRDG RDWY WIDTH CURB-TO-CURB:	039.6	FT	A) REFERENCE FEATURE:	N
(52) DECK WIDTH, OUT-TO-OUT:	043.0	FT	B) MIN VERT UNDERCLEAR:	0 FT
(32) APPROACH ROADWAY	036.0	FT	(55) LATERAL UNDERCLEARANCE RIGHT:	
(33) BRIDGE MEDIAN:	0 - No median		A) REFERENCE FEATURE:	N
(34) SKEW:	00	DEG	B) MIN LATERAL UNDERCLEAR:	000.0 FT
			(56) MIN LATERAL UNDERCLEAR ON LEFT:	000.0 FT

INSPECTIONS

(90) INSPECTION DATE:	09/12/2018	(91) DESIGNATED INSPECTION FREQUENCY:	24 MONTHS
(92) CRITICAL FEATURE INSPECTION:		(93) CRITICAL FEATURE INSPECTION DATE:	
A) FRACTURE CRITICAL REQUIRED/FREQUENCY:	N	A) FRACTURE CRITICAL DATE:	
B) UNDERWATER INSPECTION REQUIRED/FREQUENCY:	N	B) UNDERWATER INSP DATE:	08/01/1998
C) OTHER SPECIAL INSPECTION REQUIRED/FREQUENCY:	N	C) OTHER SPECIAL INSP DATE:	

CONDITION

(58) DECK:	7 - Good Condition (some minor problems)	(60) SUBSTRUCTURE:	7 - Good Condition (some minor problems)
(58.01) WEARING SURFACE:	7 - Good Condition	(61) CHANNEL/CHANNEL PROTECTION:	7 - Bank protection needs minor repairs
(59) SUPERSTRUCTURE:	7 - Good Condition (some minor problems)	(62) CULVERTS:	N - Not Applicable

CONDITION COMMENTS

(58) DECK: 7 - Good Condition (some minor problems)

Comments:
GOOD - COPINGS REPAIRED
Material:
CONCRETE

(58.01) WEARING SURFACE: 7 - Good Condition

Comments:
GOOD - SMALL HOLES IN EAST LANE, CRACKS
Material:
CONCRETE

Inspector: Stuart May
Inspection Date: 09/12/2018

Asset Name: 20-00156
Facility Carried: CR 18

Bridge Inspection Report

(59) SUPERSTRUCTURE: 7 - Good Condition (some minor problems)

Comments:

GOOD - DIAPHRAGM SPALLS, UTILITIES ALONG NORTH AND SOUTH SIDES OF STRUCTURE

Material:

PRESTR. CONC. I-BEAM

(60) SUBSTRUCTURE: 7 - Good Condition (some minor problems)

Comments:

GOOD - SPALLING AND CRACKING AT ABUTMENTS

Material:

CONC. PIERS

(61) CHANNEL/CHANNEL PROTECTION 7 - Bank protection needs minor repairs

Comments:

GOOD - BOTH SLOPEWALLS UNDERMINED / CRACKED

Material:

NATURAL/CONC. SLOPEWALL

(62) CULVERTS: N - Not Applicable

Comments:

N/A

Material:

N/A

LOAD RATING AND POSTING

(31) DESIGN LOAD:	5 - HS 20	(66) INVENTORY RATING:	36
(70) BRIDGE POSTING	5 - Equal to or above legal loads	(65) INVENTORY RATING METHOD:	0 - Field evaluation and documented engineering judgment
(41) STRUCTURE OPEN/POSTED/CLOSED:	A - Open	(66B) INVENTORY RATING (H):	20
(64) OPERATING RATING:	45	(66C) TONS POSTED :	
(63) OPERATING RATING METHOD:	0 - Field evaluation and documented engineering judgment	(66D) DATE POSTED/CLOSED:	

APPRAISAL

SUFFICIENCY RATING:	71.2	(36) TRAFFIC SAFETY FEATURE:	
STATUS:	0	36A) BRIDGE RAILINGS:	0
(67) STRUCTURAL EVALUATION:	7	36B) TRANSITIONS:	0
(68) DECK GEOMETRY:	4	36C) APPROACH GUARDRAIL:	0
(69) UNDERCLEARANCES, VERTICAL & HORIZONTAL:	N	36D) APPROACH GUARDRAIL ENDS:	0
(71) WATERWAY ADEQUACY:	8 - Bridge Above Approaches		
Comments:			
ADEQUATE			

Inspector: Stuart May
Inspection Date: 09/12/2018

Asset Name: 20-00156
Facility Carried: CR 18

Bridge Inspection Report

(72) APPROACH ROADWAY ALIGNMENT: **8 - Equal to present desirable criteria**

Comments:
CRACKING, SEALED
Material:
BITUMINOUS
72: INTER. AT EAST END

(113) SCOUR CRITICAL BRIDGES: **5 - Scour within limits of footing or piles**

Comments:

CLASSIFICATION

(20) TOLL:	3 - On Free Road	(21) MAINT. RESPONSIBILITY:	02 - County Highway Agency
(22) OWNER:	02 - County Highway Agency	(26) FUNCTIONAL CLASS OF INVENTORY RTE:	16 - Urban - Minor Arterial
(37) HISTORICAL SIGNIFICANCE:	5 - Not eligible	(100) STRAHNET HIGHWAY:	Not a STRAHNET route
(101) PARALLEL STRUCTURE:	N - No parallel structure	(102) DIRECTION OF TRAFFIC:	2-way traffic
(103) TEMPORARY STRUCTURE:		(104) HIGHWAY SYSTEM OF INVENTORY ROUTE:	0 - Structure/Route is NOT on NHS
(105) FEDERAL LANDS HIGHWAYS:	0-Not Applicable	(110) DESIGNATED NATIONAL NETWORK:	Inventory route not on network
(112) NBIS BRIDGE LENGTH:	Yes		

NAVIGATION DATA

(38) NAVIGATION CONTROL:	0 - No navigation control on waterway (bridge permit not required)	(39) NAVIGATION VERTICAL CLEAR:	000.0 FT
(111) PIER OR ABUTMENT PROTECTION:		(116) MINIMUM NAVIGATION VERT. CLEARANCE, VERT. LIFT BRIDGE:	FT
		(40) NAV HORIZONTAL CLEARANCE:	0000.0 FT

PROPOSED IMPROVEMENTS

(75A) TYPE OF WORK:	(95) ROADWAY IMPROVEMENT COST: \$	000000
(75B) WORK DONE BY:	(96) TOTAL PROJECT COST:	\$ 000000
(76) LENGTH OF IMPROVEMENT: 000000 FT	(97) YR OF IMPROVEMENT COST EST:	
(94) BRIDGE IMPROVEMENT COST:	(114) FUTURE AVG DAILY TRAFFIC:	025970
	(115) YR OF FUTURE ADT:	2031

Inspector: Stuart May

Inspection Date: 09/12/2018

Asset Name: 20-00156

Facility Carried: CR 18

Bridge Inspection Report

Inspector: Stuart May
Inspection Date: 09/12/2018

Asset Name: 20-00156
Facility Carried: CR 18

Bridge Inspection Report



PHOTO 1

Description LOOKING WEST FROM ROADWAY



PHOTO 2

Description LOOKING EAST FROM ROADWAY

Inspector: Stuart May
Inspection Date: 09/12/2018

Asset Name: 20-00156
Facility Carried: CR 18

Bridge Inspection Report



PHOTO 3

Description LOOKING SOUTH FROM CHANNEL



PHOTO 4

Description NORTH FROM CHANNEL

Inspector: Stuart May
Inspection Date: 09/12/2018

Asset Name: 20-00156
Facility Carried: CR 18

Bridge Inspection Report



PHOTO 5

Description UPSTREAM FROM STRUCTURE



PHOTO 6

Description DIAPHRAGM SPALLS AT PIER 3

Inspector: Stuart May
Inspection Date: 09/12/2018

Asset Name: 20-00156
Facility Carried: CR 18

Bridge Inspection Report



PHOTO 7

Description ABUTMENT AND UNDERSIDE STRUCTURE



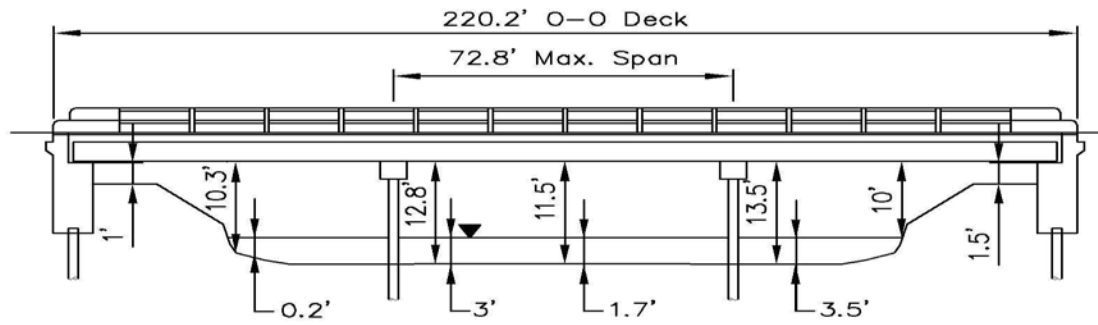
PHOTO 8

Description CRACK AND DELAMINATION IN BEAM 4 AT EAST END BENT

Inspector: Stuart May
Inspection Date: 09/12/2018

Asset Name: 20-00156
Facility Carried: CR 18

Bridge Inspection Report



SKETCH 1 Description

Miscellaneous Asset Data
Asset Management

2000033

Load Rating 2:

Has the dead load or the structural condition of the primary load carrying members changed since the last inspection?

Extended Frequency:

Submittal Date:

Inspector:

INDOT Reviewer:

This bridge has been accepted into the Extended Frequency Program.

Approval Date:

Joints: ** Indicate location, type, and rating of lowest rated joint.*

Comments:

Terminal Joints: **Rating of lowest rated terminal joint.*

Comments:

Concrete Slopewall: **Rating of lowest rated slopewall.*

Comments:

Bearings: ** Indicate type, and rating of lowest rated bearing.*

Comments:

Approach Slabs: ** Indicate if present & condition rating.*

Comments:

Paint: * Indicate if paint present , year painted & condition rating.

Not Rated

Comments:

Scour Analysis:

Scour Critical:

Scour POA?

NBI 113 Scour Comment:

Endangered Species: * If yes, add one photo to the dropdown field

Bats: seen or heard under structure? * N

Birds/swallows/nests seen? Empty nests present? * N

BRIDGE Culvert Geometry:

Barrel Length: 000.0

Height: 00.0

Width: 00.0

Inspector: Stuart May
Inspection Date: 09/12/2018

Structure Number: 2000033
Facility Carried: CR 18

Bridge Inspection Report

Channel Measurement

Date of Channel Measurements:
Distance Measured From:
Depth Measured From:
Number of Measurement Points Taken:

Number of Fixed Objects in Channel:
Water Level:
High Water Mark:
Measurement Type:

Inspector: Stuart May
Inspection Date: 09/12/2018

Asset Name: 20-00156
Facility Carried: CR 18

Bridge Inspection Report

Date Reported:

Priority:

Work Code:

Deficiency Description:

Work Description:

NO MAJOR WORK NEEDED.

Date Repairs Completed:

Maintenance Comments:

RECOMMEND INSTALLING CURRENT STANDARD BRIDGE AND APPROACH RAILING AND CLEANING END JOINTS AND EDGES OF THE DECK.

LOAD RATING - BRADIN

Load Rating Date:

National Bridge Inventory (NBI):

(66B) INVENTORY RATING (H):	20	(31) DESIGN LOAD:	5
(65) INVENTORY RATING METHOD:	0	(70) BRIDGE POSTING:	5
(66) INVENTORY RATING:	36	(41) STRUCTURE OPEN/POSTED/CLOSED:	A
(63) OPERATING RATING METHOD:	0	(66C) TONS POSTED:	
(64) OPERATING RATING:	45	(66D) DATE POSTED/CLOSED:	

Posting Configurations:

Emergency Vehicles:

EV2: LEGAL RF:

EV3: LEGAL RF:

5-Axles:

AASHTO TYPE 3S2: LEGAL RF:

SU5: LEGAL RF:

TOLL ROAD LOADING NO. 1: ROUTINE PERMIT RF:

2-Axles:

H20-44: LEGAL RF:

ALTERNATE MILITARY: LEGAL RF:

6+-Axles:

AASHTO TYPE 3-3: LEGAL RF:

LANE TYPE: LEGAL RF:

3-Axles:

HS20: LEGAL RF:

AASHTO TYPE 3: LEGAL RF:

SU6: LEGAL RF:

SPECIAL TOLL ROAD TRUCK: ROUTINE PERMIT RF:

SU7: LEGAL RF:

4-Axles:

SU4: LEGAL RF:

TOLL ROAD LOADING NO. 2:
ROUTINE PERMIT RF:

MICHIGAN TRAIN TRUCK NO. 5: ROUTINE PERMIT RF:

MICHIGAN TRAIN TRUCK NO. 8: ROUTINE PERMIT RF:

Other Configurations:

H20-44: DESIGN RF:

NRL: LEGAL RF:

SUPERLOAD-11 AXLES: SPECIAL PERMIT RF:

SUPERLOAD-13 AXLES: SPECIAL PERMIT RF:

SUPERLOAD-14 AXLES: SPECIAL PERMIT RF:

SUPERLOAD-19 AXLES (152.5T): SPECIAL PERMIT RF:

SUPERLOAD-19 AXLES (240.045T): SPECIAL PERMIT RF:

Project Description

The following information is being provided in lieu of an Abbreviated Engineer's Assessment per IDM 14-2.01(03) part 2.b.

Stage 1 Submission

Des. # 1401749

CR 18 at CR13/CR115. – Intersection Improvement

Concord Township, Elkhart County

1.) Project Location

The proposed undertaking is on County Road 18 between the intersections of County Road 115 and County Road 13. The project takes place in Concord Township, Elkhart County, Indiana.

2.) Project Need and Purpose

The need of the project stems from operational safety issues that currently exist at this intersection. From January 2014 to May 2016, the intersection experienced 17 crashes. The probable cause of these crashes in general is excessive speed, inadequate advanced warning signs for intersection, or a large total intersection traffic volume. Awareness of intersection ahead may be a major concern, due to limited intersection sight distance and the short storage lane on CR 18 between CR 115 and CR 13.

The purpose of the project is to increase the operation safety at the intersection by replacing the exiting signalized intersection with a safer alternative while still maintaining, or improving, operation capacity for the projected traffic volume demand. Roundabouts have proven to be a significantly safer and more efficient option than the standard signalized intersection up to a threshold traffic volume level. As the projected volumes for this intersection are far below the maximum threshold for roundabouts, replacing the existing signalized intersection with a roundabout is a viable option to accomplish both increased safety and satisfactory levels of service.

3.) Existing Facility

CR 18 is an east-west roadway, classified as a Minor Arterial and is a two-lane, two-way asphalt roadway. The west approach has a dedicated left-turn lane extending from the bridge to CR 13, and one shared through-right-turn lane. CR 18 is stop controlled with CR 13 and free at CR 115. CR 115 and CR 13 are north-south roadways. CR 115 is classified as a minor collector, while CR 13 is classified as a minor arterial. CR 115 and CR 13 are two-lane, undivided asphalt roadways that form two T-intersections with CR 18. CR 115 and CR 13 are stop controlled at CR 18.

4.) Traffic Data (from turning movement study provided by INDOT)

CR 18 (Minor Arterial):

2019 AADT: 10,506

Growth Rate: 0.2%

0.7% trucks

CR 13 (Minor Arterial):

2019 AADT: 8,475

Growth Rate: 0.5%

0.4% trucks

CR 115 (Minor Collector):

2019 AADT: 1,845

Growth Rate: 1.2%

0.4% trucks

5.) Identification of Proposal

The proposed scope for improvement involves construction of an elongated roundabout. Improvements to the intersection are needed to reduce the frequency and severity of vehicular accidents that occur at the intersection. This will provide a facility capable of handling the anticipated traffic growth as well as reducing the number of signalized intersections along arterial corridors.

6.) Cost Estimate

\$1,709,011.01

7.) Environmental Issues

A red flag investigation found 19 NWI – Wetlands, 12 lakes, and 1 River within a half mile of the project. The proposed alternative will not have any impacts on any of these items of concern. If impacts become necessary, then the INDOT Environmental Services Division will be consulted.

8.) Right-of-way Impact

Existing Right-of-Way extends approximately 30 feet offset of the centerline of CR 18. The Right-of-Way along CR 115 extends approximately 25 feet offset of the centerline. No recorded right-of-way exists within parts of the project limits. Right-of-way will need to be acquired as part of the project.

9.) Traffic Maintenance During Construction

The intersection will be closed and through traffic will be detoured utilizing Toledo Rd. to the north, County Road 17 to the east, and County Road 45 to the south and west.

10.) Concurrence

Kent Schumacher
Elkhart County Engineer

Land and Water Conservation Fund Grants: Indiana

The Park Service is finding out about more closures and conversions of federally protected parks than ever before. But no one knows just how many, so InvestigateWest compiled this database, which lists every LWCF grant between 1965 and 2011, as a starting point. Click a column header to re-sort the table. Click-shift to add a secondary sort.

[RETURN TO THE PROJECT PAGE](#)

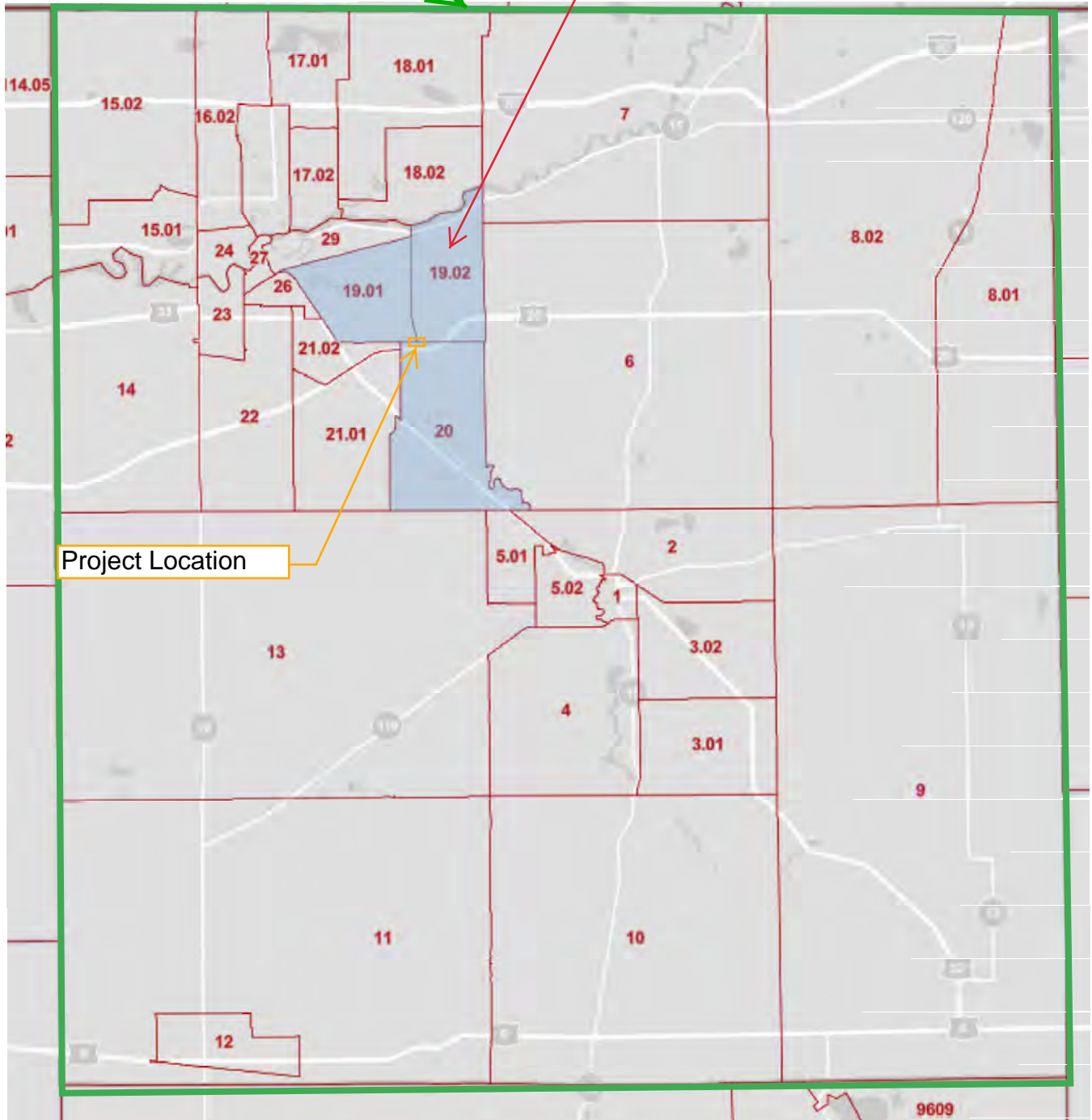
FILTER THE LIST:

Grant ID & Element	Grant Name	Sponsor	County	State	Grant Amount	Year Approved	Year Completed	Type
54 - XXX	ELKHART COUNTY PARK	ELKHART COUNTY PARK BOARD	ELKHART	IN	\$177,997.76	1969	1974	Development
64 - XXX	JOHN DERKSEN PARK	NAPPANEE PARK BOARD	ELKHART	IN	\$5,000.00	1970	1970	Acquisition
74 - XXX	OX BOW PARK ACQUISITION	ELKHART COUNTY PARK BOARD	ELKHART	IN	\$23,625.00	1971	1974	Acquisition
99 - XXX	JOHN DERKSEN PARK	NAPPANEE PARK BOARD	ELKHART	IN	\$33,708.65	1971	1974	Combination
257 - A	MASTER-ELKHART PARK IMPROVEMENTS	ELKHART COUNTY PARK BOARD	ELKHART	IN	\$89,048.76	1976	1979	Combination
257 - C	MASTER-ELKHART PARK IMPROVEMENTS	ELKHART COUNTY PARK BOARD	ELKHART	IN	\$31,591.00	1976	1979	Development
257 - B	MASTER-ELKHART PARK IMPROVEMENTS	ELKHART COUNTY PARK BOARD	MULTI-COUNTY	IN	\$74,860.24	1976	1979	Development
283 - XXX	HIGH DIVE PK IMP	ELKHART COUNTY PARK BOARD	ELKHART	IN	\$70,225.00	1977	1980	Development
310 - XXX	MCNAUGHTON PARK IMPROVEMENTS	ELKHART COUNTY PARK BOARD	ELKHART	IN	\$92,246.99	1978	1981	Development
337 - XXX	NAPPANEE GOLF COURSE AND PARK	NAPPANEE PARK BOARD	ELKHART	IN	\$197,371.00	1979	1984	Combination
339 - XXX	D/PARSONS - SHOUP WOODS	GOSHEN PARK BOARD	ELKHART	IN	\$29,977.85	1979	1983	Combination
340 - XXX	D/RIETH PARK	GOSHEN PARK BOARD	ELKHART	IN	\$22,700.00	1979	1983	Combination
354 - XXX	PIERRE MORAN PARK RENOVATION	ELKHART COUNTY PARK BOARD	ELKHART	IN	\$115,000.00	1979	1983	Development
441 - XXX	HIGH DIVE IMPROVEMENTS '85	ELKHART COUNTY PARK BOARD	ELKHART	IN	\$100,000.00	1985	1989	Development
450 - XXX	D/DERKSEN FARM ACQUISITION	NAPPANEE PARK BOARD	ELKHART	IN	\$100,000.00	1986	1992	Combination
470 - XXX	STUDEBAKER/BAKER RENAISSANCE	ELKHART COUNTY PARK BOARD	ELKHART	IN	\$100,000.00	1990	1994	Development
554 - XXX	CORBUS CREEK COUNTY PARK	ELKHART COUNTY PARK BOARD	ELKHART	IN	\$200,000.00	2005	2009	Combination

AN
INVESTIGATEWEST
DATA PROJECT

Community of
Comparison
(COC) Elkhart
County

Affected Community
(AC)



Note: This is a modied view of the original table pr oduced by the U.S. Census Bureau.

Note: This download or printed version may have missing information from the original table.

HISPANIC OR LATINO ORIGIN BY RACE

Survey/Program:

American Community Survey

Universe:

Total population

Year:

2018

Estimates:

5-Year

Table ID:

B03002

Source: U.S. Census Bureau, 2018 American Community Survey 1-Year Estimates

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see ACS Technical Documentation). The effect of nonsampling error is not represented in these tables.

While the 2018 American Community Survey (ACS) data generally reflect the July 2015 Office of Management and Budget (OMB) delineations of metropolitan and micropolitan statistical areas, in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB delineations due to differences in the effective dates of the geographic entities.

Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Explanation of Symbols:

- An "***" entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.
- An "-" entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution, or the margin of error associated with a median was larger than the median itself.
- An "-" following a median estimate means the median falls in the lowest interval of an open-ended distribution.
- An "+" following a median estimate means the median falls in the upper interval of an open-ended distribution.
- An "****" entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.
- An "*****" entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.
- An "N" entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.
- An "(X)" means that the estimate is not applicable or not available.

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

	Elkhart County, Indiana	Census Tract 19.01, Elkhart County, Indiana	Census Tract 19.02, Elkhart County, Indiana	Census Tract 20, Elkhart County, Indiana
	Estimate	Estimate	Estimate	Estimate
▼ Total:	203,604	5,915	4,119	7,570
▼ Not Hispanic or Latino:	171,843	4,628	3,511	6,667
White alone	152,835	3,840	3,199	6,153
Black or African American alone	10,694	577	84	321
American Indian and Alaska Native alone	265	0	8	23
Asian alone	2,156	26	112	47
Native Hawaiian and Other Pacic Islande	83	0	0	0
Some other race alone	599	0	0	0
▼ Two or more races:	5,211	185	108	123
Two races including Some other race	104	0	0	0
Two races excluding Some other race, a	5,107	185	108	123
▼ Hispanic or Latino:	31,761	1,287	608	903
White alone	25,187	1,080	558	836
Black or African American alone	26	0	0	0
American Indian and Alaska Native alone	175	0	0	54
Asian alone	0	0	0	0
Native Hawaiian and Other Pacic Islande	66	0	0	0
Some other race alone	4,768	207	50	13
▼ Two or more races:	1,539	0	0	0
Two races including Some other race	828	0	0	0
Two races excluding Some other race, a	711	0	0	0

Note: This is a modied view of the original table pr oduced by the U.S. Census Bureau.

Note: This download or printed version may have missing information from the original table.

POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE

Survey/Program:
American Community Survey

Universe:
Population for whom poverty status is determined

Year:
2018

Estimates:
5-Year

Table ID:
B17001

Source: U.S. Census Bureau, 2018 American Community Survey 1-Year Estimates

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see ACS Technical Documentation). The effect of nonsampling error is not represented in these tables.

While the 2018 American Community Survey (ACS) data generally reflect the July 2015 Office of Management and Budget (OMB) delineations of metropolitan and micropolitan statistical areas, in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB delineations due to differences in the effective dates of the geographic entities.

Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Explanation of Symbols:

An "***" entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.

An "-" entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution, or the margin of error associated with a median was larger than the median itself.

An "-" following a median estimate means the median falls in the lowest interval of an open-ended distribution.

An "+" following a median estimate means the median falls in the upper interval of an open-ended distribution.

An "****" entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.

An "*****" entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.

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An "(X)" means that the estimate is not applicable or not available.

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

	Elkhart County, Indiana	Census Tract 19.01, Elkhart County, Indiana	Census Tract 19.02, Elkhart County, Indiana	Census Tract 20, Elkhart County, Indiana
	Estimate	Estimate	Estimate	Estimate
✓ Total:	199,933	5,915	4,097	7,519
✓ Income in the past 12 months be	26,675	1,756	242	1,064
✓ Male:	11,307	1,104	67	414
Under 5 years	2,016	50	14	94
5 years	192	50	0	33
6 to 11 years	1,936	322	14	54

12 to 14 years	614	20	0	0
15 years	329	18	0	83
16 and 17 years	337	18	0	0
18 to 24 years	1,252	301	25	11
25 to 34 years	727	78	0	51
35 to 44 years	1,267	58	14	0
45 to 54 years	1,258	159	0	52
55 to 64 years	882	30	0	18
65 to 74 years	238	0	0	18
75 years and over	259	0	0	0
▼ Female:	15,368	652	175	650
Under 5 years	1,547	49	0	101
5 years	188	0	0	0
6 to 11 years	1,925	159	0	20
12 to 14 years	1,121	75	13	0
15 years	171	17	0	0
16 and 17 years	329	17	27	0
18 to 24 years	2,022	12	47	141
25 to 34 years	2,525	103	23	150
35 to 44 years	1,934	166	10	47
45 to 54 years	1,259	28	24	79
55 to 64 years	1,201	0	11	38
65 to 74 years	461	0	0	17
75 years and over	685	26	20	57
▼ Income in the past 12 months at	173,258	4,159	3,855	6,455
▼ Male:	87,311	2,246	1,853	3,066
Under 5 years	5,696	19	56	162

From: [Fair, Terri](#)
To: [Lisa Vale](#)
Cc: [Miller, Brandon](#); [Bales, Ronald](#)
Subject: FW: Des 1401749 EJ Population Coordination
Date: Friday, July 17, 2020 3:01:37 PM
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image007.png](#)

Hi Lisa,

INDOT-Environmental Services Division (ESD) has reviewed the project information along with the Environmental Justice (EJ) Analysis for the above referenced project. With the information provided, the project may require minimal right-of-way, require no relocations, and would not disrupt community cohesion or create a physical barrier. With the information provided, INDOT-ESD would not consider the impacts associated with this project as causing a disproportionately high and adverse effect on minority and/or low income populations of EJ concern relative to non EJ populations in accordance with the provisions of Executive Order 12898 and FHWA Order 6640.23a. No further EJ Analysis is required.

Best,

Terri Fair

NEPA Specialist

100 North Senate Ave., Room N642-ES
Indianapolis, IN 46204

Office: (317) 232-0680

Email: tfair@indot.in.gov



To ensure that all NEPA documents are submitted appropriately in ERMS to the NEPA Document Review Unit, please be sure to include the following:

1. The document type (CE/EA/EIS/PCE for ITS/Noise Analysis/ECF/AI/NTF/Bat Language) within the subject line and the body of the text.
2. State in the body of the email who the document is intended for based on the CE Manual
 - a. PCE and State projects that are a CE-2 or lower to the appropriate district environmental supervisor/team lead
 - b. LPA and State projects that are a CE-3 and above or EA/EIS to the INDOT ESD Document Team Lead at Central Office.
 - c. Specify the name and email address of the recipient who should get the final document (e.g. Brandon Miller, NEPA Document Team Lead at Central Office; email: bramiller1@indot.in.gov)

From: Lisa Vale <lvale@trovergroupp.com>

Sent: Wednesday, June 17, 2020 3:23 PM

To: Bales, Ronald <rbales@indot.IN.gov>

Cc: CJ Cunningham <cjc@troergroup.com>

Subject: Des 1401749 EJ Population Coordination

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Hi Ron,

I am coordinating the EJ analysis results for Des. No. 1401749 a LPA project for a roundabout located along CR 18, at the intersections with CR 13 and CR 115, in Elkhart County. Using the update guidance, EJ populations of concern were identified for census tract 19.01 for both minority and low income populations. Attached is the data for your review.

The scope of the project is the construction of an elongated, "peanut-shaped" roundabout. Roadway reconstruction adjacent for grading will be needed. A block retaining wall will be installed along the east side of CR 115. That will require ROW from adjacent parcels that will be over 0.5 acre, approximately 0.5 acre of permanent and 0.7 acre of temp ROW will be necessary. No relocations are necessary. Tree clearing within 100 ft of the roadways will occur and permanent lighting will be installed. MOT will be a full detour planned to divert traffic to SR 17 and use CR 45 for local traffic.

Please let me know if you need anything else for your determination.

Thanks,
Lisa Vale

Lisa Vale | Senior Environmental Scientist
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